

**CITY OF PEORIA, ARIZONA
COUNCIL COMMUNICATION**

cc: HR
Amend No. _____

Date prepared: June 17, 2008

Council Meeting Date: July 1, 2008

TO: Carl Swenson, City Manager
FROM: Glen Van Nimwegen, AICP, Community Development Director **GN**
PREPARED BY: Chris M. Jacques, AICP, Interim Planning Manager **CJ**
SUBJECT: DR 08-09, Design Review Principles and Guidelines

City-initiated proposal to consider and approve the *Design Review Principles and Guidelines*. This document provides a comprehensive revision to the City's design standards relating to architectural, site design and thematic quality.

Recommendation:

The Mayor and Council concur with the Design Standards Advisory Board recommendation (4-0) and adopt the attached Ordinance approving the new *Design Principles and Guidelines* document. This document is a comprehensive revision to the City's existing design standards and encompasses architectural, site design and thematic elements.

Design Standards Advisory Board Action:

On June 11, 2008, the Design Standards Advisory Board voted 4-0 to recommend approval of the Final Draft with four (4) corrections (See Draft Minutes). The corrections have been integrated into the Final Draft forwarded to the Mayor and City Council.

Introduction

The City of Peoria adopted its first set of design guidelines in 1997. The design guidelines generally were delineated between architectural and site design. The architectural standards were *quantitative* meaning that a minimum score had to be achieved. In achieving this overall score, several subareas were scored including fenestration (windows), articulation (horizontal break to building footprint), roof plane variation and many others. Several site elements were also scored. In 2005, the design standards were modified, namely replacing the quantitative system with a balance of hard requirements and qualitative (discretionary) guidelines. In short, staff found that good design was difficult

CITY CLERK USE ONLY:

Consent Agenda
 Carry Over to Date: _____
 Approved
 Unfinished Business (Date heard previous: _____)
 New Business

ORD. # 0822 RES. # _____
LCON# _____ LIC. # _____
Action Date: _____

to quantify and that scores could be skewed particularly if one subarea scored excessively high.

Over the last year and a half, the planning staff has embarked upon a comprehensive update to its design standards. Although to some degree the effort was motivated by concerns expressed on design results, staff believed the Manual warranted a fresh look – particularly at the ten-year anniversary of the establishment of the initial standards.

Document Organization

The document has been organized into five (5) chapters replete with photos and diagrams. The photos and diagrams are particularly useful in supplementing the policies and providing shape to the City's overall design message. Some of the highlights of the document include:

Universal

- Architectural Form, Site Design and Thematic Quality standards.
- Includes walls, parking, landscaping, lighting, refuse enclosures etc.
- New CPTED (*Crime Prevention Through Environmental Design*) standards promoting safe design.

Administration (Chapter 1)

- Enhanced Design Review Process
- Definitions

Non-Residential (Chapter 2)

- Includes specific uses such as drive-through facilities, gas stations, car washes, hotels, shopping centers, industrial and "big boxes."

Single-Family Residential (Chapter 3)

- Includes lot and tract design, subdivision "traffic calming" measures, open spaces and architectural form.

Multi-Family Residential (Chapter 4)

- Includes site layout and building orientation, enhanced project entry identity and architectural character.

Supplemental Standards (Chapter 5)

- Old Town
- Lake Pleasant Parkway
- Standards for utility box location and screening

City Council Study Session

On December 18, 2007, the Initial Draft was introduced to the City Council at a study session. Several themes or specific concerns were presented. The following table identifies the concern and proposed action.

Table1: Study Session Comments

Concern	Relevant Policy	Action
Revisit industrial zoning adjacent to residential	Page NR-21 - 22 Policy C.1 – 8	-Avoid new Industrial / SF edges through zoning process. Provide for appropriate transitions and buffering. -Investigate Zoning Code Amendment to provide building height “stepback” provision. -On such edges, residential facing sides to be consistent with front elevations.
Views into Single-Family from Multi-Family	Page MF-2 Policy A.e	-Improve transition through building orientation, clerestory windows, wall height and landscaping. -Investigate Zoning Code Amendment to provide building height “stepback” provision. -Investigate Zoning Code Amendment to increase landscape buffer width from 20 to 30 feet.
Open Space in Multi-Family	Page MF-7: Policies A.6a-f	-Active and passive areas required -Formula changed based on bedroom composition -Project amenities based on units
Traffic Calming – residential neighborhoods	Page SF-4: Policies A.3a-d	-All collector streets exceeding 1,000 feet in length to proactively incorporate traffic calming -Local streets exceeding 1,000 feet to be curvilinear unless traffic calming provided.
Art at corners	Pages NR-17 - 21 Policies A.1a-i Policies B.1a-k	-Thematic consistency throughout developments -Supplemental design criteria for centers over 50,000 square feet and for Large Scale Retail (“Big Box”)
Improve appearance of backs of buildings	Pages NR-13 - 15 Policy A.4 Policies B.1-5 Policies C.1-3 Policies D.1-3	-Proportionate 360 degree architecture -Break building mass into smaller components through articulation, fenestration and wall plane projections
Retention Areas / Drainage Basins (more space)	Page NR-3 Policy A.7	-Drainage basins to not be in front setback unless designed as attractive landscape feature. -Revisit Zoning Code provisions for landscape buffer width along Lake Pleasant Parkway.

Review Process

Since that time, staff held three (3) workshops with stakeholder groups for Non-Residential, Multi-Family Residential and Single-Family Residential and a briefing before the Planning & Zoning Commission. The workshops elicited a dialogue and resulted in the generation of suggested revisions (See attached stakeholder review comments) to the document. Many of the suggestions have incorporated into the Final Draft Design Principles and Guidelines (July 2008).

Table 2: Summary of Review Process

December 18, 2007	City Council Study Session (Process Kick-Off)
January 2008	Draft Design Manual issued
January 22, 2008	Special Joint Meeting with Design Standards Advisory Board and Design Review Appeals Board.
March 13, 2008	OPEN HOUSE: Non-Residential
March 14, 2008	OPEN HOUSE: Multi-Family Residential
March 14, 2008	OPEN HOUSE: Single-Family Residential
May 1, 2008	Planning & Zoning Commission briefing
May 28, 2008	Design Review Appeals Board work session
June 11, 2008	Design Standards Advisory Board Public Hearing
July 1, 2008	City Council Public Hearing and action

Discussion / Analysis:

Through the review process, the City received over 120 suggested revisions (see attached stakeholder reviews). Many of the suggestions resulted in revisions to the document. In summary, the feedback was positive in regards to the design direction that the City was forging. Some of the respondents noted that the standards should continue to maintain a balance between hard requirements flexibility in application. Certainly, staff recognizes that no singular design solution is appropriate or feasible for all application types. With that said, the directive is to upgrade the quality of design in new development projects. Should a design impasse result between the City and applicant, the document proposes a more robust and immediate forum to resolve the dispute.

There were five major topics of discussion from the review process. The topics were introduced to the Design Review Appeals Board at their study session on May 28, 2008. The topic areas were as follows:

[a] Site Plan Process (Chapter 1): The new draft proposes that the 3rd review of Site Plans (Multi-Family / Non-Residential) be automatically forwarded to the Design Review Appeals Board for final action in those cases where “substantial progression” has not been illustrated or a design impasse has been reached. The Planning Manager will determine if “substantial progression” has occurred and whether the item will be forwarded to the Design Review Appeals Board for final action.

[b] Shopping Centers, Office Complexes and other centers exceeding 50,000 square

feet of gross floor area (Chapter 2: Pages NR-18-19): The Draft has been revised to require 4 of the 9 amenities listed in the document. The intent is to prevent “placeless” development through identity and enhancements. The standards in this section are targeted to this development scale and are in addition to other standards in this Chapter.

- [c] Single-Family Roof Materials, Styles and Colors (Chapter 3: Table 2): Discussion on the existing provisions to achieve diversity in single-family residential communities. The number of standard plans, roof colors, roof types and styles is driven by the number of lots within a given Plat.
- [d] Appropriate interface between Industrial uses and single-family residential uses: Typically, the Zoning Ordinance defines the transition in terms of buffering, landscaping, setbacks and other requirements. Staff has proposed new policies in an attempt to blunt some of the impacts in those instances where such land uses are abutting such as roof plane variation, articulation and design treatments so that such facades do not appear secondary (“four-sided architecture”).
- [e] Appropriate interface between multi-family residential and single-family residential areas: Like item [d], the Zoning Ordinance drives many of these factors. However, staff has proposed a new policy to give ability through design review to achieve appropriate building orientation and other design considerations (i.e. clerestory windows) to enhance transition.

Internal Review

In addition to external stakeholders, the document has been routed and reviewed by the Police Department (Crime Prevention through Environmental Design standards) and the Development Engineering Department. The participation of these Departments was critical. To be effective, the document has to be defensible, relevant and feasible to administer.

Public Notice

Public notice was provided in the manner prescribed under Section 14-39-8.D.

Findings / Recommendation:

Based on the findings contained below, the Design Standards Advisory Board forwarded a recommendation of **Approval** of case DR 08-09.

- The format and organization of the new *Design Principles and Guidelines* enhances the overall usability and clarity in design message through the use of photo references, retooled policies and an enhanced process for resolving design impasses; and
- The proposed *Design Principles and Guidelines* advances the General Plan by elevating the sensitivity and quality of the community design and built environment by promoting:

- a. Diversity in development;
- b. Distinctive and safe development templates;
- c. Increased pedestrian and multi-modal connectivity;
- d. Preservation and retention of environmentally sensitive areas;
- e. Sustainable natural and built environments;
- f. Enhancement of the value of property; and
- g. Protection of the overall health, safety and welfare of the community.

Attachments:

- Draft Minutes from June 11, 2008 Design Standards Advisory Board Meeting
- Stakeholder Review Comments
- Ordinance

- Final Draft: *Design Principles and Guidelines (July, 2008)*

DRAFT

DESIGN STANDARDS ADVISORY BOARD MINUTES CITY OF PEORIA, ARIZONA COUNCIL CHAMBERS JUNE 11, 2008

A **Regular Meeting** of the Design Standards Advisory Board of the City of Peoria, Arizona, convened at 8401 W. Monroe St., Peoria, AZ in open and public session at 6:32 p.m.

Members Present: Chair Jon Van Treese, Board Members Sharon Ketelboeter, Roy Bade, and Brandon Squire.

Members Absent: Board Member John Chappelle.

Others Present: Chris Jacques, Acting Planning Manager, Robert Gubser, Senior Planner, Adam Pruett, Senior Planner, and Bev Parcels, Planning Assistant.

Opening Statement: None.

Final call for speaker request forms.

Audience: None.

Note: The order in which items appear in the minutes is not necessarily the order in which they were discussed in the meeting.

CONSENT AGENDA

All items listed with a "C" are considered to be routine by the Design Standards Advisory Board, and were enacted by one motion

1C DISPOSITION OF ABSENCE: None

2C MINUTES: Minutes of the Regular Meeting held January 22, 2008.

Commission Action: Board Member Bade moved to approve the Consent Agenda. The motion was seconded by Board Member Squire. Upon vote, the motion carried unanimously.

REGULAR AGENDA

NEW BUSINESS, PUBLIC HEARINGS and/or ACTION:

3R City of Peoria Design Review Ordinance

Staff Report: Chris Jacques, Acting Planning Manager presented a PowerPoint presentation outlining the revisions made to the Design Manual as proposed by industry representatives.

A discussion occurred concerning the following items:

- Progression of Design Review cases and at what point in the process they should be referred to the Design Review Appeals Board for a decision. The wording 'substantial' was discussed as being too broad a term and subjective to interpretation. Roy Bade inquired about being able to request a review with the Design Review Appeals Board before the 3rd submittal. Chris Jacques replied that that was acceptable.
- Large Scale Retail standards in terms of the 4 requirements for design elements. Roy Bade asked whether retail centers under 50,000 square feet would follow the same standards. Chris replied that those centers would comply with the previous standards set.
- The interface between industrial developments and residential developments that abut. Mr. Jacques explained about increasing the setback away from residential for industrial buildings that want multiple stories.
- The interface between multi-family and single family residential developments. Mr. Jacques commented on providing privacy for single family homes and how angling buildings, landscaping, and building heights would improve privacy.
- The process of changing the Design Manual would involve an initiation to make a change and would be brought before the Design Standards Advisory Board for a vote.
- The findings were presented and staff asked that the Board vote to recommend approval to the proposed amendments to the City of Peoria Design Review Ordinance to City Council.

