

Arrowhead Mixed Use Center

Previously known as
“Arrowhead Commerce Center”
“Griffith Commerce Center”

Northeast Corner of 83rd Avenue and Thunderbird Road

Z02-31A.6

Planned Area Development Standards and Guidelines Report

Approved May 6, 2003
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Commercial

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Residential

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Introduction

Commercial

This is a request to rezone a 21.88 gross acre parcel from PUD with PC-2 to PAD. The proposed PAD includes a mixture of retail and office uses with a perimeter landscape theme. The PAD includes a permitted use list and development standards within this narrative.

The property is located on the northeast corner of 83rd Avenue and Thunderbird Road. It is approximately one-fourth mile east of the Thunderbird Road and Loop 101 freeway interchange. The subject site is relatively flat and undeveloped as are most of the adjacent parcels. Properties along the south side of Thunderbird Road are developed with a mixture of retail and office uses. The adjacent property to the east along the north side of Thunderbird Road is developed with a plant nursery. All other adjacent properties remain undeveloped (see Vicinity Map, Exhibit A).

Residential

The property was originally zoned for commercial development in 2002. Over the years, a limited number of commercial uses have been developed on the property, but the bulk of the property has remained vacant. This proposed amendment seeks to modify the name of the project to the Arrowhead Mixed Use Center and commensurately allow for the addition of multi-family housing on the property. Given the market shift away from brick and mortar retail stores and the breadth of existing commercial located immediately adjacent as well as within a short distance of the intersection, multi-family is an appropriate and desirable use for the property and therefore should be included as an allowed use within the PAD.

Based on our analysis, an amendment to the property to allow for multi-family will energize the existing businesses and spur development of the remaining vacant commercial pads within the Arrowhead Mixed Use Center. The infusion of the area with residents and providing them with easy and convenient access will create a walkable and integrated neighborhood befitting the goals and objectives of the City of Peoria for lasting and timeless development.

General Plan

Commercial

The PAD request is consistent with the Peoria General Plan. City Council adopted Resolution 02-186 does not require a General Plan amendment when a property is being rezoned from one existing commercial zoning district to another commercial zoning district. The property is currently zoned PUD with PC-2 as the underlying zoning district. The Land Use Element of the General Plan includes an Objective to “Foster commercial, industrial and business park employment centers that are compatible with Peoria’s economic needs and opportunities.” The proposed PAD may add between 255 and over 500 new jobs depending on the exact land use mix developed on the property. The proposed PAD is consistent with the General Plan.

Residential

The subject property and surrounding area is currently designated within the General Plan as “Residential Low” which supports zoning for single-family residential with a density of 2 to 5 units per acre. The subject property and the surrounding multi-family and commercially zoned parcels are out of conformance with the General Plan. In order for multi-family housing to be allowed within the Arrowhead Mixed Use Center, a Minor General Plan Amendment will be needed to designate the property for High Density Residential (15+ du/ac). Given the existing non-conformity, the land use designation of the property is ripe for changing and there are many goals and policies within the Peoria General Plan for Land Use, Growth, and Housing that support a high-density residential designation. The General Plan Amendment will be filed and reviewed concurrently with this PAD Amendment, however, below is a discussion related to how this property fulfills the purpose of the General Plan.

General Plan Goals and Objectives:

The T83 development both aligns with the elements, goals, objectives and policies of the existing General Plan, but amending the General Plan Map to higher density will further support the purposes of the General Plan, and further support existing land uses and adjacent businesses.

Land Use Element

Goal 1: Provide a balance of land uses that will preserve and enhance neighborhoods, promote economic development and encourage redevelopment at appropriate locations.

Objective 1.H: Encourage the development or redevelopment of vacant or underutilized infill sites within the city.

Objective 1.M: Provide a diversity of housing types to meet the needs of persons of all income levels and ages.

Objective 1.N: Support healthy residential environments that provide for safe and convenient access, open space and recreational opportunities, access to public schools and services and protection from incompatible land uses.

Objective 1.O: Support well-designed, high-quality multi-family residential development in appropriate areas which maintains compatibility between adjacent developments.

The proposed development addresses the Land Use Element goal and objectives by the following:

- Allows for development of a parcel that has sat vacant for years.
- Utilizes existing infrastructure, including the use of existing utilities, schools, easy access to existing arterial streets and convenient transportation opportunities, as well as increasing the consumer population at an ideal location for existing retail, grocery, and commercial development that surrounds this area.
- The apartments will boast three stories of living spaces with studio, one, and two bedroom options for a total of 183 units. Our smaller unit options will provide overall lower rents.
- Secured gated access, while remaining walkable and interconnected with surrounding development.
- Includes abundant open space equaling 44% greater than the required amount.
- Social and recreational amenities that will be included are a state of the art club house, indoor fitness center, business and social centers, pool, and spa with a video wall and lounge areas, outdoor fitness plaza, outdoor kitchen, dog park, barbecue and picnic areas

throughout, and a children's play area. There will also be secured bicycle storage near the front of the complex that will promote a user friendly alternative mode of transportation.

- Increased community character and sense of place through the extensive amenities package of social and recreational environments as well as community social and service events.
- Developing multifamily instead of the General Plan designation of single family is a more suitable product based on the existing development and surrounding land uses, and we have designed adequate and considerate buffer to all neighboring development.
- A focus on high quality buildings that align closely with the direction of Peoria's Design Review Manual, and high dedication to the environment and our community experience.

The expected High density Residential (HDR) designation allows for 15+ du/ac; however the target density and presumption is 18 du/ac. The Peoria General Plan specifically allows for an increase to the Target Density for parcels within designated infill areas. While the subject parcel is located within the North Peoria Redevelopment Area, this is not specifically an “infill area” so the automatic increase of 1 du/ac does not apply. To justify a Target Density above 18 du/ac for the planned project, we will “exhibit superior quality and design” or “provide amenities, dedications, or improvements above and beyond the minimum city standards, as discussed elsewhere in our narrative.

Circulation Element

Objective 1.A: Develop a sustainable transportation system within Peoria that is compatible with and designed to complement, the existing and proposed land uses as provided in the Land Use Plan, without diminishing the efficient movement of all users, all modes, goods and services.

Objective 1.C: Develop neighborhood street (local) patterns and circulation systems which preserve neighborhood integrity and serve local traffic and discourages non-local or cut-through traffic.

The proposed development addresses the Circulation Element goal and objectives by the following:

- The location selection for this development supports Peoria's circulation goal and seeks to maximize the city's multi-modal transportation while minimizing adverse impacts on neighborhoods and businesses.
- To ensure walkability, the proposed development establishes a neighborhood with easy access to public transportation, including Metro Busline 138, as well as crosswalks to neighboring stores such as CVS, Sprouts and multiple dining options to ensure walkability.
- Entrances and exits onto and off of access roads will connect with arterial streets. The existing deceleration lane from Thunderbird Road to the access road will assist in avoiding congestion, along the well-traveled arterial.

Housing Element

Objective 1.A: Promote quality residential development through diversity in housing type.

Policy 1.A.4: Review demographic data and respond to housing-type preference trends. The City's housing stock and Zoning Code should be reviewed against this data to ensure desired housing-types are provided and encouraged.

The proposed development addresses the Housing Element goal and objectives by the following:

- This development will address the need for quality residential development through diversity in housing types. The proposed development will contribute toward the need for rental apartment housing.
- The City of Peoria is projected to need 5,151 new multi-family dwelling units between 2010 and 2020. Based on historically low multi-family vacancy rates as well as rising rents, this site will assist in filling the City of Peoria's need of multi-family housing.
- Based on the current and anticipated demographics of this area, impacted by the proximity to nearby colleges, the draw to the P83 area, and the existing availability of single family homes for rent, we are providing a more urban style community that will distinguish ourselves from the other options and will serve current and future needs of the residents of the City of Peoria.