Board Action: Board Member Bade recommended approval to City Council of the proposed amendments to the City of Peoria Design Review Ordinance. The motion was seconded by Board Member Squire, and upon vote the motion carried unanimously with the following stipulations:

1. Article 20-78-2.II.C.1 (page NR-4) - wording be changed from shall to should.
2. Article 20-78-2.II.C.3 (page NR-4) - wording be changed to – *Parking canopies shall incorporate a design similar to the principle structure on the site or other existing structures in the area.*
3. Article 20-78-2.II.G.8 (page NR-9) - add this item to the single family section of the manual.
4. Article 20-78-2.III.G.5 (page NR-17) - eliminate the word 'bold' from the text.

CALL TO THE PUBLIC FOR NON-AGENDA ITEMS: NONE

REPORT FROM THE DESIGN STANDARDS ADVISORY BOARD: NONE

ADJOURNMENT: There being no further business to come before the Design Standards Advisory Board, the meeting adjourned at 7:25 p.m.

Jon Van Treese, Chair

Date Signed

City of Peoria
 Draft Design Review Manual
 December, 2007
 ERRATA SHEET

No.	Date	Entity	Page	Policy	Comment	Response (City)
					ADMINISTRATION - CHAPTER 1	
1	03/13/08	Industry Meeting	AD-4	E.1/2	Regular scheduling of DRB hearings instead of "as-needed"?	No change. The City Code specifies meeting schedule to be as needed. The meetings are intended to adjudicate appeals or design intent if warranted.
2	02/28/08	JJ Company	AD-4	E.2.b	E.2.b. advisory-describe term/length of appointments to Commission if any exist	No change. Terms already specified in Chapter 20 of the City Code.
3	03/13/08	Industry Meeting	AD-5/6	E.3 a/c	What if staff generates new comments on 2 nd review? The requirement to take all cases to the DRB at 3 rd review seems to be too restrictive. Create flexibility in the process.	Reword to provide flexibility as follows: "In those cases where the 3 rd submittal of the Site Plan has been provided and substantial progression has not been illustrated or a design impasse has been reached, such cases shall be forwarded to the Peoria Design Review Appeals Board for final action."
4	02/28/08	JJ Company	AD-6	E.3 d.1	E.3 d.1 what if no Site Plan exists (grandfathered projects)	Add clarification language that says if no Site Plan exists, an amendment will trigger a Site Plan application and hence Design Review compliance.
5		Staff	AD-10	#30	Add definition for "Façade Articulation Exhibit"	To read: "An exhibit provided with Single-Family design review applications that demonstrates sufficient facade articulation (per Chapter 3) of all standard plans in a Plat."

City of Peoria
 Draft Design Review Manual
 December, 2007
 ERRATA SHEET

6	03/28/08	Pulte	AD-11	#45	We propose to change the word "specific" to "minimum" so that the statement reads "An exhibit which prescribes the <u>minimum</u> front setback line, with the exception of knuckle lots or lots fronting on a cul-de-sac. The word "specific setback" limits the ability to increase the setback to accommodate driveway slopes, product articulation, etc. This definition could be interpreted in the future to mean that any house which deviates from the setback exhibit, even if the setback is increased, is not in conformance with the design review guidelines, which I do not believe was the intent.	Delete #45 and add definition for Facade Articulation Exhibit. The new design standards require the submittal of a "Facade Articulation Exhibit" (SF-12) with single-family design review submittals to replace the Setback Exhibit. Through the Facade Articulation Exhibit, the builder provides the footprint and upper story extent of all standard plans demonstrating compliance with undulation requirements. The intent is to place the emphasis on product design to achieve a varied and interesting street profile.
NON-RESIDENTIAL - CHAPTER 2						
7	02/28/08	JJ Company	NR-3	A 1	A.1. typo (design to designed)	Delete word "design"
8	03/31/08	Valley Partnership	NR-3	A 4	Many buildings will reasonably back onto planned amenities. Please explain what the City seeks to avoid with this guideline.	No change. Infrequent occurrence. The policy is structured as a should statement allowing for flexibility and discretion.
9	03/31/08	Valley Partnership	NR-5	B.6	We recommend that the "shall" be rewritten as a "should" Integration of this type of amenity should be evaluated on a project-by-project basis	Change "shall" to should to align with the wording "as appropriate" recognizing differing site circumstances.
10	03/31/08	Valley Partnership	NR-5	C.1	Does this refer to compact cars/spaces? Please explain exactly what is intended by this guideline.	Reword as follows: " Utilize compact spaces to the maximum extent possible." The policy is intended to reduce the overall amount of pavement devoted to automobile usage.
11	03/31/08	Valley Partnership	NR-6	D.2	It is our understanding that the list of unifying elements is intended as a list of examples, not that the full list is required in each project. As such, we recommend rewording the language to read: "Elements such as project icons, thematic arches, special paving treatments, and mature, full sized-landscaping shall be used to unify the project."	Change as recommended.

ERRATA SHEET

12	03/31/08	Valley Partnership	NR-6	E.6	It is not practical, nor is it desirable, to have windows on all four facades of each building. Necessity, safety and aesthetics must all be considered when placing windows on facades. As such, Valley Partnership would like the opportunity to further discuss with the City a more effective and reasonable guideline regarding windows.	No change. The statement is structured as a "should" and intended to encourage "eyes on the street" where activity occurs.
13	04/18/08	Police	NR-7	B & C	Shrubs, bushes and trees should be limited in numbers. What is planted should be high above an average person's head or low to the ground to avoid concealment and allow for high visibility.	Keep as is Item addressed
14	02/28/08	JJ Company	NR-7	E.10	(...perimeter walls, which are not otherwise designed as an amenity, as described in 20-78-2.11 E)	Add additional language as requested.
15	02/28/08	JJ Company	NR-8	G.1	(add e.) Provide shade for pedestrian-vehicular uses.	Add additional language as requested
16	03/31/08	Valley Partnership	NR-8	F.4	Most commercial developments have multiple tenants, but not all require public spaces. In particular, industrial projects. Additionally, listing seat walls as a required element limits the development community's ability to meet this requirement in a creative and innovative way. We recommend that the "shall" be rewritten as a "should" and that, if the City wishes to include seat walls, it be included in a list of example options beginning with "such as".	Reword as follows" Commercial developments with multiple tenants shall provide common outdoor plaza areas or similar architecturally integrated public spaces which should include amenities such as seatwalls and enhanced or expanded walkways."
17	02/28/08	JJ Company	NR-10	I.1-3	I.1-3. Advisory-May wish to elaborate on minimum and maximum heights, length w/out undulations-columns.	Wall heights and perimeter wall undulation already specified in Zoning Ordinance. However, add new #2 to read: "Parking screen walls shall undulate a minimum of three (3) feet for every one-hundred (100) linear feet."
18	03/31/08	Valley Partnership	NR-10	I.7	Many desirable commercial projects, while sited next to other developments, may not need to seamlessly transition from one to another. Variables may include different uses, ages, materials, etc. We recommend the elimination of this guideline.	No change. Policy is structured as a should and would hinder efforts to promote continuity if eliminated.

City of Peoria
 Draft Design Review Manual
 December, 2007
ERRATA SHEET

19	02/28/08	JJ Company	NR-11	J.5	J.5. heavy-dense landscaping, dense-evergreen trees	Agreed. Change as noted. Drop "evergreen" as the prescriptive tree variety.
20	04/18/08	Police	NR-11	J	Trash Enclosures. All such areas should be enclosed as mentioned but also LOCKED. With the increase of identity theft more and more identity thieves will be targeting dumpsters for names, addresses, account information, etc. Lighting around these areas would also help deter all types of crimes.	Forward concern to Engineering for consideration as part of enclosure detail. The detail currently requires a gate. Reword E 4 on page NR-6 (CPTED) to read: "Trash enclosures shall be sited in a safe manner that avoids blind spots and/or hiding areas."
21	03/31/08	Valley Partnership	NR-11	J.5	To provide the opportunity for creative and innovative screening methods, we recommend that the "shall" be rewritten as a "should" and that, if the City wishes to include this list of elements, it be included in a list of example options beginning with "such as".	The City is interested in the screening of loading areas. However, there are various design solutions. Rewrite as follows: "Loading areas shall be screened from view utilizing design solutions including, but not limited to, decorative walls, trellis/greenscreens, berming, dense landscaping, or a combination thereof."
22	04/18/08	Police	NR-12	K	As in #2 all utility sheds should be kept locked at all times and lighted in hours of darkness. The same applies to garden supply sheds. these are prime targets for thieves.	Duty noted.
23	03/31/08	Valley Partnership	NR-12	L1-L5	Some kinds of commercial developments, industrial in particular, are not appropriate draws for pedestrians for safety reasons. Additionally, this guideline may not be reasonable for smaller projects. We recommend that this guideline be excluded for industrial projects and that this guideline only apply to others "where appropriate."	Staff acknowledges the differences between commercial and industrial business parks. Item L-1 refers specifically to "commercial developments". Rewrite L-5 to read: "In the event that bollards are warranted to separate pedestrian and/or public spaces from circulation routes, said bollards shall compliment the architectural style of the center. Corporate colors applied to the bollards are expressly discouraged."
24	03/13/08	Industry Meeting	NR-13	III.A.4	360 degree architecture. Back doesn't need to be identical to the front. Remove "equal". Consider pad buildings only. Opportunity to evaluate the need for 360 degree based on site specifics.	Replace "equal" with "proportionate"
25	02/28/08	JJ Company	NR-13	A.1	III.A.1. "Corporate chain" may wish to re-think/re-word this as it will be difficult for commercial developers/brokers to secure tenants for their shops with strong over-riding language that impacts all commercial in Peoria.	No change. Coporates templates are fine. However, treatments and localized design alternatives should vary.

City of Peoria
 Draft Design Review Manual
 December, 2007
 ERRATA SHEET

26	03/31/08	Valley Partnership	NR-13	A.1	Please provide a definition or detailed description of "corporate chain" architecture.	The City is attempting to avoid "placeless architecture" where the user is easily identified prior to occupancy.
27	03/31/08	Valley Partnership	NR-13	A.4	The term "equal" may be misunderstood to mean that the design on the front of the building should be exactly mirrored on each side and back of the building. We do not believe this is the City's intention. We recommend rewording this standard to read: "All sides of the building shall receive design consideration and treatment (360 degree architecture)."	Replace "equal" with "proportionate"
28	03/13/08	Industry Meeting	NR-15	D	Add pictures to illustrate modest design examples of façade articulation. Add text to support smaller scale projects.	No change. The City is attempting influence design solutions through positive examples. Additionally, D.3 provides flexibility.
29	03/31/08	Valley Partnership	NR-16	E.1	We agree that varied rooflines can be an attractive design element. Delineating the specific intervals for all commercial projects, however, is too prescriptive and limits the development community's ability to meet this requirement in a creative and innovative way. We recommend the 50 foot requirement be eliminated.	Agreed. Delete sentence "Rooflines shall be.....stepbacks."
30	03/13/08	Industry Meeting	NR-16	E.1	Roof planes varying at 50' intervals will create a "pronounced" look. Create ways to break the plane without being overly prescriptive.	Agreed. Delete sentence "Rooflines shall be.....stepbacks."
31	03/31/08	Valley Partnership	NR-16	E.4	A 7' parapet may not be tall enough to adequately screen roof equipment from a second story window view on an adjacent home or commercial building. As such, we recommend that the language be reworded to read: "Parapets shall not exceed 7 feet in height or the height necessary to adequately screen roof equipment from view by an adjacent second story view, whichever is greater."	Remove E.4 in deference to operational guidance from the Fire department.
32	03/31/08	Valley Partnership	NR-17	G.1	Please explain which kind of natural materials the City seeking. Natural stone, etc. can be extremely cost prohibitive and, at 25% of the visible facades, would make most projects impossible to fund. Will the City accept veneers and similar design/construction elements?	No single type sought. Strike the language requiring 25% - too prescriptive.