Density Justification:

The expected High Density Residential (HDR) designation allows for 15+ du/ac; however the target density and presumption is 18 du/ac. The target density within density range is generally considered the maximum allowable density for projects meeting the minimum City requirements. While the subject parcel is located within the North Peoria Redevelopment Area, this is not specifically an "infill area" so the automatic increase of 1 du/ac for infill does not apply. In order to exceed the Target Density of 18 du/ac for the planned project, the T83 project must "exhibit superior quality and design" or "provide amenities, dedications, or improvements above and beyond the minimum city standards."

With its 183 units, T83 proposes a gross density of 21.35 units per acre. In order to justify this density the project enhancements, include but are not limited to:

- Enhanced entry monument including a pillar within which presents the project name in a brushed nickel sign and glass mosaic tile, and an extending theme wall made of welded wire mesh and glass infill, as well as textured paving throughout the entire entry drive and entry gates designed to tie in the clean and elegant architectural form with tubular steel to match our balconies and steel gridding, all creating a strong sense of arrival.
- Multiple building types and color schemes, all with unique layered depth and building massing along with pop-out projections and variable roof planes to create residential scale. The building walls will vary between lace stucco finish, sand finished banded stucco, and stone accents at selected wainscot locations. Painted steel will be used for railings and guardrails, entry doors and garage doors, as well as the shade louvers at West and South exposures. Concrete tile will be used on the roof, with different presentation on different buildings. (See Exhibit G for Building Elevations and Materials).
- Building designed with consideration for neighbors in mind, with buildings setback further than required, and staggered building heights toward Thunderbird exposure. A tallest building height of 40', lower than the 48' maximum, and 6' CMU walls along Northern and Eastern boundaries, increasing to 7' at Northeastern corner with hostile vegetation, out of consideration for single family residential concern of individuals climbing their fences.
- High quality building design with multiple options related to size of units and layouts.
- Both attached and detached garages in an auto-court design, and staggered building types and locations to offer buffering from neighboring developments.

- Exceeding the useable open space requirement of 39,450 sq. ft. by 44%, encouraging communal gathering, and enjoyment of space and landscape.
- Enhanced and additional community amenities including:
 - a clubhouse with multiple lounges for social and community gathering and events,
 - a large pool and deck lounge area with misting and fogging for comfort and enjoyment during the Summer,
 - an oversized spa for relaxing with friends,
 - an oversized video wall and sound system in the pool and spa area for community movies, sporting events and general enjoyment,
 - an outdoor kitchen pavilion with spacious gas grill, seating and lounge area, flat screen TV, pool table and fire pit,
 - a community garden with herbs and citrus near the outdoor pavilion and cooking area for visual and culinary satisfaction,
 - a large exercise and fitness room attached to the clubhouse with top of the line fitness equipment and 'Fitness on Demand' technology to encourage both individual and social exercise,
 - an outdoor exercise/yoga/fitness plaza with 'Fitness on Demand' interactive exercise routines available for individuals or groups, and a neighboring open turf area to increase the space available for active engagement or relaxation,
 - a central dog park for recreation and to help maintain a clean and friendly environment for everybody,
 - three grilling stations located throughout the community,
 - a children's play area within a large open space that is central to our property,
 - secured, lockable bicycle parking to encourage that commute and leisure option,
 - and electric car charging stations spaced throughout the development.
- Increased community character and sense of place through the extensive amenities package of social and recreational environments as well as community social and service events.
- Infill of a vacant parcel and utilization of existing infrastructure. See Exhibit H for Existing Condition Site Photos.
- Incorporation of water conservation within the individual units will take place through low-flow faucets and showerheads, low water use toilets, higher efficiency clothes washers and dishwashers, as well as individual water meter rough-ins as well as drought-tolerant landscaping. We have also incorporated onsite retention facilities to capture stormwater runoff from the developed site, storing 100% of the 100-year 2-hour storm event or 2.4" of rainfall for this particular location. This rain water will be captured onsite and recharged back into the ground water through the use of onsite drywells, recharging the groundwater and allowing the water to be stored in the existing aquifer for later use through extraction by the use of properly permitted wells.
- Applying energy efficient building standards throughout, including high efficiency windows, energy star appliances, exterior metal louvers on select windows for sun control, exterior amenities to reduce heating and cooling demand as well as encourage exercise and socializing, as well as high building and insulation standards for comfortable and efficient living environments.

- Increased interconnectivity and walkability throughout the mixed use center, to neighboring retail, grocery, dining and other commercial, and access to main bus lines, transit corridors and bike paths.
- The General Plan projects a need for 5,151 new multi-family dwelling units between 2010 and 2020, and our proposed increased density, while also exceeding requirements and elevating the standard for multi-family developments will help the city meet this need.

PAD Conceptual Plan

The PAD conceptual plan illustrates the potential for a neighborhood shopping center and two office or general commercial parcels. The shopping center concept illustrates a major tenant, shops, and four retail pads. The center provides one driveway access off of 83rd Avenue and three driveways off of Thunderbird Road with a fourth driveway access to accommodate delivery vehicles. A cul-de-sac along the north side of the site allows two driveways and the delivery vehicle driveway. The cul-de-sac also provides access to the adjacent parcels east of the shopping center and to the properties located north of the site.

The conceptual plan illustrates how two parcels can be created east of the shopping center parcel to allow development of office or other commercial uses. The plan shows access to these two parcels with three driveways off Thunderbird Road and a secondary access driveway off the cul-de-sac to the north. The perimeter landscape tracts along Thunderbird Road and 83rd Avenue are 20-feet wide. The landscape tract along the cul-de-sac is 10-feet wide. The landscape tract along the east property line is 20-feet wide.

The conceptual plan illustrates the potential for retail and office uses with different size buildings and building arrangements. The plan provides a 20-foot perimeter-building setback to create separation from 83rd Avenue and Thunderbird Road as well as provide for perimeter landscaping. The plan illustrates the ability to hide parking behind landscaping and screen walls to block visibility to parking lots from 83rd Avenue, Thunderbird Road, and the cul-de-sac. The driveway and parking lot layout illustrate how separate users can provide cross-access easements to allow the distribution of traffic movements to and from different driveways along 83rd Avenue and Thunderbird Road. The plan shows that entry features and paving treatments can be provided to add character.

The Landscape Ordinance requires 20 percent of the net site area to be landscaped with five percent of the net site to be open space. The plan shows the potential to create landscaping along the perimeter of the site and adjacent to buildings. There are several opportunities to create useable open space areas between buildings.

Residential

T83 (see Exhibit F for Preliminary Development Plan) proposes 183 multi-family apartments consisting of a mixture of studio, one bedroom, and two bedroom units.

Our building layout consists of five 3-story buildings, two 2-story buildings, three 1-story detached garage structure, as well as the amenities package that will be discussed later. We have designed this community to encourage the enjoyment of our outdoor areas with abundant open space, beautiful landscaping, and with various outdoor exercise, lounge, cooking and play areas.

As demonstrated on the Preliminary Plan, T83’s “L” shape wraps the remaining commercial parcels within the Arrowhead Mixed Use Center and allows for a buffer for the existing residential to the north, northeast, and east and adds a residential face to Thunderbird Road. The main access will be from Ludlow Drive with secondary resident-only gates providing access into the Center’s common drive aisles serving to connect with the existing and planned commercial and the adjacent arterial roadways. Permission to utilize the Center's common drive aisles is granted in the Arrowhead Commerce Center's CCRs, Section 5.b.iii, stating, "If Lot B3 (Southeast Lot) is developed for the Permitted Residential Use, vehicle access to Thunderbird Road may be provided over the east boundary of Lot B5 and the west boundary of Lot B4 or the east boundary of Lot B4 and the west boundary of Lot B3, and such access need not be gated."

As a part of Arrowhead Mixed Use Center, T83 will be a suburban infill development with walkable access within the Center as well as to the commercial retail and offices located west and southwest of the site. Secured bike storage will be provided as an amenity given the prime location near the P83/Arrowhead Regional Mall and convenient access to the Skunk Creek trail system. T83 is located along Valley Metro Bus Route 138 and Route 83 providing connections to all Valley Metro busses and the light rail system.

Development Standards

The development standards for the property are in accordance with the following Development Standards Table:

Commercial

Minimum Size of Lot		Minimum Setbacks for Principal Buildings					Maximum Building Height in Feet	Maximum Percent Lot Coverage
Area	Width	Front	Least Side	Total Sides	Rear	Perimeter		
30,000 Square Feet	200 Feet N/A ⁺	25 Feet	**	**	**	**	36-Feet	45%

- * The minimum lot area allowed shall be determined by the aggregate area of buildings, required yards, off-street parking and loading space and any other specified lot area requirements applicable to the proposed use.
- ** No building shall be closer than twenty (20) feet to any residentially zoned property. No interior building setbacks are required, except as required in the Building Code. Buildings shall maintain a minimum 15-foot setback from any perimeter street right-of-way line or perimeter property line for the entire PAD site.
- + Minimum lot width requirement removed with amendment Z02-31A.3.
Site Plan Review approval is required for principal and accessory buildings in accordance with Section 21-320 of the Zoning Ordinance.

Residential

Lot Standards		Compliance
Maximum Lot Coverage by all Structures	50	23
Minimum Lot Width (feet)	N/R	N/R
Maximum Gross Density (du/ac as calculated by General Plan)	18 ¹	21.35
Principal Building Standards		
Maximum Building Heights (feet)	48 ²	40
Minimum Building Setbacks (feet)		
Front Setback	20	20
Exterior Side and Rear Setbacks	10 ³	10
Interior Side and Rear Setbacks	0 ⁴	5
Accessory Building Standards		
Maximum Building Heights (feet)	20 ⁵	16/22 Club
Minimum Building Setbacks (feet)		
Front Setback	20	20
Exterior Side and Rear Setbacks	10 ³	10
Interior Side and Rear Setbacks	0 ⁴	5
Wall Height Standards		
Maximum Wall Height (feet)	7	7
Landscape Standards		
Landscape Coverage (percent)	20	20
Minimum Landscape Buffer		
Exterior Property Lines	10 ³	13 ⁶
Interior Property Lines	0 ⁴	0

1. Maximum density may be increased up to seven (7) additional units per acre based on a finding that the project incorporates additional amounts of open space, recreational amenities, enhanced landscaping, enhanced project amenities, and/or pedestrian corridors.
2. Building Height: Maximum thirty (30) feet high within thirty (30) feet of any Single-Family Residential District. The height may be increased by one (1) foot per each three (3) feet of additional setback to a maximum of 48 feet.
3. Applies to property lines comprising the exterior boundaries of the PAD.
4. Applies to property lines internal to the PAD.
5. 30 foot height allowed for a clubhouse.
6. Slight encroachment from refuse enclosures.

Parking

Commercial

Parking and Loading Requirements are required in accordance with Section 21-823 through Section 21-826 of the Zoning Ordinance, with the exception that the ratio for required number of parking spaces shall be one (1) parking space per 150 square feet of floor area for all Restaurants uses (including outdoor dining) allowed within the Permitted Uses section on the subsequent pages.

Residential

The required parking spaces total 326 or 1.78 spaces per unit; this is calculated based on 15 spaces for our 15 studios, 132 spaces for our 88 1 beds, 160 spaces for our 80 2 beds, and 18.3 spaces based on 1 additional space per 10 units. Our community meets this standard with 95 spaces in garages and 231 surface parking spaces, including 104 covered carport spaces, 16 compact and 7 handicap accessible spaces.

Signs

Signs are allowed in accordance with Section 21-827 through Section 21-837 of the Zoning Ordinance. T83's entry monument entry monument includes a 7'4" pillar within which presents the project name in a brushed nickel sign and glass mosaic tile, and a 6' x 12' extending theme wall made of welded wire mesh and glass infill. See Exhibit I for Signage Graphic.

Open Space and Landscaping

Commercial

Landscaping will comply with Section 21-815 through Section 21-822 of the Zoning Ordinance.

Residential

Across our community, we have provided useable open space of 56,740 sq ft, exceeding the required open space of 39,450 sq ft by 44% (see Exhibit J for Open Space Plan). We have also designed an lush landscape (see Exhibit K for Conceptual Landscape Plan) across the entire community, breaking up parking and transitions within the property, offering shade and accentuating designated areas, as well as offering a buffer to our neighbors and along fences and drive aisles to enjoy a visual oasis within this community. To enhance the beauty of our community, we have exceeded the landscaping trees requirement by over 25%.

Lighting and Site Security

Residential

Throughout the T83 community, along paths, surrounding amenities and open space, and at key locations for site security and pedestrian visibility, we have provided attractive 12' round pole mounted, dark sky compliant, light fixtures with heavy gauge aluminum and tempered glass jars. Around the parking areas located within drive aisles, we have provided 16' square pole mounted light fixtures for added visibility and illumination. We will also have lighting within the internal hallways of our buildings, for safe access to and from our residences. This community will have sufficient lighting for convenience and security, while providing a pleasant ambience and without undo distraction.

Grading and Drainage

Arrowhead Mixed Use Center is relatively flat and generally drains to the northwest. On-site detention basins or underground retention will be reviewed and approved in the Site Plan review, which will accommodate all storm water drainage. As required by Peoria, drainage will be designed to accommodate 100 year, two-hour storm events.

Permitted Uses

Commercial

- Permitted uses listed in C-2 under Section 21-503 of the Zoning Ordinance. Conditional Uses listed in C-2 permitted by approval of a Conditional Use Permit unless identified as a permitted use in this PAD.
- Mini-Storage.
- RV, Boat Storage. Must be under protective shade cover and screened so not to be visible from public streets or residentially zoned properties.
- Computer center
- Customer service center
- Sub-acute transitional care facility (i.e. short-term surgical recovery center for stays 90 days or less)

Design Guidelines

Commercial

Development within the PAD shall comply with the City of Peoria Design Review Manual for non-residential uses in terms of Architectural Design, Site Design, Landscape Design, and Quantitative Guidelines.

All Landscape Design shall comply with the City of Peoria Zoning Ordinance and the Non-Residential Design Review Manual except for the following: The landscape buffer located along the eastern perimeter of the Mini & RV Storage and within the sewer easement shall be improved with one (1) 5-gallon shrub spaced at each twenty (20) foot interval. No trees shall be placed within the sewer easement.

Residential

T83 has staggered building placement to buffer our site layout from neighboring residential communities by setting 3-story buildings further back and by placing a 1-story detached garage with staggered roof design along Thunderbird. As much as possible, our parking layout is broken into smaller nodes and have strategically used landscaping to break up and beautify parking areas. Our development has considered all design standards for internal pathways for circulation, amenity access, safety and rest, as well as pedestrian pathways to the other areas of the Arrowhead Mixed Use Center, to encourage business growth and resident convenience. Our

buildings have been designed to create intrigue with design, materials, colors and varied effects, as well as to provide shade for South and West facing windows; the 3-story building footprint creates an autocourt for additional interest and variation in building design and to break up any appearance of rows of buildings. We have created beautiful buildings using the design manual's criteria and principles. Our project entry was designed to invite and engage visitors and residents. Our project was designed reflects the crime prevention design standards with its lighting, visibility and access restrictions. As previously discussed, our amenities package is extensive and exceeds the minimum requirements and design standards, for the enjoyment of our community. We have exceeded the requirements for landscaping, and have varied types of trees and shrubs, using native and drought-tolerant vegetation. We have incorporated the outlined standards related to lighting, walls, mechanical equipment and refuse enclosures.