City of Peoria
 Draft Design Review Manual
 December, 2007
ERRATA SHEET

33	03/31/08	Valley Partnership	NR-18	A 1	Two of these elements seems to be excessive, especially for smaller project that falls into this size category. Valley Partnership would like the opportunity to further discuss with the City the size trigger point for this requirement	III A.1 revised to require 3 of the 6 elements: Item 1.a has been rewritten as follows: "Provide freestanding architectural features that are thematically consistent. Such features may include, but are not limited to....."
34	03/31/08	Valley Partnership	NR-18	A.1.d	Installation of artwork may not be desirable or feasible, especially smaller project that falls into this size category. To achieve the overall goal of integrating artistic elements into commercial projects, we recommend the following language (a similar version was recently adopted by a Valley municipality): "Where appropriate, locate artistic elements in open public locations. Artistic elements must be consistent with the overall theme of the center/facility. Artistic elements may include sculptures and murals, gates, fences, walls, benches, fountains and walkways, etc."	Agreed. Reword A.1.d as suggested.
35	03/13/08	Industry Meeting	NR-18	G.5	"Rich" color in context of surrounding environment/landscape. Eliminate the term "bold".	Reword as follows" As the context dictates, a rich and/or bold color palette shall be employed to create a sense of variety and interest to exterior elevations "
36	02/28/08	JJ Company	NR-19	B.1.b	B.1.b. Landscape triangle.....or other architectural features, (add this) but do not impede sight distance requirements for vehicular traffic safety.	No change. Architectural features can go in the landscaped triangle. The City is attempting to "soften" and provide identity to such corners. All corner elements must meet existing site visibility requirements.
37	02/28/08	JJ Company	NR-19	B.1.c	B.1.c (.....no direct access to any parking spaces.) This prevents any TND or Main Street types of development from occurring, really what was intended?	The objective was to avoid direct and immediate conflicts from busy primary entry points mixed with cars backing up. Reword last sentence to read: "..... and shall provide no direct access to any parking spaces within 100 feet of the entry." Mixed use templates like Kierland Commons and Park West have parking areas some distance from the entry drive.
38	03/31/08	Valley Partnership	NR-19	A.1.f	Please explain what the City intends by "unique."	Strike word "unique"

ERRATA SHEET

39	03/31/08	Valley Partnership	NR-19	B.1.b	It seems that this requirement might be in conflict with #4 under Site Design, which strongly encourages clusters of buildings at the street edge. Additionally, this traditional type of entryway landscaping may not be in line with the City and the development community's common goals for responsible water use and maximizing the value of usable land. Valley Partnership would like the opportunity to further discuss with the City other options to meet the City's vision for primary entrances on large scale retail projects.	No change. The city is attempting to soften and provide identity to such primary themed entry into a site. The landscaping may certainly be Xenscape. Additionally, there are opportunities outside of this entry to maximize the "curb appeal" through building clustering near the street edge.
40	03/31/08	Valley Partnership	NR-20	B.1.g	Please define "parking area," from which a walkway must be directly accessible.	Delete last sentence "At least one...."
41	02/28/08	JJ Company	NR-21	B.1.j	j. Public Space – no mention of segregating smoking areas?	No change. Proprietor decision subject to restrictions contained under State Law.
42	02/28/08	JJ Company	NR-24	E.4/5	E 4. typo-no Figure 2.68	Change 4 reference to be "Figure 2.65" and change 5 reference to be "Figure 2.66."
43	03/13/08	Industry Meeting	NR-24	E 6	Think through corner commercial / drive-through	No change. The City is attempting to upgrade the aesthetic quality of the streetscape by emphasizing the location of well-designed buildings and relegating drive-through lanes and parking areas to a secondary role. With that said, the policy is structured as a "should" recognizing varying site circumstances.
44	03/13/08	Industry Meeting	NR-19	B.1.b	Large scale retail – primary entry-change to "should".	Change to 'should' for amenity listing.
45	03/13/08	Industry Meeting	General		Trending away from over-done elevations and tower elements in exchange for covered walkways and pedestrian-friendly elements.	Noted. New photo to be taken.
SINGLE-FAMILY - CHAPTER 3						

City of Pe
 Draft Design Review Manual
 December, 2007
ERRATA SHEET

46	03/24/08	Pulte	SF-1	A.1	We propose that vorbage be added to include the sentence: "Single-Family detached lots that are 70' wide and larger shall be exempt." It is our belief that homes on lots of this size already portray the necessary attributes for a diverse streetscape.	Staff disagrees. Streetscape is as much about architecture as it is variety in lot width.
47	10/13/05	DR Horton	SF-3	Table 2	The heading reads, "Proposed Number of Single-Family Units" what will this number be based on? Per plat? Per subdivision? Per neighborhood? Per phase? We should clarify.	No change. The classification relates to the the number of lots per Plat as Identified.
48	03/28/08	Standard Pacific	SF-3	A.1.b	Figure 3.5 could use some clarification as to which lot angles should be 45 degrees or greater or if there is a minimum distance from the rear corner to the lot line.	No change. The policy is intended to discourage pie-shaped lots that are aligned with corners in an effort to provide more "usable" area.
49	03/04/08	HBACA	SF-3	A.1.e	Through lots/double frontage lots are <i>prohibited</i>	Change as requested.
50	04/18/08	Police	SF-4/5		No specific recommendations, but always apply CPTED principles of High visibility, Lighting especially in public areas	Noted. Policy 2.f does point to open space and recreational areas to be located so that they are visible from nearby homes.
51	03/28/08	Standard Pacific	SF-4	A.2.a	Change text. "The front door should be clearly visible from the street."	Delete provision.
52	03/24/08	Pulte	SF-4	A.2.a	We propose that this line be removed, because the "front door" is seldom ever used by the resident (and occasionally used only by a guest). We question the issue of Crime Prevention in order to require that it "be clearly visible from the driveway". If it must remain, then we are concerned that the definition of the "front door" will be mis-interpreted down the road. We prefer that it read: "front door or gate or courtyard opening".	Delete provision.
53	03/24/08	Pulte	SF-4	A.2.b	Despite the fact that the word "should" is used in the line: "Windows on all sides of the house should provide full visibility of the property", we are concerned that the interpretation later will include adding windows that invade on the privacy of the neighboring lots.	Reword as follows: "To the extent feasible, windows, doors or other openings should be provided on all sides of a façade to create visual interest and prevent dead spaces."

City of Peoria
 Draft Design Review Manual
 December, 2007
 ERRATA SHEET

54	03/04/08	HBACA	SF-4	A 3	#3. "...setback provisions of this ordinance." This is key to flag lot design.	No change This same provision is in the Zoning Ordinance for Single-Family Districts. It actually provides flexibility for the determination of setbacks for such irregularly shaped lots.
55	03/04/08	HBACA	SF-4	A 4	#4. By what means?	Delete provision. This provision does not relate to lot and tract design and is relevant to operational standards for fire and police response.
56	05/06/08	Traffic Engineering	SF-5	A 3.a	All local residential streets exceeding 1000 feet in length shall be curvilinear in design unless proactively incorporate traffic calming measures are utilized.	Reword as follows: "All local residential streets exceeding 1000 feet in length shall be curvilinear in design unless traffic calming measures are proactively utilized "
57	05/06/08	Traffic Engineering	SF-5	A 3.b	All collector and non-residential collector streets exceeding 1000 feet in length shall proactively incorporate traffic calming measures. Examples of traffic calming devicee measures suitable for local streets include, but are not limited to; such as roundabouts, traffic circles, landscape islands or medians, chicanes, chokers, raised intersections and/or crosswalks, or other approved measures. (collector streets or lower classification) shall be incorporated proactively in neighborhoods with streets of 1000 feet in length or longer with uninterrupted block lengths of 600 feet or greater.	Reword as proposed.
58	03/04/08	HBACA	SF-5	A 3 b	May include speed bumps	See revised language above.
59	05/06/08	Traffic Engineering	SF-5	A.3 c	To minimize potential conflict points, four-way street intersections at local/local intersections are discouraged.	Reword as proposed.
60	02/28/08	JJ Company	SF-5	A 3 c	3.c. This prevents TND design and 4 way intersections are preferred by insurance comapanies and traffic engineers. It is only an Arizona trend to have three way interseactions everywhere in residential neighborhoods.	See above response. The City of Peoria Engineering Department discourages four-way intersections as a general standard.

City of Peoria
 Draft Design Review Manual
 December, 2007
ERRATA SHEET

61	02/28/08	JJ Company	SF-5	3 d	3 d (...cul-de-sacs and knuckles to reduce...)	Delete provision. The Development Engineering Department has recommended the deletion of this provision.
62	05/06/08	Traffic Engineering	SF-5	A 3 d	Developments should consider the use of landscaped islands and/or parking nodes into the design of cul-de-sacs to reduce pavement coverage, enhance neighborhoods, and provide functional space. The islands shall be designed to meet all required turning radii for fire access and refuse collection (Figure 3-12). Pedestrian connections should be established throughout the community to provide connectivity and to reduce walking distances, especially at the ends of cul-de-sacs and along routes to destinations such as parks, schools and services.	Change as proposed.
63	05/06/08	Traffic Engineering	SF-5	Figure 3.12	Note: Remove Figure 3.12.	Switch out photo for Figure 3.12 to address new language.
64	05/06/08	Traffic Engineering	SF-6	A 4 d	Detached sidewalks shall be provided adjacent to all arterial roadways and some collector roadways in accordance with the Infrastructure Guidelines Detached sidewalks should be separated from the roadway by a minimum of 4 feet.	Change A 4 d as proposed.
65	05/06/08	Traffic Engineering	SF-6	A 4 d	Note: Second part of d. is now the new e. The Sidewalks along arterial roadways should may provide for have a gentle meander to elevate the for aesthetic and pedestrian quality. The sidewalk shall return to the back of curb or right-of-way line no less than 20' from any intersection to minimize the potential for pedestrian - /automobile conflicts (Figure 3.16)	Add as new A.4.e - A.4.d was broken into two separate policies.
66	03/04/08	HBACA	SF-6	A 4 c	Fig 3.15 only when design allows	Delete second sentence. The intent is to soften or open up cul-de-sac areas when not fronting lots or fire access.
67	03/25/08	CVLCI	SF-7	A 4 e	The 4 foot offset for sidewalks on local streets will push the sidewalk into the right-of-way which will push forward facing garages back. Will water meters, fire hydrants, street lights etc. be installed between the curb and sidewalk? If not, they will fall within the PUE and possibly conflict with other utilities	As a 'should' statement, this item provides flexibility to work around PUEs and ROW issues. See traffic engineering response below (#69)

City of Poona
 Draft Design Review Manual
 December, 2007
ERRATA SHEET

68	03/04/08	HBACA	SF-7	A 4 e	E. front setback will need to be measured from back of curb	No change. Setback is measured from the property line. When provided on local streets, the intent is for the sidewalk to be within the ROW or access tract
69	05/06/08	Traffic Engineering	SF-7	A 4 e	Local street sidewalks should be offset from public or private streets by a minimum of 4 feet (Figure 3.17). Detached sidewalks should be provided adjacent to local roadways. Detached sidewalks should be separated from the roadway by a minimum of 4 feet.	Change as proposed.
70	05/06/08	Traffic Engineering	SF-7	A 4	To the extent practical, all utilities and ground mounted mechanical equipment shall be vaulted or fully screened from view through landscaping placement and/or color treatment. Note: No changes, except this is now g. rather than f.	Noted
71	03/04/08	HBACA	SF-8	A 5 c	*...or continue existing or approved open space areas * -Too open ended	Staff disagrees The City is looking to incorporate safe pedestrian connectivity between communities that reduces the need for automobile use and pedestrian activity along major streets.
72	02/28/08	JJ Company	SF-8	B.1	Walls and Fences (may wish to add min/max heights, % materials, color)	No change. Wall heights are governed by the Zoning Ordinance. Staff has not placed a percentage value on color or material usage to avoid an overly prescriptive policy and retain flexibility.
73	03/04/08	HBACA	SF-9	B.1.c.2	Incorporation of raised planters is extreme and allows for climbing and access into private yards	Noted. Reword as follows: "Incorporation of raised planters containing hostile vegetation or minimal heights to discourage unwanted access to private yards."
74	03/04/08	HBACA	SF-9	Figure 3.23	Fig. 3.23 See example of trash (cups) in photo	Noted. New photo to be taken.
75	03/28/08	Pacific	SF-10	B 2 a	The use of only backlit decorative lighting for walls and signage is too limiting There should be more flexibility	Change "shall" to "should".

ERRATA SHEET

76	03/28/08	Pulte	SF-10	B.2.a	There are many ways to provide entry feature lighting other than back-lit and the developer may wish to incorporate another lighting system in order to achieve the desired theme or design concept. Perhaps the statement could read "primary themed entry features should incorporate decorative lighting to enhance walls and signage. Possible lighting options could include back-lit lettering or uplighting."	See above.
77	03/28/08	Pulte	SF-10	B.3	The City of Peoria Engineering Standard Detail PE-119 specifies the type of railing required by Peoria at drainage crossings. Perhaps the word "should" should be utilized in this statement rather than "shall" so that there is not a conflict with the existing Peoria detail. By using the word "shall" the design guidelines conflict with the engineering detail because a decorative railing would be required in all cases and the standard detail would not be able to be used.	Keep as shall. Engineering has the ability to change details as needed.
78	10/13/05	DR Horton	SF-11	C.1.b	We should reword so that we will be allowed to build the same model or standard plan on adjacent lots just as long as they are different elevations. For example, we should be able to build a 1037 plan, elevation A next to a 1037 plan, elevation B.	Language revised to read as follows: "No more than 2 consecutive lots (adjacent side lot lines) shall share the same standard plan and elevation."
79	10/13/05	DR Horton	SF-11	C.1.e	Too restrictive. The attached PDF example shows a footprint of a plan that has a covered porch pulled forward of the garage plane, the livable area pulled forward of the garage plane (two features that you encourage), but it does not meet this requirement.	Special circumstances or product types can be considered on a case-by-case basis through a DR waiver.
80	03/28/08	Standard Pacific	SF-11	B.4b	Change and add verbiage "...structures should be placed along side yard lot lines or open space tracts..." If mailboxes are placed in open space areas they would not necessarily be along a side yard lot line.	Change as noted. Remove "utilized" and "special circumstances".