Grading and Drainage

The PAD site slopes from southeast to northwest. On-site retention basins approved in Site Plan Review will accommodate all storm water drainage. Drainage will be designed to accommodate 100-year, two-hour storm events.

Public Utilities and Services

There is an existing 15-inch sewer line and 12-inch water line available to the site from Thunderbird Road. There is an existing 21-inch sewer line and 12-inch water available to the site from 83rd Avenue. There is an existing storm sewer line in Thunderbird Road. There are no overhead utilities on 83rd Avenue with small overhead utilities on Thunderbird Road. There are no streetlights along the 83rd Avenue or Thunderbird frontages. Both 83rd Avenue and Thunderbird Road are constructed to curb and gutter with no sidewalks except along the 83rd Avenue bus pullout. Existing irrigation channels will be under grounded or abandoned. Overhead utilities will be placed under ground. The 83rd Avenue and Thunderbird Road intersection is signalized. The developer will install driveway curb cuts, detached sidewalks, landscaping, and streetlights per Site Plan Review approval.

Sewer	City of Peoria
Water	City of Peoria
Electricity	Arizona Public Service
Telephone	Qwest Communications
Cable TV	Cox Communications
Gas	Southwest Gas Company
Refuse	City of Peoria
Fire and Emergency	City of Peoria
Police	City of Peoria

Phasing

The PAD will be developed as determined by market demand. The necessary onsite and offsite improvements will be determined during Site Plan Review in accordance with Section 21-320 of the Zoning Ordinance.

Stipulations Approved by City Council

- a. The development shall conform to the approved Planned Area Development Standards and Guidelines Report stamp dated January 23, 2003.
- b. Storage facility shall be developed in accordance with the approved stipulations as they pertain to both the RV / Boat Storage and Mini-Storage uses.
 - Mini & RV Storage to be located in the northeast corner of the PAD with limited frontage on Thunderbird Road as generally depicted in the site plan.
 - Mini & RV Storage to provide a minimum 10-foot high perimeter screen wall with minimum 10-foot wide landscaping on the outside of said wall to screen Mini & RV Storage from public view.
 - Landscaping on the perimeter of the Mini & RV Storage along the screen wall shall meet the landscape requirements with the exception that all trees shall be 24-inch box trees.
 - Mini & RV Storage walls and buildings to provide architectural elements that share some colors and materials used in the adjacent retail and office buildings.
 - Mini & RV Storage to provide only one customer entry off Thunderbird Road.
 - Mini & RV Storage shall be limited to 5-acres or less of the PAD site. RV Storage shall be limited to 1.5-acres within the 5-acres.
 - Fifty percent of RV parking spaces to include canopies.
 - Limit Mini & RV Storage hours of operation from 7am to 7pm.
 - Mini-Storage units shall be used for storage only, and not for hobbies or other business pursuits.
 - Conceptual site plan identifies the general location of the Mini & RV Storage. The site plan is illustrative and subject to design and site plan review.
- c. Applicant to provide finalized PAD Standards and Guidelines Report containing approved language, conceptual site plan and conceptual elevations at the time of final approval.
- d. All engineering Improvement plans shall comply with the City of Peoria requirements. Refer to the Infrastructure Guide.
- e. In accordance with the Transportation Element of the Comprehensive Master Plan, 65 feet $\frac{1}{2}$ street right-of-way on both 83rd Avenue and Thunderbird Road is required. The developer shall dedicate a minimum of 65 feet, 35 feet by 35 feet triangle at the intersection, and an 8-foot Public Utility Easement (PUE) along the right-of-way line. Additional right-of-way /easement may be required to accommodate the required meandering 6-foot sidewalk.

- f. The developer of the center will be required to provide full ½ street improvements along 83rd Avenue and Thunderbird Road. These improvements include, but are not limited to curb, gutter, 6-foot meandering sidewalk, paving, and landscaping within the rights-of-way.
- g. A final plat for the entire parcel is required. The final plat review shall be coordinated with the improvement plans.
- h. The Developer will be required to install interconnect along the Thunderbird Road frontage.
- i. The Developer is responsible to provide an Agreement to Install Improvement for the public improvements required by the development with an accompanying financial assurance for subdivision improvements in accordance with City’s requirements.
- j. The Development is responsible to pay all current impact, repayment, and City required fees.
- k. The development will be responsible to comply with the Storm Water Pollution Prevention criteria. This should include runoff control, erosion control, and sediment control.

Approved Amendments

Z02-31A.1 – Allow for 5 gallon shrubs in lieu of 24” box trees to be placed in the 15’ landscape buffer along the eastern PAD boundary.

Z02-31A.2 – Add a use listing to the PAD Standards & Guidelines Report permitting sub-acute transitional care facilities (i.e. short-term surgical recovery center for stays 90 days or less) in the manner described in the narrative and further delineated in the proposed revision to the PAD; and, Clarify parking ratio for this use as 1 .2 spaces per patient room; and, Clarify that interior building setbacks within the PAD is limited to the requirements of the Building Code. In other words, the setbacks applied to other commercial centers (no interior setbacks) are to be applied accordingly.

Z02-31A.3 – Alter the minimum lot width requirements from 200 feet to no minimum required (n/r).

Z02-31A.4 (*Residential*) – The development shall substantially conform to the approved Arrowhead Mixed Use Center PAD Standards and Guidelines Report dated 11/20/2015. The multifamily development within the Arrowhead Mixed Use Center shall be limited to a maximum of 183 units. The Developer shall provide an ALTA Survey reflecting existing boundary and recorded easements on the site with the Civil Improvement Plans. The Developer shall construct forced turn islands to prohibit egress onto eastbound Thunderbird Road at the intersection of Driveway 3 and Thunderbird Road, and at the intersection of Driveway 4 and Thunderbird Road. The Developer shall provide illumination at the southern and western pedestrian entrances to the site.

Z02-31A.5 – For all new commercial developments, a 10' landscape buffer is required along the south and west sides of the internal spine road that separates the commercial parcels from the future residential portion of the mixed-use center.

Z02-31A.6 – Revise the required parking ratio for restaurants with or without outdoor dining uses within the commercial center to one parking space (1) per 150 square feet of gross floor area.

Exhibit A

Vicinity Map

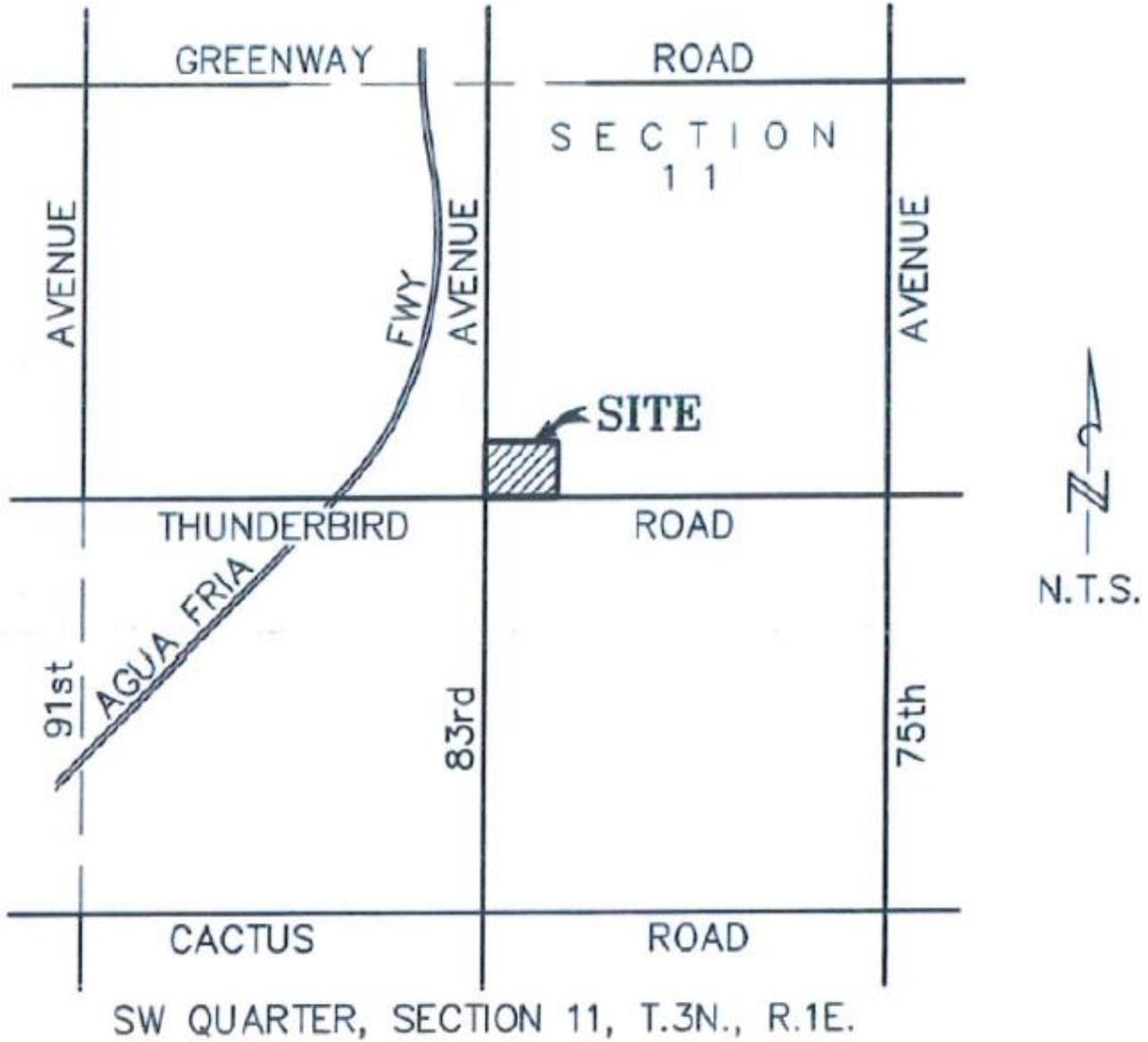


Exhibit B

Previous Land Use Exhibit



Exhibit C

Current Land Use Exhibit



Exhibit D

Zoning Surrounding Arrowhead Mixed Use Center



Exhibit E

Development Existing within Arrowhead Mixed Use Center



Exhibit G

Building Elevations and Materials Building Type 1 - Color Scheme A



BUILDING TYPE 1 FRONT ELEVATION



BUILDING TYPE 1 REAR ELEVATION



BUILDING TYPE 1 LEFT ELEVATION



BUILDING TYPE 1 RIGHT ELEVATION

Building Type 1 - Color Scheme B



BUILDING TYPE 1 FRONT ELEVATION



BUILDING TYPE 1 REAR ELEVATION



BUILDING TYPE 1 LEFT ELEVATION



BUILDING TYPE 1 RIGHT ELEVATION

Building Type 1 - Color Scheme C



BUILDING TYPE 1 FRONT ELEVATION



BUILDING TYPE 1 REAR ELEVATION



BUILDING TYPE 1 LEFT ELEVATION



BUILDING TYPE 1 RIGHT ELEVATION

Building Type 2 - Color Scheme A



BUILDING TYPE 2 LEFT ELEVATION



BUILDING TYPE 2 RIGHT ELEVATION

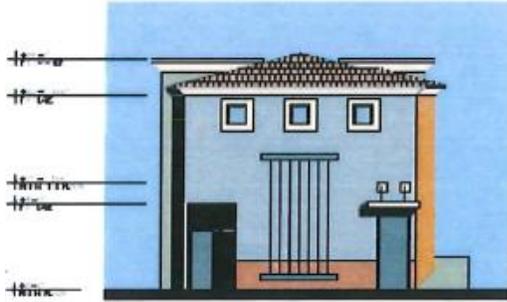


BUILDING TYPE 2 REAR ELEVATION



BUILDING TYPE 2 FRONT ELEVATION

Building Type 2 - Color Scheme B



BUILDING TYPE 2 LEFT ELEVATION



BUILDING TYPE 2 RIGHT ELEVATION



BUILDING TYPE 2 REAR ELEVATION



BUILDING TYPE 2 FRONT ELEVATION

Clubhouse Building - Color Scheme A



CLUBHOUSE RIGHT ELEVATION



CLUBHOUSE LEFT ELEVATION

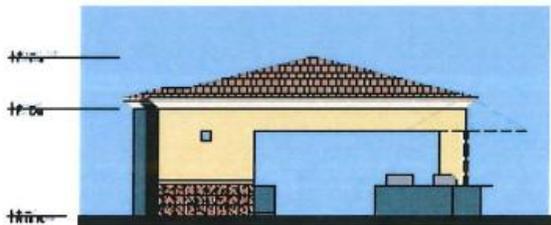


CLUBHOUSE REAR ELEVATION

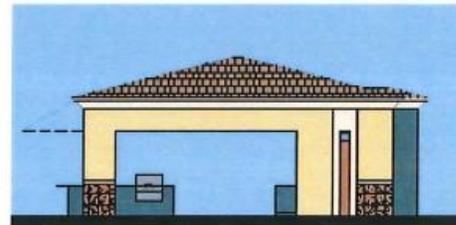


CLUBHOUSE FRONT ELEVATION

Pavilion Building - Color Scheme A



PAVILION LEFT ELEVATION



PAVILION RIGHT ELEVATION



PAVILION REAR ELEVATION



PAVILION FRONT ELEVATION

Garage Building - Color Scheme A



Garage Building - Color Scheme B



Garage Building - Color Scheme C



Colors and Materials - Scheme A

T 83

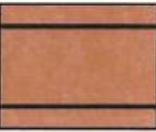
COLOR & MATERIAL LEGEND "A"

	FACIA TRIM / WINDOW COVERS DUNN EDWARDS DE C 153 (ALMOND)
	BODY DUNN EDWARDS DE 5359 (CROSSROADS)
	BODY DUNN EDWARDS DE 6361 (BABY SEAL)
	BODY DUNN EDWARDS DE 528 (RIDDY OAK)
	BODY ACCENT DUNN EDWARDS DE 5272 (SEDONA AT SUNSET)
	BODY ACCENT / HANDRAILS DUNN EDWARDS DE 5148 (LOCHNESS)
	ROOF TILE EAGLE ROOFING CHERRY WOOD
	STONE ACCENT
	BANDED STUCCO / BASE DUNN EDWARDS DE 528 (RIDDY OAK)

Colors and Materials - Scheme B

T 83

COLOR & MATERIAL LEGEND "B"

	FAC & TRIM / WINDOW LOUVERS DUNN EDWARDS DEC 753 (ALMOND)
	BODY DUNN EDWARDS DE 5358 (CROSSROADS)
	BODY DUNN EDWARDS DE 632 (DUSTY DREAM)
	BODY ACCENT / HANDRAILS DUNN EDWARDS DE 5141 (STONE BRIDGE)
	BODY ACCENT DUNN EDWARDS DE 5212 (SECONA AT SUNSET)
	BODY ACCENT DUNN EDWARDS DE 6285 (LINDEN SPEAR)
	ROOF TILE EAGLE ROOFING PADRE BROWN FLASHED (SLURRY FINISH)
	STONE ACCENT
	BANDED STUCCO / BASE DUNN EDWARDS DE 5388 (RIDDY OAK)