City of Peoria
 Draft Design Review Manual
 December, 2007
 ERRATA SHEET

81	03/28/08	Pulte	SF-11	B.4b	Although every effort is made to place clustered mailboxes in locations that do not conflict with SF driveways or collector street intersections, in some cases limited flexibility is afforded in determining the location of clustered mailboxes. In communities with smaller lot sizes it may not be possible to place a mailbox structure along side lot lines and still meet the requirement to be 100' away from a collector street intersection. Perhaps item b could read, "When clustered mailboxes are utilized, the structures should be placed along side- lot lines when possible or a minimum of 100' from a collector or arterial intersection."	See above response. (#79)
82	03/24/08	Pulte	SF-11	C.1.a	Architectural Form – Design Profile: We propose that verbage be added in (3.II.C.1.a) to include the sentence: "Adding a 4th elevation for each standard plan shall allow for one less standard plan to be required under the two columns in Table 2 labeled (100-199 units) and (+200 units).". In order to achieve a diverse streetscape, sometimes adding additional non-built model homes to the product offering will not always work. We have found that most times consumers will buy only what they can see and feel. But adding a more elevations will more likely provide the diversity we all desire.	No change. Discuss intent of diversity in materials and elevations.
83	03/28/08	Pulte	SF-11	C.1.e	This statement could be construed to mean that undulation greater than six feet is prohibited.	No change. Minimum undulations are prescribed.
84	03/04/08	HBACA	SF-11	Fig 3-30	Instead of a <i>multitude</i> ... should read <i>various</i>	Change as noted.
85	03/28/08	Standard Pacific	SF-11 & 12	C.1.e	Figure 3.31 reflects more of a staggered setback exhibit. Second floor areas should count as part of the required changes in plane and should be shown in the Plan View Exhibit (Figure 3.31)	... plan view exhibit of footprint and upper stories. Change as requested.
86	03/28/08	Standard Pacific	SF-12	C.1.f.1	Is a maximum undulation really necessary to be prescribed? Is it assumed that the stagger is measured from the furthestmost projection to the front?	See below (#87) re elimination of setback exhibit.

City of Peoria
 Draft Design Review Manual
 December, 2007
 ERRATA SHEET

87	10/13/05	DR Horton	SF-12	C.1.f.3	What about allowing non-garage elements (covered porch, livable portions of the home, etc.) to encroach into the front building setback? What about allowing encroachment for side-entry garages?	Addressed by zoning ordinance or PAD. Items F.1-4 deleted - no more setback exhibit
88	03/28/08	Standard Pacific	SF-12	C.2.a	Change text. "All developments should integrate a variety of exterior accent materials as appropriate with the architectural style of the home. The use of stone, brick, masonry etc. is strongly encouraged.	Change as requested
89	10/13/05	DR Horton	SF-12	C.2.a	Still too subjective – "substantial" and "balanced"?	Refer to the use of the phrasing "in appropriate quantities" as a means to consider specific product types and circumstances.
90	10/13/05	DR Horton	SF-11	C.1.d	Too subjective. Are these elements required on every elevation of every plan? I assume we are referring to front elevations?	The policy requires integration of architectural embellishments to avoid flat facades and create shadow lines
91	03/24/08	Pulte	SF-12	C.2.a	Architectural form – Materials and colors: Change the word, shall, to instead read: "should" integrate multiple exterior materials including brick, stone and masonry..." We see stone and brick accents as a trend that will evolve into something different down the road. So keeping the guidelines flexible is our preference.	Change as requested.
92	03/24/08	Pulte	SF-13	C.2.e	Architectural Form – Materials and Colors: We propose to add the verbage, "Exposed flues (B-vent) are acceptable only with fireplaces without a chimney."	Change as requested.
93	03/25/08	CVLCI	SF-13	C.3.a	If the concern is cars blocking a driveway, can the note be changed to read "Driveways shall be located a minimum of 150'....."	No change. Intent is not to prevent blocking driveways. Intent is to prevent longer vehicles parked in driveways from blocking sidewalks.
94	10/13/05	DR Horton	SF-13	C.3.b	I want to confirm that using a salt finish in lieu of the standard "broom" finish will meet this requirement	Confirmed.

ERRATA SHEET

95	10/13/05	DR Horton	SF-13	C.3.c	Why all visible elevations? This requirement should only apply to the forward facing portion of a side-entry garage. The elevation adjacent to the side property line on a side-entry garage should not have more requirements than the same elevation on a forward-facing garage.	Reword as follows" The forward facing (from street view) elevations of a side entry garage shall appear as livable area by utilizing windows....."
96	02/28/08	JJ Company	SF-14	C.3.d	May think about limiting (% basis) the amount of the front elevation that a garage may comprise.	Noted.
97	03/28/08	Standard Pacific	SF-14	C.3.d	Change text. "Front-facing garages should be offset no less than four (4) feet from the plane of a living area..." This section also implies that the garage should be behind the livable area to "soften garage dominance". Consideration should be given for a percentage of garage dominant plans to enhance the diversity of the community.	No change. Offset refers to change in plane.
98	02/28/08	JJ Company	SF-14	D.1.a	typo exceeding 8 nine (9) feet	Change as requested.
99	03/28/08	Standard Pacific	SF-14	D.1.a	Typo-delete 8. "Accessory buildings exceeding 8 nine (9)..."	Change as requested.
100	10/13/05	DR Horton	SF-14		Insert "or covered porch plane" after "...living area plane"	Change as requested.
101	03/28/08	Standard Pacific	SF-15	D.3	Title: Terminology – "Covered Patios" is more accurate than "Patio Covers" as it is the covered patio as a whole not just the cover that is under consideration.	Change as requested.
102	03/28/08	Standard Pacific	SF-15	D.3.a	"Covered patios shall be compatible..."	Change as requested.
103	03/28/08	Standard Pacific	SF-15	D.3.b	"Covered patios shall be constructed on an approved concrete..." (Patio covers are constructed on posts or columns.)	Change as requested.

ERRATA SHEET

104	03/28/08	Standard Pacific	SF-16	D.5	Title: Is the intention of this section for Detached RV Covers or Garages? A cover implies an open "carport". Is a detached RV (enclosed) Garage not allowed?	Written as intended. Both types of structures have been witnessed and need to be addressed.
105	03/04/08	HBACA	SF-16	D.5 e	#5E. Change to read "The structure shall not exceed 20' – 22' in overall height and no greater than 800 square feet in area."	Remove item. Addressed by zoning ordinance.
106	03/28/08	Standard Pacific	SF-15	D.5 e	A maximum 16 foot height limit for Detached RV garage may preclude the design of a pitched roof depending on the size and shape of the garage, which may be more integral with the design of the main structure.	Remove item. Addressed by zoning ordinance.
107	10/13/05	DR Horton	Table 2	Roof Colors	Four or five different roof colors is excessive. Usually, three is plenty, even with 200+ plus lots, especially if we are required to have two different roof styles and at least two different roof materials.	This existing requirement has been satisfied since its inception. Again, the purpose is to avoid the "sea of red tile roofs" and encourage diversity.
108	10/13/05	DR Horton	Table 2	Roof Material Types	Three different roof materials is excessive. Using two, flat or mission, is acceptable and provides diversity, but beyond that, we would not mix asphalt shingles with concrete tile, and going to a clay tile would be cost prohibitive.	No change to existing requirement of <u>two</u> roof material types. Above comment indicates 3 varieties is acceptable when dealing with color. The same should hold true with materials.
MULTI-FAMILY - CHAPTER 4						
109	05/22/08	Staff	MF-2	A.1	Add new policy to address the interface between multi-family residential and single-family residential.	Add new policy A.1.e to read "The transition between multi-family residential and existing single-family residential areas shall be enhanced to achieve maximum compatibility through design solutions including but not limited to, building orientation, use of clerestory windows for upper stories, wall height and landscape buffering."
110	02/28/08	JJ Company	MF-3	2.b	2.b. may wish to rethink and provide vehicular connections as well to prevent traffic from having to enter onto streets just to go to an adjacent development if possible	Change policy from "shall" to "should" to address resident desires and specific site circumstances.
111	02/28/08	JJ Company	MF-5	4 a.7	4 a.7. Other feature (s)	Change as proposed.

ERRATA SHEET

112	02/28/08	JJ Company	MF-11	b	b. Cluster mailboxes should be screened or designed in a manner consistent...)	Change as proposed.
113	02/28/08	JJ Company	MF-13	B.1.d	B.1.d. may wish to address TND or new urbanism designs where building massing is desired in excess of two stories along the street corridor.	Change from "shall" to "should" to deal with TND and other applications where such massing may be desired.
SUPPLEMENTARY STANDARDS - CHAPTER 5						
114	02/28/08	JJ Company	SS-6	C.1.1	C.1.1. may wish to limit front facing setbacks for these outdoor spaces otherwise streetscape appearance may suffer.	Noted. The Zoning Ordinance already contains provisions (General Provisions Section) to limit outdoor sidewalk sales.
115	02/28/08	JJ Company	SS-8	3.a-b	3.a-b. no mention to on-street parking which is typically associated with historical downtown streets or the re-introduction of historical appearances.	The provisions are intended to address on-site parking if provided.
116	02/28/08	JJ Company	SS-10	6.d	6.d. tree canopies should not be designed to interfere with pedestrian-parking movements either.	Reword as follows" Tree canopies should allow sufficient room for growth without impacting building elements or impeding pedestrian and other modes of circulation."
117	02/28/08	JJ Company	SS-10	8	8. may think about adding a signage section or referencing a signage section here	No change. See pages SS-10/11 9.a-c in regards to signage.
118	03/28/08	Pulte	SS-36	A (intent)	Is this statement intended to preclude the placement of cabinets on collector roads or arterials? In many cases this is not possible as cabinets for feeder infrastructure lines must be placed on collector or arterial streets. Every effort is made to minimize the visual impact of utility cabinets via placement, landscape shielding, etc. however the flexibility to relocate the cabinets is limited by running line length, service locations, etc. as determined by the utility provider.	It is not intended to preclude but rather guide the placement of such facilities away from intersections and other visible locations.

City of Peoria
 Draft Design Review Manual
 December, 2007
ERRATA SHEET

119	03/28/08	Standard Pacific	SS-37	B.2.a	"The (utility) cabinet should be located in PUE's in an area contained with the side/rear yard landscape tracts " Although preferable, this is not always achievable.	This will change. The main intent is to address cabinets at the primary entry into a subdivision.
					MISCELLANEOUS	
120	03/31/08	Valley Partnership	N/A	N/A	Overall, our members found the format easy to read and to follow.	Duly noted.
121		Staff	NR-20/21		Order skips from f to i	Reorder a-h
122		Staff	TITLE	Title	Change title of document	Document should be titled "Principles and Guidelines" to match up with Chapter 20 of the City Code.

ORDINANCE NO. 08-22

AN ORDINANCE OF THE MAYOR AND COUNCIL OF THE CITY OF PEORIA, MARICOPA COUNTY, ARIZONA, AMENDING CHAPTER 20 OF THE PEORIA CITY CODE (1977 EDITION), BY INSERTING SECTION 20-78, PROVIDING FOR A NEW DESIGN REVIEW MANUAL ENTITLED "DESIGN PRINCIPLES AND GUIDELINES" AS IDENTIFIED THEREIN; PROVIDING FOR SEPARABILITY AND PROVIDING FOR AN EFFECTIVE DATE.

WHEREAS, the Design Standards Advisory Board of the City of Peoria, Maricopa County, Arizona, held a public hearing on June 11, 2008 to consider comprehensive amendments to the City's Design Review Manual, as promulgated in Section 20-70 of Chapter 20 of the Peoria City Code, after notice in the manner provided by law; and

WHEREAS, due and proper notice of such Public Hearing was given in the time, form, substance, and manner provided by law including publication of such in the Peoria Times on May 23, 2008; and

WHEREAS, the Design Standards Advisory Board of the City of Peoria, Arizona at its regularly convened meeting of June 11, 2008, voted to recommend to the Mayor and Council of the City of Peoria, Arizona, that amendments be made to Section 20-70 of Chapter 20 of the Peoria City Code (1977 edition); and

WHEREAS, the Mayor and Council of the City of Peoria, Arizona, have considered the recommendation of the Design Standards Advisory Board of the City of Peoria, Arizona, and deem it to be in the best interest of the public health, safety and welfare of the residents of the City of Peoria, Arizona; and

NOW, THEREFORE, BE IT ORDAINED by the Mayor and Council of the City of Peoria, Arizona as follows:

SECTION 1. The Design Review Manual as promulgated in Section 20-70 of Chapter 20 of the Peoria City Code (1977 edition) be amended as referenced in Exhibit A ("Design Principles and Guidelines, July 2008").

SECTION 2. Effective Date. This Ordinance shall become effective on the date provided by law.

SECTION 3. SEPARABILITY. If any section, subsection, sentence, clause, phrase or portion of this Ordinance is for any reason held invalid or unconstitutional by any court of competent jurisdiction, such decision shall not affect the validity of the remaining portions of this Ordinance.

PASSED AND ADOPTED by the Mayor and Council of the City of Peoria, Maricopa County, Arizona this 1st day of July, 2008.

Bob Barrett, Mayor

Date Signed

ATTEST:

Mary Jo Kief, City Clerk

APPROVED AS TO FORM:

Stephen M. Kemp, City Attorney

Published in: Peoria Times Publication Dates: July 4 and 11, 2008

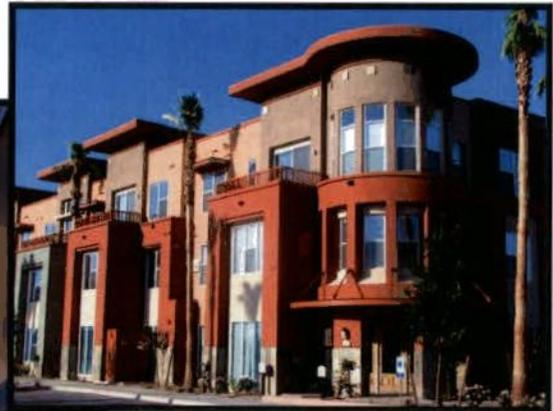
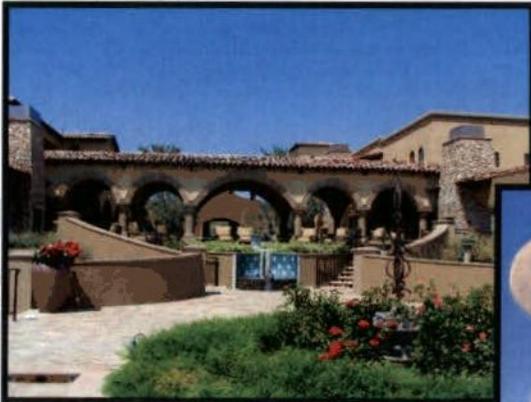
Effective Date: _____



DESIGN REVIEW MANUAL

July 2008

DESIGN PRINCIPLES AND GUIDELINES



Includes Design Principles and Guidelines for:

- Non-Residential
- Single-Family Residential
- Multi-Family Residential
- Supplementary Standards
 - Old Town
 - Lake Pleasant Parkway
 - Utility Cabinets

CITY COUNCIL
Bob Barrett, Mayor

Dave Pearson	Ron Aames
Cathy Carlat	Joan Evans
Vicki Hunt	Carlo Leone

CITY MANAGER
Terrence L. Ellis

DEPUTY CITY MANAGERS

John Wenderski
Carl Swenson
Susan Thorpe



DESIGN REVIEW TEAM

Glen Van Nimwegen, AICP, Community Development Director
Chris M. Jacques, AICP, Project Manager - Principal Planner
Chad Daines, AICP, Former Planning Manager
Rob Gubser, AICP, Senior Planner
Adam Pruett, Senior Planner
Cody Gleason, Planning Technician
Melissa Zechiel, Planning Technician
Bev Parcels, Planning Assistant

Table of Contents

Chapter 1: Administration

- I. Introduction
 - A. Intent and Use of the Design Manual
 - B. Interpretations
 - C. Use of Illustrations and Photos
 - D. Organization and Content
 - E. Administration of Design Manual
 - F. Glossary

Chapter 2: Non-Residential

- I. Intent
 - A. Applicability
 - B. Exemptions
- II. Site Design
 - A. Site Layout and Orientation
 - B. Access and Circulation
 - C. Parking Areas
 - D. Project Entry and Character
 - E. Crime Prevention Through Environmental Design (CPTED)
 - F. Pedestrian Amenities / Public Space
 - G. Landscaping
 - H. Lighting
 - I. Walls and Fences
 - J. Loading Areas and Trash Enclosures
 - K. Utilities and Mechanical Equipment
 - L. Other Thematic Elements

Table of Contents

III. Architectural Form

- A. Design Theme
- B. Building Mass
- C. Fenestration / Wall Penetrations
- D. Façade Articulation
- E. Roof Planes
- F. Storefront Proportion
- G. Colors and Materials

IV. Specific Development Criteria

- A. Shopping Centers, Office, Commercial Centers over 50,000 sf or 5 net acres
- B. Large Scale Retail
- C. Industrial
- D. Gasoline Service Stations and Car Washes
- E. Drive-Through / Drive-Up Facilities

Chapter 3: Single-Family

I. Intent

- A. Applicability
- B. Exemptions

II. Single-Family Residential Design Standards

- A. Subdivision Design
- B. Thematic Form
- C. Architectural Form
- D. Existing Development

Table of Contents

Chapter 4: Multi-Family

- I. Intent
 - A. Applicability
- II. Multi-Family Residential Design Standards
 - A. Site Design
 - B. Architectural Form

Chapter 5: Supplemental Standards

- I. Intent
- II. Old Town
 - A. Introduction
 - B. Applicability
 - C. Site Design
 - D. Architectural Form
 - E. Colors/Materials
- III. Lake Pleasant Parkway
 - A. Introduction
 - B. Applicability
 - C. Existing Context
 - D. Design Standards
 - E. Character Areas Design Concepts and Guidelines
- IV. Utility Cabinet Screening
 - A. Intent
 - B. Standards

Chapter 1: Administration



I. Introduction (20-78-1.I)

A. Intent and Use of the Design Manual (20-78-1.I.A)

The goal of this document is to provide clear and instructive principles and guidelines for the design and review of commercial, industrial, and residential development in Peoria. The principles and guidelines are intended as a baseline for the minimum qualitative design expectations in Peoria. In addition to forging a distinct community identity, the Peoria Design Review process is intended to promote:

- Diversity in development;
- Distinctive and safe development templates;
- Increased pedestrian and multi-modal connectivity;
- Preservation and retention of environmentally sensitive areas;
- Sustainable natural and built environments;
- Enhancement of the value of property; and
- Protection of the overall health, safety, and welfare of the community.

The development community is urged to consult and apply these principles and guidelines at the onset of the process to assure that the review and permitting processes are as efficient as possible. It should also be noted that these guidelines are a minimum starting point for approaching design consensus. Furthermore, it should be recognized that these principles and guidelines do not encompass every possible technique for achieving a high level of design quality. The development team is

encouraged to use its own creativity and experience to improve upon the means for realizing the design goals and expectations.

The Design Principles and Guidelines may be interpreted with some flexibility in the application to specific projects as not all design criteria may be appropriate for each project. In some circumstances, one guideline may be relaxed in order to accomplish another, more important, guideline. The overall objective is to ensure that the intent and spirit of the Design Principles and Guidelines are followed and to attain the best possible design within reason.

B. Interpretation (20-78-1.I.B)

Guidelines which employ the word 'should' are intended to be applied as stated. However, an alternative measure may be considered if it meets or exceeds the intent of the guideline.

Guidelines using the words 'shall' or 'must' are mandatory and must be included in the project's design.

Guidelines using the words 'encouraged' or 'discouraged' or 'promote' are desirable but not mandatory.

C. Use of Illustrations and Photos (20-78-1.I.C)

The images used to illustrate the compliance (or non-compliance) with the standards should be reviewed in context to the specific guideline with which the image is associated and not necessarily with all of the principles within the document. For example, an image used to highlight an encouraged form of building articulation may also contain a site condition that is not encouraged. The intent is for the reader to focus on the operable portions of the photo highlighted with the caption or reference in the guideline. In addition, although an image may illustrate a portion of the guideline with which it is associated, the intent of the entire guideline should be met. The illustrations and photos depict examples of implementation options. In any event, please consult the context and description supplied with each image.

D. Organization and Content (20-78-1.I.D)

The Peoria Design Principles and Guidelines identify critical components of a comprehensive and integrative design policy which provide a framework within which each development project must operate. The Design Guidelines are organized into five chapters. The following is a brief synopsis of each chapter.

1. Chapter 1: Introduction

This Chapter explains the overarching goals of the Design Principles and Guidelines. Additionally, this chapter outlines the process and administration of the guidelines and follows with a Glossary of operable terminology used frequently throughout the document.

2. Chapter 2: Non-Residential

This Chapter addresses elements of site and architectural design in relation to commercial, industrial, and other non-residential types of development. This chapter includes specific provisions for large scale retail, drive-through establishments and other use types.

3. Chapter 3: Single-Family Residential

This Chapter addresses elements of lot layout, subdivision design, and architectural character in relation to detached and attached Single-Family Residential development.

4. Chapter 4: Multi-Family Residential

This Chapter addresses elements of site design and architectural character in relation to all types of multi-family residential development, regardless of lease or ownership structure.

5. Chapter 5: Special Standards

This Chapter addresses overlay standards for specific study areas such as Old Town and the Lake Pleasant Parkway Corridor. It is intended that this section work in conjunction with the specific type of development that is being proposed in the study area. For example, a commercial development along Lake Pleasant Parkway would consult this section for specific guidance and Chapter 2 pertaining to Non-Residential Development. In the event of a conflict between Chapter 5 and any other section, Chapter 5 would prevail.

E. Administration of Design Manual (20-78-1.I.E)

1. Establishing the Design Standards Advisory Board

Board Structure and Purview

- a. The Design Standards Advisory Board is established pursuant to City Code, Chapter 14, Section 20-76 and further referenced in Section 14-37-6 of the Peoria Zoning Ordinance.
- b. The Board shall consist of a total of five (5) members appointed by the Mayor with the approval by the City Council.

- c. All meetings shall be noticed as required and set forth in the City of Peoria City Code.
- d. The Board functions as an advisory body concerning updates and amendments to the Peoria Design Principles and Guidelines and other duties as described therein (Section 20-77 of the City Code). All amendments to the Design Review Principles and Guidelines will be drafted by staff and presented to the Design Standards Advisory Board. The Board will then recommend to the City Council approval, approval with changes or denial of the proposed amendment.

2. Establishing the Design Review Appeals Board

Board Structure and Purview

- a. The Design Review Appeals Board is established pursuant to City Code, Chapter 14, Section 20-75 and further referenced in Section 14-37-7 of the Peoria Zoning Ordinance.
- b. The Board shall consist of a total of five (5) members plus one (1) alternate appointed by the Mayor with the approval by the City Council. The Design Review Appeals Board shall serve as the primacy body on design review matters and appeals and shall include the following composition:
 - 1. One (1) member shall be a registered Architect;
 - 2. One (1) member shall be a registered Landscape Architect;
 - 3. One (1) member shall be a registered Professional Engineer;
 - 4. One (1) member shall have a background or experience in planning or urban design;
 - 5. One (1) private citizen who is a resident of the City; and
 - 6. One alternate member meeting any of the qualifications contained above.
- c. All Design Review Appeals Board members shall reside or maintain an office in the City of Peoria.
- d. All meetings shall be noticed as required and set forth in the City of Peoria City Code.

- e. The Design Review Appeals Board is authorized to hear and render final decisions on any appeals to a final staff decision on a design review submittal or cases experiencing limited progression and/or a design impasse.

3. Development Review Process

Each Chapter in the Peoria Design Principles and Guidelines will open with a section on the thresholds and applicability. The design principles are in turn administered through staff review of various case types including Preliminary Plats, Site Plan Review, Conditional Use Permits, Design Review, and others.

a. Non-Residential

1. Refer to Chapter 2 and 5 applicability.
2. Chapter 2 site design and architectural design principles administered through review of Site Plan (Administrative).
3. In those cases where the 3rd submittal of the Site Plan has been provided and substantial progression has not been demonstrated or a design impasse has been reached as determined by the Planning Manager, such cases shall be forwarded to the Peoria Design Review Appeals Board for final action. The scope of the Design Review Appeals Board will be limited to final action on the site and architectural design components of the case as affected by the Peoria Design Principles and Guidelines. Such actions consist of recommendations (requiring a follow up meeting), conditions of acceptance, or continuance for further information. The Community Development Department shall provide notice at least fifteen (15) days prior to the Design Review Appeals Board hearing by publishing in a newspaper of general circulation within the City.
4. Continued compliance with approved Site Plan and Design Review verified through review of construction documents (Administrative).

b. Single-Family Residential

1. Refer to Chapter 3 and 5 applicability.
2. Chapter 3 subdivision design principles are administered through review of Preliminary Plat (Administrative).