Colors and Materials - Scheme C

T 83

COLOR & MATERIAL LEGEND "C"

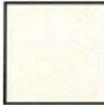
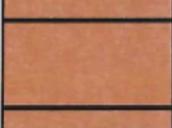
	FACIA TRIM, WINDOW LOUVERS DUNN EDWARDS DE 103 (ALMOND)
	BODY DUNN EDWARDS DE 5359 (CROSSROADS)
	BODY DUNN EDWARDS DE 518 (BBQ)
	BODY ACCENT DUNN EDWARDS DE 5272 (SEBONA AT SUNSET)
	BODY ACCENT HANDRAILS, WINDOW SHADE LOUVERS DUNN EDWARDS DE 6316 (LOCKING GLASS)
	ROOF TILE EAGLE ROOFING CHERRY WOOD
	STONE ACCENT
	BANDED STUCCO + BASE DUNN EDWARDS DE 588 (RUDDY OAK)

Exhibit H

Existing Conditions Site Photos

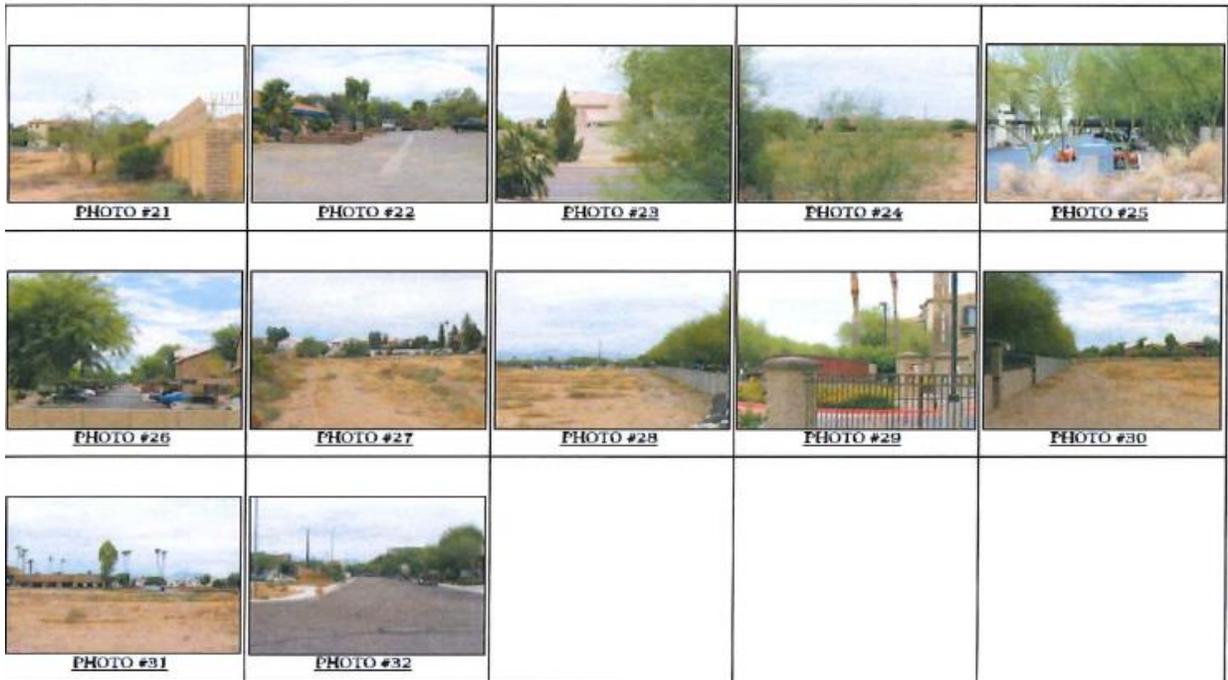
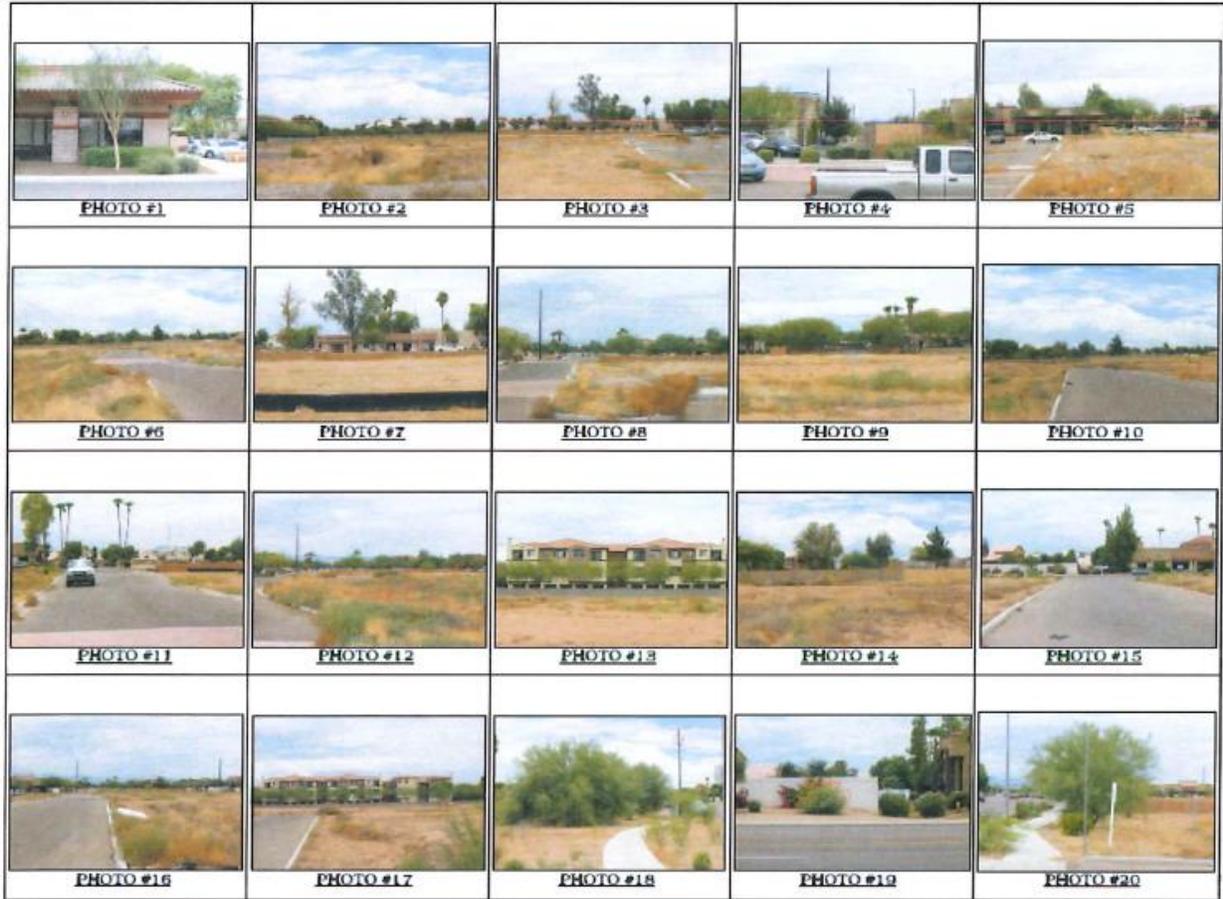


Exhibit I

Signage Graphic

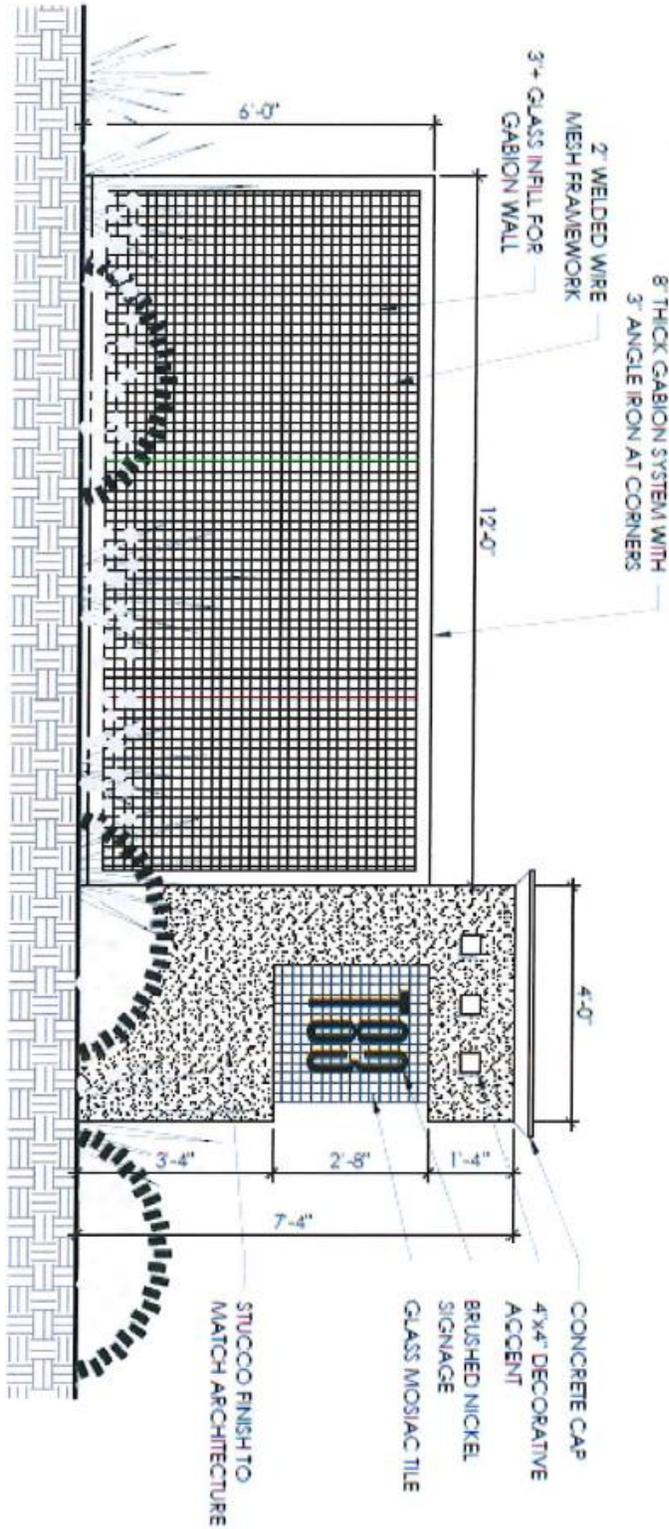


Exhibit J

Open Space Plan

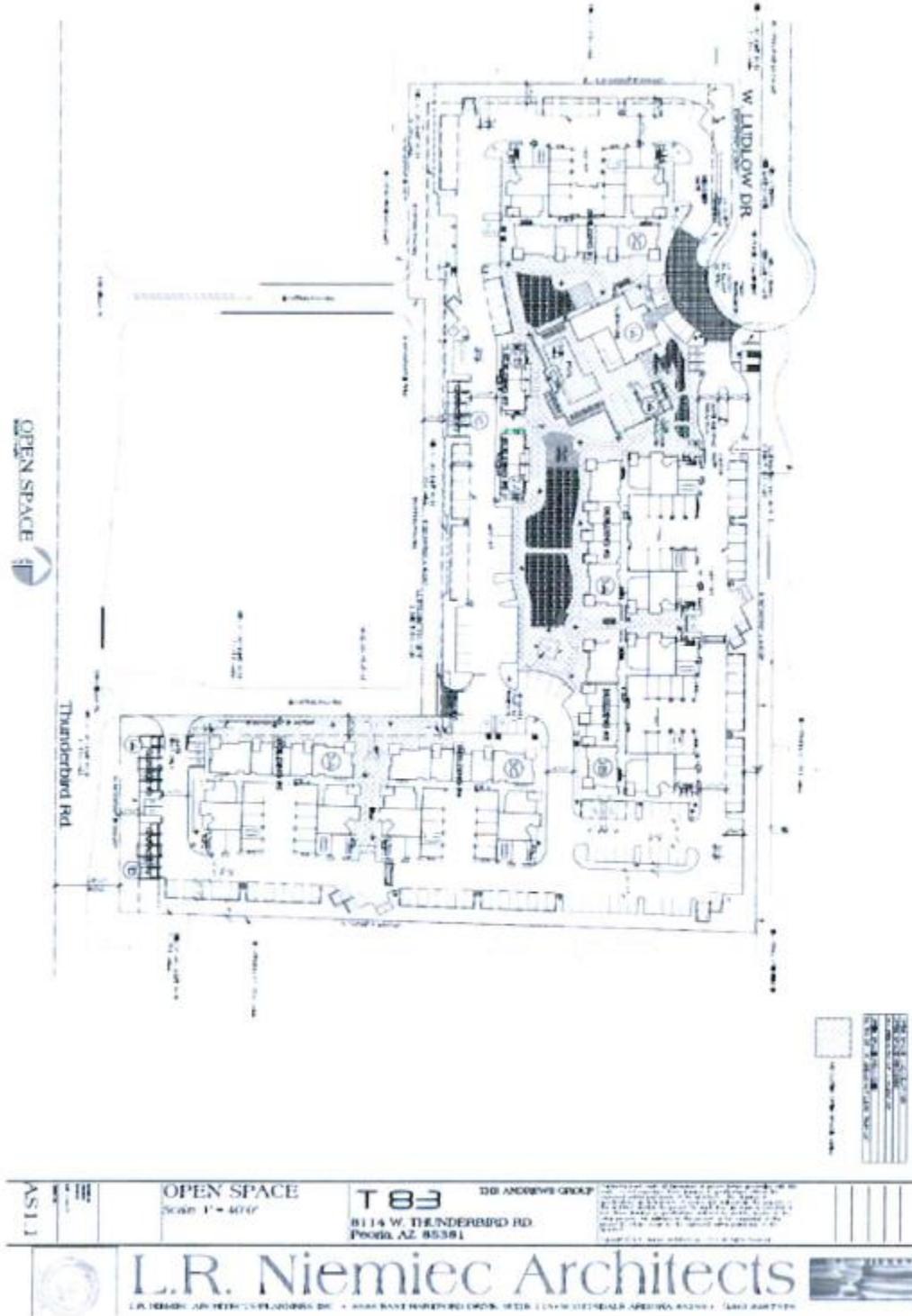
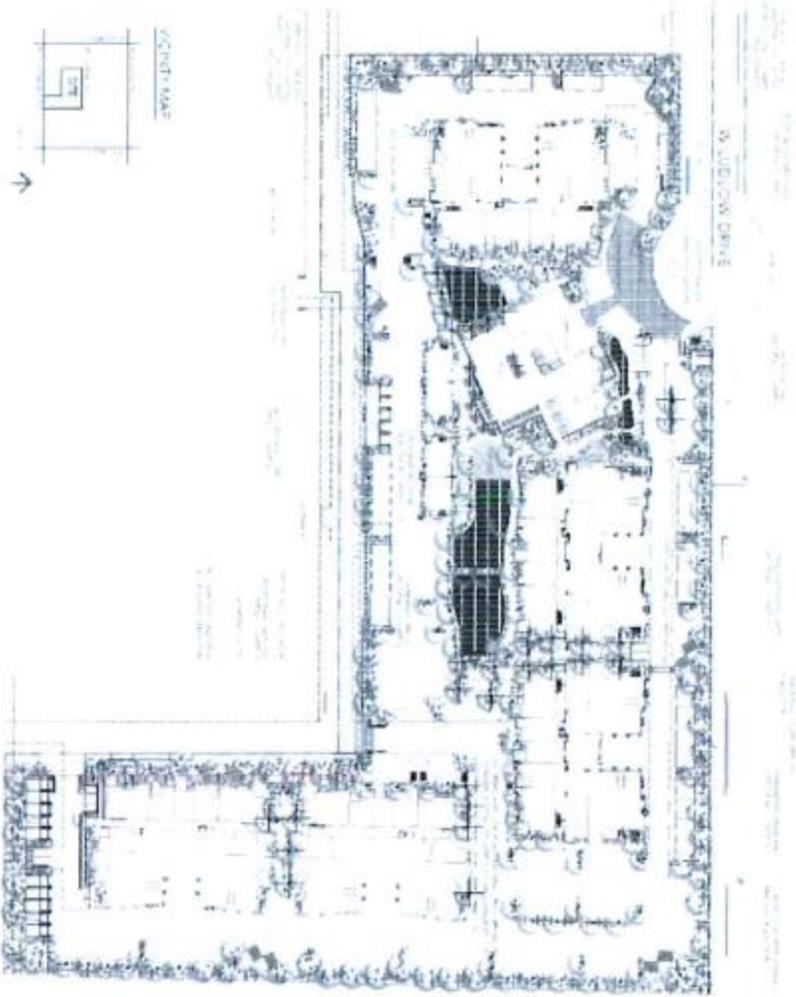