3. A Single-Family Design Review application must be approved prior to the issuance of any one building permit within a subdivision. The review shifts to the architectural composition of the subdivision standard plans and options (Administrative).

c. Multi-Family Residential

1. Refer to Chapter 4 and 5 applicability.
2. Chapter 4 site design and architectural design principles are administered through review of Site Plan (Administrative).
3. In those cases where the 3rd submittal of the Site Plan has been provided and substantial progression has not been demonstrated or a design impasse has been reached as determined by the Planning Manager, such cases shall be forwarded to the Peoria Design Review Appeals Board for final action. The scope of the Design Review Board will be limited to final action on the site and architectural design components of the case as affected by the Peoria Design Principles and Guidelines. Such actions consist of recommendations (requiring a follow up meeting), conditions of acceptance, or continuance for further information. The Community Development Department shall provide notice at least fifteen (15) days prior to the Design Review Appeals Board hearing by publishing in a newspaper of general circulation within the City.
4. Continued compliance with approved Site Plan and Design Review verified through review of construction documents (Administrative).

d. Changes to existing structures

1. For non-residential or multi-family, if the addition or modification requires an amendment to the approved Site Plan, the site and architectural design standards will be administered through the review of the Amended Site Plan (Administrative). If no Site Plan exists, an amendment will trigger a Site Plan application and hence Design Review compliance.
2. Changes to non-residential or multi-family residential facades will be reviewed through a Design Review application (Administrative).
3. Qualifying additions or structures in existing Single-Family Residential subdivisions will be reviewed in conjunction with the applicable permit.

4. Duration of Approval

All Design Review approvals shall be congruent with the timeframes specified for Site Plan Review (18 months), Preliminary Plat (1 year) or the applicable permit (single-family residential addition/structure). For façade renovations to an existing structure a permit must be obtained within 18 months of the Design Review approval.

5. Appeal Process

a. Design Review Waivers (Planning Manager)

Any requests for a waiver from an applicable provision shall be submitted in writing to the Planning Manager along with all necessary exhibits and fees. The Planning Manager may deny, approve, modify, or approve with conditions any waiver request.

b. Appeal from an Administrative Decision (Design Review Appeals Board)

Any recommendation made by staff on any design review item or any action on a design review waiver may be appealed to the Design Review Appeals Board. The appeal shall be filed in writing with the City within ten (10) calendar days of the final determination. The Community Development Department shall provide notice at least fifteen (15) days prior to the Design Review Appeals Board hearing by publishing in a newspaper of general circulation within the City.

F. Glossary (20-78-1.1.F)

1. Arcade: A series of arches supported on piers or columns or an arched or covered passageway, usually with shops on each side.
2. Arch: A curved structure supporting its weight over an open space such as a door or window.
3. Articulation: Describes the degree or manner in which a building wall or roofline is made up of distinct parts or elements. A highly articulated wall will appear to be composed of a number of different planes, usually made distinct by their change in direction (projections and recesses) and/or changes in materials, colors or textures.
4. Building Mass: The height, width, and depth of a structure.
5. Cantilever: A projecting beam or other structure supported only at one end.
6. Chicane: Planoconvex, off-set curb extensions at an intersection or mid-block for the purpose of narrowing a street.
7. Choker: Planoconvex, matching curb extensions at an intersection or mid-block for the purpose of narrowing a street.
8. Colonnade: A row of evenly spaced columns usually supporting the base of a roof structure.
9. Corbeling: A projection from the face of a wall used to support a cornice or an arch.
10. Cornice: The horizontal projection at the top of a wall; the top course or molding of a wall when it serves as a crowning member.
11. Cupola: A small, usually domed, structure surmounting a roof.
12. Dentils: A series of small rectangular blocks projecting from a molding or beneath a cornice.
13. Dormer: A projection from a sloping roof that contains a window.

14. Facade Articulation: Stepping back or moving forward a portion of a building's facade for the purpose of breaking up the building's mass.
15. Facade Articulation Exhibit: An exhibit provided with Single Family design review applications that demonstrates sufficient facade articulation (per Chapter 3) of all standard plans in a Plat.
16. Flag Lot: A lot type by which access is provided via a narrow private access way.
17. Fenestration: The arrangement, proportioning and design of buildings, windows, and other exterior openings.
18. Hardscape: The use of hardened surfacing materials such as colored concrete, stamped concrete, pavers, Streetprint pavement texturing, and the like to create unique patterns of color, design, and texture in order to create visual interest. Exposed aggregate is not considered an acceptable hardscape option.
19. Human Scale: A term used to refer to the use of human proportioned architectural features and site design elements clearly oriented to human activity.
20. Keystone: The central wedge-shaped stone of an arch that locks its parts together.
21. Marquee: Any hood, canopy, awning, or permanent construction that projects from a wall of a building, usually above an entrance.
22. Monolithic: A single large flat surface (façade) without relief; a massive unyielding structure.
23. Mullions: The divisional pieces in a multi-paned window.
24. Ornamentation: Details added to a structure solely for decorative reasons (i.e. to add shape, texture, or color to an architectural composition).
25. Parapet: A low wall generally running around the perimeter of a flat roof.
26. Pedestrian Refuge Area: An area of retreat primarily for pedestrian usage.

27. Pedestrian Walkway/Path: An improved circulation path of varying materials for primarily pedestrian usage that is separate and distinct from traffic circulation. Such connections provide linkages between open spaces and/or outlying uses and are distinct from sidewalks as required by the City Engineer.
28. Perimeter Wall: A subdivision wall adjacent to a collector or higher street classification, retention area, open space area, multi-use trail area, and/or a wall on the perimeter boundary of the subject development.
29. Pilaster: A column attached to a wall or pier.
30. Pitch: The slope of a roof expressed in terms of ratio of height to span.
31. Pop-out: Variation in the plane of an exterior wall. Pop-outs create shadow and depth on the wall surfaces. (See *façade articulation*)
32. Portico: A porch or vestibule (lobby or passage between entrance and lobby) roofed and partly opened on at least one side.
33. Precast: Concrete that is cast in the form of a structural element before being placed in final position.
34. Primary Themed Entry: The principal or highest hierarchical access into a subdivision.
35. Projection: Any component, member, or part that juts or is set forward from the general wall surface.
36. Recess: Any surface set inward from the general wall.
37. Return: A surface turned back from a principal surface, such as the side of a pilaster or the jamb of a window or door opening.
38. Rhythm of façade elements: The pattern of fenestration, projections, recesses, and color and material changes on the face of an elevation.
39. Roof, Gable: A ridged roof forming a gable at both ends of the building.

40. Roof, Gambrel: A gabled roof with two slopes on each side, the lower steeper than the upper.
41. Roof, Hip: A roof with four uniformly pitched sides.
42. Roof, Mansard: A roof with two slopes on each of four sides, the lower steeper than the upper.
43. Ridge: The highest line of a roof where sloping planes intersect.
44. Rustication: A method of forming stonework with recessed joints and smooth or roughly textured block faces.
45. Sash: The framework into which window panes are set.
46. Soffit: The underside of a structure, such as the underside of a staircase and archway, or colonnade.
47. Symmetry: In architecture and landscape architecture, the balance of part by part which may be precise repetition, or repetition in counterpart of one element of a building or landscape in relation to another.
48. Texture: Texture refers to variations in the exterior facade and may be described in terms of roughness of the surface material, the patterns inherent in the material, or the patterns in which the material is placed.
49. Through Lots (a.k.a. Double Frontage): A lot which fronts upon two parallel streets, or which fronts upon two streets which do not intersect at the boundaries of the lot.
50. Traffic Calming: Mitigative traffic measures for the purpose of reducing the speed of traffic, reducing neighborhood cut-through traffic, and increasing traffic and pedestrian safety.
51. Useable Open Space: Areas containing active or passive amenities for the purposes of providing area to recreate or congregate.
52. Utility Cabinets: Any utility box, transformer, switching cabinet, capacitor bank, pedestal, or other accessories, which are owned or operated by a utility company, cable television company, or telecommunications provider. Traffic signals and utility cabinets associated with the functionality of the signal may be exempt from certain placement and screening standards.

Chapter 2: Non Residential Development

Administration

Non-Residential

Single-Family

Multi-Family

Specific Areas

I. Intent (20.78-2.I)

The purpose of this Chapter is to ensure the successful integration of safe, well-designed, and attractive non-residential development into the community. Such development should be designed in context with adjacent uses and sensitive to environmental resources. Such areas typically enjoy high visibility or serve as employment destinations. The community *image* is further enhanced by designs that exhibit exceptional quality, identity, and interest.

A. Applicability (20-78-2.I.A)

1. Non-residential development for new construction, excluding interior Tenant Improvements.
2. Alterations to the exterior façade of an existing building and/or suite/storefront including significant color or material changes or the use of exposed neon lighting.

B. Exemptions (20-78-2.I.B)

1. Interior alterations that combine one or more suite/storefront shall not be considered new construction or a new construction addition.
2. Cabinet or pan channel sign changes shall not constitute alterations under this section.
3. The following uses shall be held exempt from Design Review but are subject to evaluation of walls, colors, screening and context:
 - a. Monopoles or other wireless communication



Figure 2.1—A sculpted building corner adds visual interest.



Figure 2.2—Strong corner orientation minimizes view of parking fields.

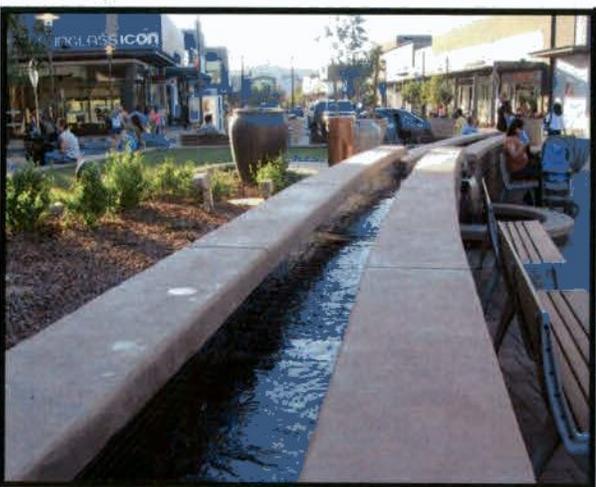


Figure 2.3—The buildings and public spaces are complementary and foster community interaction.

facilities.

- b. Utility substations.
- c. Accessory fabric canopy structures (i.e. car wash canopies).
- d. Mini-storage facilities, excluding accessory offices and any visible facades.

II. Site Design (20-78-2.II)

A. Site Layout and Orientation (20-78-2.II.A)

1. The site should integrate natural features into the development template such as topography, prominent view corridors, washes, and significant vegetative stands to enhance the character of the development.
2. Building placement and orientation along the street shall be staggered, angled, or curved to provide modulation and interest. Angled or sculpted building corners and/or an open plaza are encouraged at corner locations. (Figure 2.1)
3. Dated "L" shaped suburban shopping centers should be avoided. Clusters of smaller buildings with pad buildings at the street edge are strongly encouraged. (Figure 2.2)
4. Buildings should be oriented towards public spaces and should not back onto existing or planned amenities such as parks and open space. (Figure 2.3)
5. Driveway access points and internal circulation should be located as far away as possible from residential properties, schools, parks, and other sensitive uses.

6. The development shall be designed to facilitate the efficient use of energy through building orientation, window and door placement, landscaping, awnings, canopies, window treatment (i.e. glazing), and other appropriate design solutions.
7. Drainage basins should not be located within the front setback unless designed as an attractive landscaped element. Stormwater retention areas shall be designed as landscape features rather than large, unadorned depressions in the site.

B. Access and Circulation (20-78-2.II.B)

1. Parking lots should be designed with a clear hierarchy of circulation, major access drives with no parking, secondary circulation drives with little or no parking, and then parking aisles for direct access to parking spaces.
2. All non-residential developments shall provide at least one contiguous pedestrian walkway from the right-of-way to the building entrance walkway or sidewalk adjacent to the building. This walkway shall be separated from all vehicular movements except where drive aisle crossings are necessary. An on-site pedestrian circulation system shall link the various pads, buildings, and public spaces throughout the site. (Fig. 2.4)
3. All walkways that traverse vehicle drive aisles shall be distinguished with various hardscape materials such as specialty pavers, Streetprint™, or stamped colored concrete. Decorative materials should be used to clearly delineate pedestrian travel areas from drive aisles. Specialty paving material for walkways shall be developed in accordance with ADA requirements. Exposed aggregate is not viewed as an acceptable material. (Figure 2.5)
4. The use of landscaped pedestrian walkways, separated from vehicular movements in parking



Figure 2.4—A Break in the screen wall allows pedestrian access from the right-of-way to the building entrance.