Exhibit K

Conceptual Landscape and Wall Plans

T83 - PEORIA

CONCEPTUAL SITE PLAN
 SHEET 1 OF 4



CITY CENTER - SITE PLANNING DATA

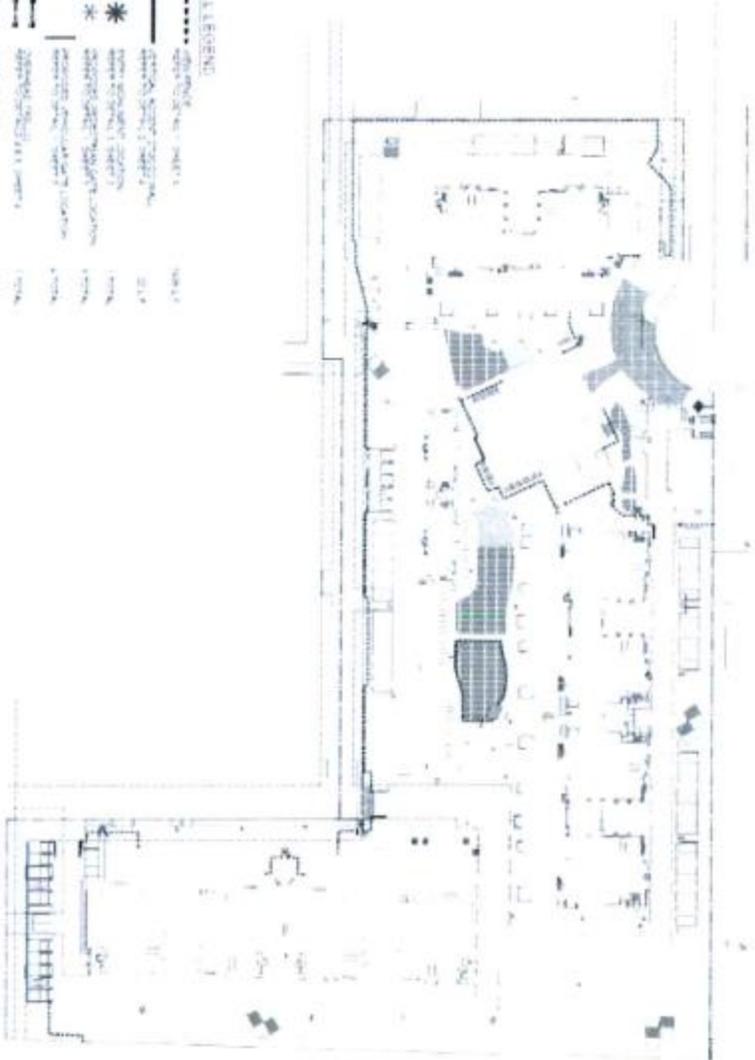
Item	Value
Site Area	1,200,000 sq. ft.
Building Area	1,000,000 sq. ft.
Parking Area	100,000 sq. ft.
Landscaping Area	100,000 sq. ft.
Other	0 sq. ft.

LEGEND

Symbol	Description
[Solid Black]	Building Footprint
[Dotted]	Parking Area
[Hatched]	Landscaping Area
[White]	Other



- WALL LEGEND**
- OPENING
 - OPENING - PART 1
 - OPENING - PART 2
 - OPENING - PART 3
 - OPENING - PART 4
 - OPENING - PART 5
 - OPENING - PART 6
 - OPENING - PART 7
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 - OPENING - PART 98
 - OPENING - PART 99
 - OPENING - PART 100



T83 - PEORIA

1110 UNIVERSITY AVENUE, PEORIA, ILLINOIS 61614

ARCHITECT: UVA URBAN DESIGN STUDIO

DATE: 2014

SCALE: 1/8" = 1'-0"

PROJECT NO.: T83

DATE: 2014

SCALE: 1/8" = 1'-0"

PROJECT NO.: T83

DATE: 2014

SCALE: 1/8" = 1'-0"

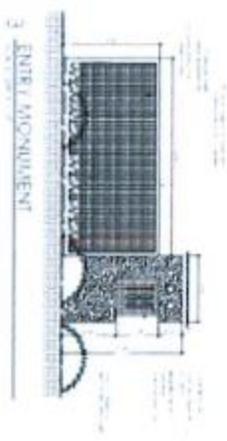
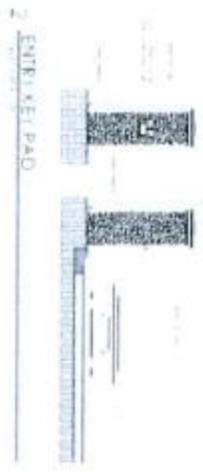
PROJECT NO.: T83

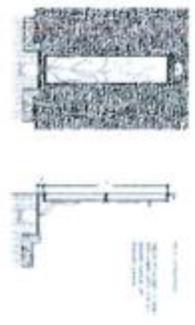
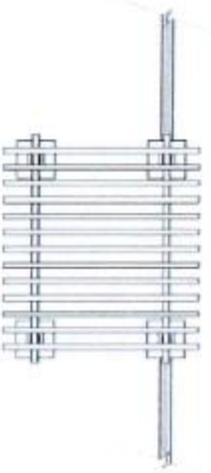
DATE: 2014

SCALE: 1/8" = 1'-0"

PROJECT NO.: T83



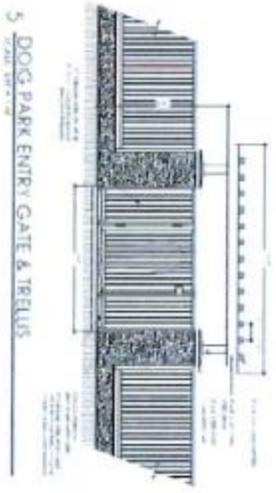




3. WALL-MOUNTED TRELLIS



4. COMMUNITY GARDEN TRELLIS



5. DOG PARK ENTRY GATE & TRELLIS

T83 - PEORIA
CONCEPTUAL LANDSCAPE PLAN
SHEET 1 OF 3



Exhibit L

Driveway Reference