Figure 2.5—Well-defined pedestrian crossing using stamped colored concrete.



Figure 2.6—Thematic pedestrian entry feature provides access from commercial to residential.



Figure 2.7—Transit stop in close proximity to development offers alternative modes of transportation.



Figure 2.8—Parking canopy and CMU columns reinforce the architectural design of the complex.



Figure 2.9—Parking structure is integrated into the building's architectural character.

areas, should be maximized. When appropriate, walkways should provide shaded pedestrian refuge areas, which may consist of trees, benches, tables, shade structures, or other appropriate elements. Such areas shall be functional and integrated into the site.

5. When appropriate, allow access from adjacent residential areas by incorporating fence and landscape penetrations into the pedestrian circulation element. (Figure 2.6)
6. To encourage alternative modes of transportation, site design should include, as appropriate, connections to existing and planned off-site trails/paths and existing and future/proposed commercial developments; and bikeways, bicycle parking and storage areas; and designs facilitating the use of mass transit. (Figure 2.7)

C. Parking Areas (20-78-2.II.C)

1. To reduce the visual impact, large parking fields should be "interrupted" through design solutions such as building placement, orientation and landscaping.
2. Utilize compact spaces to the maximum extent possible.
3. Parking canopies shall incorporate a design similar to the principle structure on the site or other existing structures in the area. (Figure 2.8)
4. The architectural style of adjacent buildings shall be incorporated into the design of parking structures.
5. The ground level of all parking structures shall be screened from view by one or more of the following: (Figure 2.9)

- a. Walls containing architectural details and embellishments.
 - b. Trees and shrubs.
 - c. Grillwork and/or greenscreens incorporating decorative metal artwork or panels.
 - d. An alternative design that meets the intent of the screening requirements.
6. Substantial massing should occur at the corners of parking structures to anchor the building and keep the structure proportions more inline with a regular commercial building.
 7. Horizontal openings of parking structures should be broken up with vertical columns, creating a sense of rhythm reflecting the contextual proportions of the center or area.

D. Project Entry and Character (20-78-2.II.D)

1. To foster a strong “sense of entry”, the primary vehicular entrance should be aligned with the most prominent building on-site. (Figure 2.10)
2. Elements such as project icons, thematic arches, special paving treatments, and mature, full-sized landscaping shall be integrated as a means to unify the project and accentuate the entry. (Figure 2.11)
3. The entry throat into the development shall be distinguished with hardscape materials such as pavers, Streetprint™, or patterned, stamped or colored concrete. Exposed aggregate is not viewed as an acceptable material.

E. CPTED — Crime Prevention Through Environmental Design (20-78-2.II.E)

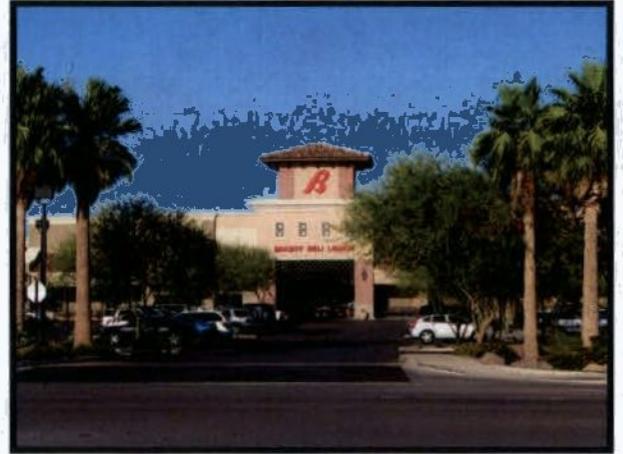


Figure 2.10— Trees and vertical design elements define the primary vehicular entrance to the project.



Figure 2.11— Thematic arch coupled with specialty paving accentuates primary entrance.



Figure 2.12—Public space is visible from the many windows and doors of the surrounding buildings.



Figure 2.13—Covered walkway/colonnade and a richness of materials enhance the pedestrian experience.



Figure 2.14—Covered customer entrances provide shelter from the elements.



Figure 2.15—Arcaded walkway with pedestrian lighting and arched architectural elements.

1. Entrances into parking lots should be defined by landscaping, architectural design, or monitored by a guard.
2. All doors that open to the outside should be well lit and visible from the street, parking area, or neighboring uses.
3. Parking areas, pedestrian walkways, elevators, stairwells, and recreation areas should be visible from windows and doors. (Figure 2.12)
4. Trash enclosures shall be sited in a safe manner that avoids blind spots and/or hiding areas.
5. Buildings should be sited so that the windows and doors of one unit are visible from another.
6. All four facades should have windows.
7. Building entrances should be accentuated by architectural elements, lighting, and/or landscaping.
8. Landscape design should not preclude visibility or surveillance capabilities to common areas and units.
9. For safety purposes, exterior doors should be designed with a solid core, peep holes, deadbolt locks, and reinforced with strike plates.
10. "Hostile Vegetation" such as Ocotillo, Cacti, or other plant types that discourage pedestrian movement or vandalism should be placed under windows or adjacent to long, remote expanses of perimeter walls, which are not otherwise designed as an amenity.

in a similar color and relate to other site furnishings. (Fig 2.20)

8. Use of native vegetation and low water plants shall conform to the Phoenix Active Management Areas Low Water Using Plant List, and be planted in conjunction with an efficient water system.

H. Lighting (20-78-2.II.H)

1. Lighting design shall be compatible with the building architecture, with fixtures of a consistent type, and size within the development. (Figure 2.21)
2. Lighting shall be provided within public spaces to provide visual interest as well as to serve a security function.
3. Light fixtures shall be sited, directed, and/or shielded to prevent spot lighting, glare, or light spillage beyond property lines.
4. Decorative theme lighting, accent lighting or lighted bollards shall be placed along access routes and pedestrian pathways to define areas of visual interest, improve visibility, and enhance safety. (Figure 2.22)

I. Walls and Fences (20-78-2.II.I)

1. Entry signage, parking, perimeter screen walls, and other site elements shall be consistent with the established thematic character of the development through the use of common architectural attributes such as colors, materials, and architectural style. (Figures 2.23, 2.24, 2.25)
2. Walls adjacent to retention areas, trails, parks, or other useable open space areas shall incorporate



Figure 2.22— Architecturally integrated light fixtures define an entrance gateway.



Figure 2.23—Consistent entry signage reinforces a centers design theme.



Figure 2.24—Parking screen wall is consistent with established architectural character of the development.



Figure 2.25—Design elements from a façade are echoed in the building's perimeter wall.



Figure 2.26—Trash enclosure utilizing colors and materials from the primary building.



Figure 2.27—Loading dock is artfully concealed with trellis feature.

regular undulation or variation in materials. Where appropriate, view fencing is strongly encouraged. Parking screen walls shall undulate a minimum of three (3) feet for every one hundred (100) linear feet.

3. All non-transparent perimeter walls and/or fences shall incorporate landscaping whenever possible.
4. Where security fencing is required, it shall be a combination of solid columns or short wall segments and wrought iron grillwork. (Figure 2.26)
5. Chain-link or similar metal wire fencing with slats is not considered appropriate for screening purposes.
6. Fences and walls should be constructed as low as possible while still performing screening, noise attenuation, and security functions.
7. To bring continuity to the overall streetscape, similar elements, such as columns, materials, and cap details, should be incorporated on perimeter walls that transition from one development to another.

J. Loading Areas and Trash Enclosures (20-78-2.II.J)

1. Loading areas and trash enclosures shall be oriented away from arterial streets and abutting residential areas to minimize visual clutter and avoid nuisances.
2. Trash enclosures shall be designed with similar finishes, materials, and details as incorporated with the primary buildings of the project. (Figure 2.26)

F. Pedestrian Amenities / Public Space (20-78-2.II.F)

1. Site design shall incorporate elements that enhance the pedestrian environment, such as features that reflect human scale, the use of covered walkways for the shelter and shade of the pedestrian, and richness of materials at the pedestrian level. (Figures 2.13 and 2.15)
2. Customer entrances shall provide weather protection features such as awnings, arcades, or colonnades. (Figure 2.14)
3. Walkways shall be enhanced by special design features such as towers, arcades, porticos, pedestrian light features, bollards, planter walls, and other architectural elements that define circulation ways and outdoor spaces. (Figure 2.15)
4. Commercial developments with multiple tenants shall provide common outdoor plaza areas or similar architecturally integrated public spaces which should include amenities such as seatwalls, and enhanced or expanded walkways. (Figure 2.16)
5. All pedestrian refuge areas shall provide pedestrian scale lighting and concentrated landscaping where appropriate.
6. Outdoor furniture and fixtures should be compatible with the project architecture and should be considered as integral elements of the project.
7. The area between buildings should be consciously designed and not an incidental remnant space without a definable function. (Figure 2.17)



Figure 2.16—Landscaped and lighted pedestrian area also includes integrated bicycle parking.



Figure 2.17—Well defined spaces between buildings create places for congregation.

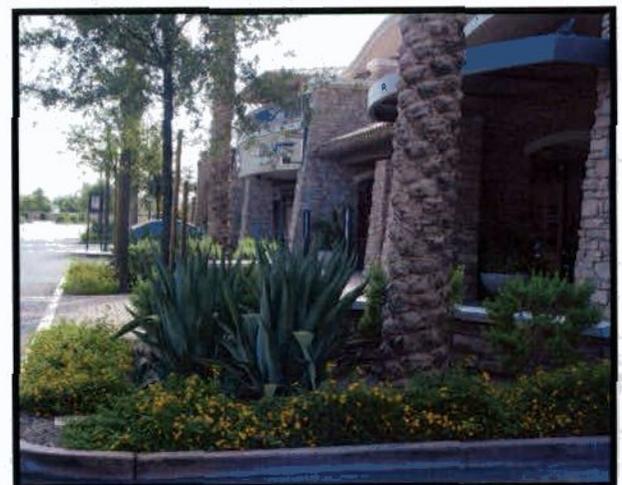


Figure 2.18—Abundant landscaping accents architecture and defines pedestrian areas.



Figure 2.19—Generous landscaping creates a transition between the building's facade and walkway.



Figure 2.20—Tree grates and guards protect trees while contributing to the design of a streetscape.



Figure 2.21—The design of wall-mounted and freestanding light fixtures correlates with the center's architecture.

G. Landscaping (20-78-2.II.G)

1. Landscaping shall be used to: (Figure 2.18)
 - a. Define areas such as building entrances, key activity hubs, focal points, and the street edge;
 - b. Provide screening for unattractive/unsightly service areas;
 - c. Serve as buffers between neighboring uses;
 - d. Screen drive-through/drive-up lanes;
 - e. Provide shade for pedestrian-vehicular uses.
2. Landscaping shall be provided at the foot of buildings to soften the transition between paved areas on the ground plane and building materials on the vertical plane. (Figure 2.19)
3. Potted plants and other ornamental landscaping should enhance courtyards, plazas, and other gathering areas. Trees should drain directly into the subsoil and should be protected by grating in hardscape areas.
4. Flowering trees should be used to provide color and accentuate entrances.
5. Canopy trees should be used in parking areas to reduce the impact of large expanses of paving and to provide shade, as well as to reduce glare and heat build-up.
6. Tree grates should occur along street edges and plazas where a continuous walking surface is needed. Grate sizes should be a minimum of four feet in diameter. Knockouts must be provided to enlarge the inside diameter for supporting a larger tree trunk as the tree grows. (Figure 2.20)
7. Tree guards should extend vertically from tree grates, and serve to protect trees in highly active areas. Tree guards should be narrow and painted

3. Trash enclosure locations shall be placed in proximate locations for tenant convenience and pickup service.
4. Trash enclosure locations shall not hinder internal traffic circulation visibility.
5. Loading docks shall be screened from view utilizing design solutions including, but not limited to, decorative walls, trellis/greenscreens, berming, dense landscaping, or a combination thereof. (Figure 2.27 and 2.28)
6. Public circulation should not route through loading and service areas.
7. If visible from public-view, roll-up doors are discouraged; however, where such doors occur, the doors should be recessed a minimum of twelve (12) inches into the building to provide a shadow line.
8. Service and roll-up doors shall be painted to match the building or trim colors.

K. Utilities and Mechanical Equipment (20-78-2.11.K)

1. Noise generating equipment should be located away from residential units, public spaces, and pedestrian areas.
2. Backflow preventers for landscape irrigation and domestic water shall not be located at visually prominent locations and shall be well-screened with shrubs, berming, or low-screen walls.
3. Roof access ladders and roof drains/downspouts shall be internalized within the building.



Figure 2.28—Loading area is screened with a decorative wall and trellis structure.



Figure 2.29—A fountain distinguishes the entrance to a retail center.



Figure 2.30—Bicycle racks are incorporated into overall thematic site design.



Figure 2.31—Congregation areas are defined by the use of bollards.



Figure 2.32—A cohesive and consistent design is threaded evenly through the commercial center.



Figure 2.33—Design elements from entry façade are continued on all sides of the building.

4. Ground and roof mounted mechanical equipment shall be screened from public view (street-level).
5. Utility screening requirements, including Service Entrance Sections, are located in Chapter 5 Supplementary Standards.

L. Other Thematic Elements (20-78-2.II.L)

1. Visual features, such as fountains, should be incorporated into commercial developments to attract pedestrians. Where a large number of children may be present, pop-jet fountains should be considered. (Figure 2.29)
2. Drainage crossings shall utilize decorative railings consistent with the thematic character of the complex. The determination whether a railing is warranted for a specific crossing shall be made by the City Engineer or designee.
3. The design of any outdoor storage or garden facilities shall complement the architecture of the primary building as well as the overall site design.
4. Bicycle racks should be selected that are durable and visually appealing. (Figure 2.30)
5. In the event that bollards are warranted to separate pedestrian and/or public spaces from circulation routes, said bollards shall compliment the architectural style of the center. Corporate colors applied to the bollards are expressly discouraged. (Figure 2.31)

III. Architectural Form (20-78-2.III)

A. Design Theme (20-78-2.III.A)

1. In an effort to encourage diversity in design, the use of "corporate chain" architecture is strongly discouraged.
2. Multi-tenant commercial centers shall have a consistent architectural and thematic palette throughout the development.
3. A commercial complex shall establish and maintain a consistent architectural style with individual buildings designed with complementary forms and materials. (Figure 2.32)
4. All sides of the buildings shall receive proportionate design consideration and treatment (360 degree architecture). (Figure 2.33)

B. Building Mass (20-78-2.III.B)

1. Perceived building mass shall be reduced by dividing the building mass into small scale components by providing a well-defined base, middle and top to the building. (Figures 2.34, 2.35)
 - a. A solid building base may be achieved by elements such as low planters and walls, base planting, a base architectural veneer banding (wainscot) and treatments defined by a different material, texture or color.
 - b. A distinct building middle may be achieved by the addition of covered walkways, trellises, colonnades, or architectural awnings that provide deep shadow.
 - c. A well defined building top may be achieved by utilizing features such as distinct and multiple architectural roof forms, clearly pronounced eaves, and distinct parapet designs and cornice treatments.
2. Building mass should be broken by dividing the building into smaller components and creating functional public space and pedestrian oriented



Figure 2.34—Building mass is reduced through a well-defined wainscot, middle, and cornice treatment.



Figure 2.35—Perceived building scale is minimized by an architecturally distinct building middle and cornice.



Figure 2.36— Building mass is reduced by creating a functional public gathering area between the buildings.



Figure 2.37—Front façade utilizes varying window styles and sizes.



Figure 2.38—Windows are accented by mullions, metal canopies, and building recesses and projections.



Figure 2.39—A well articulated façade exhibits an offset wall plane, color change, and columns.

areas between buildings. (Figure 2.36)

3. The use of colonnades along street fronting façades should be considered to reduce the massing of tall buildings and add pedestrian scale and interest.
4. Non-entry facades that face roads or views from public spaces should incorporate additional architectural treatments, such as pilasters, recessed areas, and windows with spandrel glass that give the appearance of windows facing the street.
5. Surface detailing, such as score lines or heavy stucco, should not serve as a substitute for distinctive massing.

C. Fenestration / Wall Penetrations (20-78-2.III.C)

1. Windows and doors should include visually prominent framing and accent elements. Materials, shape, and proportions shall complement the architectural style of the building.
2. Twenty-five percent (25%) of the primary entry façade shall be windows. Commercial developments in excess of 70,000 square feet shall provide fifteen percent (15%) of the primary entry façade in windows. Industrial developments shall provide ten percent (10%) of the entry façade in windows. Faux windows may be counted toward this requirement. (Figure 2.37)
3. Windows shall employ design details appropriate to the architecture, such as mullions, arched windows, shutters/faux shutters, window surrounds, awnings, and canopies to break the scale of the façade into smaller components. (Figure 2.38)

D. Façade Articulation (20-78-2.III.D)

1. To ensure minimal amount of horizontal articulation/undulation, no building wall on the primary building façade shall run more than fifty (50) feet without employing one (1) or more of the following: (Figure 2.39)
 - a. A twelve (12) inch offset in wall plane;
 - b. A column or pier at least one (1) foot wide and eight (8) inches deep;
 - c. A building corner or projection;
 - d. Or significant texture change.
2. Architectural elements, such as overhangs, trellises, projections, awnings, insets, material, texture, and color, shall be used to create shadow patterns that contribute to the building's character. (Figure 2.40)
3. Alternative designs will be reviewed for consistency with the intent of this section.

E. Roof Planes (20-78-2.III.E)

1. Rooflines shall be varied in height, form, and materials. Parapet rooflines shall be varied by stepping up and down or incorporating pitched roof elements. (Figure 2.41)
2. Parapet walls shall be designed and constructed in a manner to appear as a solid, three-dimensional form rather than a veneer. Parapets should include one (1) or more of the following detail treatments: (Figure 2.42)
 - a. Pre-cast elements;
 - b. Continuous banding or projecting cornices;
 - c. Dentils;
 - d. Caps;



Figure 2.40—Awnings create shadowing that augments existing façade articulation.



Figure 2.41 –Undulating parapet rooflines are composed of multiple materials and arched elements.

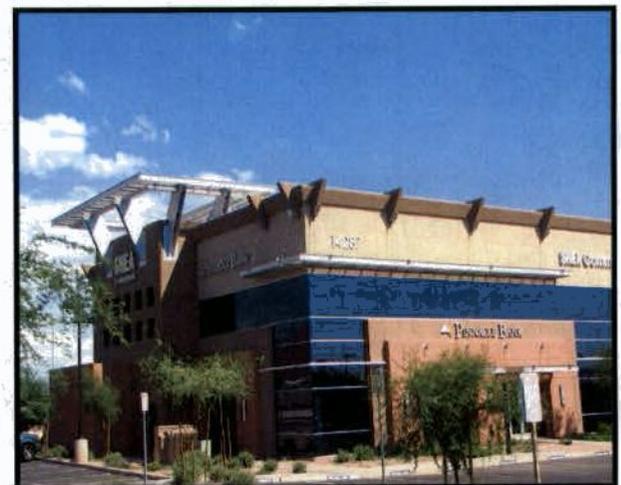


Figure 2.42 –Parapet wall exhibits substantial projecting cornice treatment.



Figure 2.43—Storefront entrance is defined by decorative arched framing and covered walkways.



Figure 2.44—Store entries are emphasized through the use of porticos and changes in roofline.



Figure 2.45—Stone, masonry, and stucco add character to the building exterior.

- e. Variety in pitch (sculpted);
- f. Clean edges without unfinished flashing.

3. Deep overhangs should be integrated to create shadow and add depth to facades.
4. All roof-mounted mechanical equipment shall be screened from view. A line of sight exhibit shall be provided showing the effectiveness of the proposed screening methods from the public right-of-way. For projects abutting single-family residential homes, mechanical equipment shall be screened from all adjacent second story windows.
5. All screening materials shall be compatible with the colors, materials, and design of the building.

F. Storefront Proportion (20-78-2.III.F)

1. The main entry into a store should be emphasized to announce a point of arrival in one or more of the following ways: (Figure 2.43 and 2.44)
 - a. Flanked by columns, decorative fixtures, or other details;
 - b. Recessed within a larger arched or cased decorative opening;
 - c. Covered by means of a portico (formal porch) projecting from or set into the building face;
 - d. Punctuated by means of a change in roofline, tower, or a break in the surface of the subject wall.
2. Commercial storefront entries should be recessed and/or sheltered by a covered arcade structure, colonnade, canopy, or awning.

G. Colors and Materials (20-78-2.III.G)

1. All developments shall employ the integrative use of multiple exterior accent materials including, but not limited to, brick, stone, and masonry in appropriate quantities with the proposed elevations. (Figure 2.44)
2. Material changes shall occur at intersecting planes, preferably at the inside corners of changing wall planes or where architectural elements intersect, such as pilaster, projection, or fence line. (Figure 2.45 and 2.46)
3. When stucco is utilized a light to smooth finish is required and shall be blended with other finish materials, such as stone, brick, wood, and/or iron.
4. Material and colors shall be used to enhance different parts of the building's façade.
5. As the context dictates, a rich and/or distinctive color palette shall be employed to create a sense of variety and interest to exterior elevations. (Figure 2.47)
6. Bright primary colors shall be limited to trim and accent features only.

IV. Supplemental Development Criteria (20-78-2.IV.)

All of the following items shall be achieved in addition to the other required Design Review elements:

A. Shopping Centers, Office Complexes and Other Commercial Centers Exceeding 50,000 square feet (GFA) or



Figure 2.46—Multiple material and color changes occur at intersecting planes.

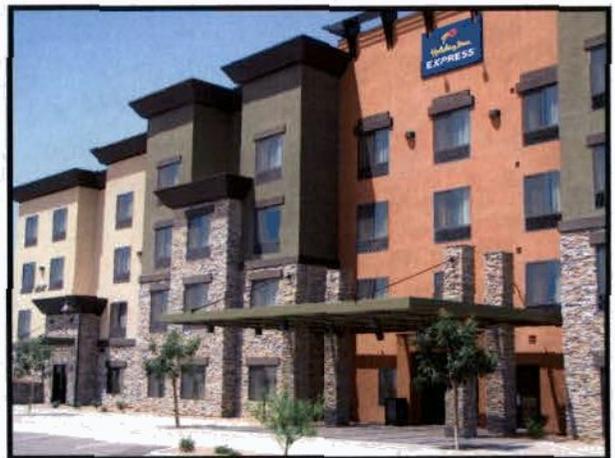


Figure 2.47—Rich, bold colors accent the building's architectural features.



Figure 2.48—Clock tower creates a visual focal point for the retail center.



Figure 2.49—Integrated freestanding iconography help establish center identity.

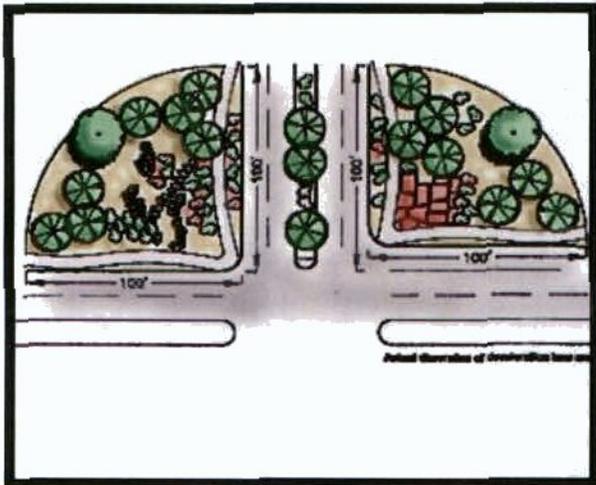


Figure 2.50—Triangular landscape areas are located at the entrance to a Primary Themed Entry



Figure 2.51—Hardscape treatment and palm trees act to define main circulation drive aisle.

5 acres (NET) Overall (20-78-2.IV.A)

1. A minimum of four (4) of the following items shall be achieved in addition to the other required Design Review elements:
 - a. Provide freestanding architectural features that are thematically consistent. Such features may include, but are not limited to entry arches, fountains, formal entry aisles, obelisks, trellises, integrated artwork, clock towers, and other similar features. (Figure 2.48 & 2.49)
 - b. Provide angled or staggered building orientation to achieve varied alignment of building fronts and site design features that in turn create functional outdoor spaces, and/or enhanced intersection feature.
 - c. Provide tree-lined "Boulevard" at median break (s), with detached sidewalks and landscaping.
 - d. Where appropriate, locate artistic elements in open public locations. Artistic elements must be consistent with the overall theme of the center/facility. Artistic elements may include sculptures and murals, gates, fences, walls, benches, fountains, walkways, etc.
 - e. Stagger parking setback by at least five (5) feet or vary orientation of parking lot along arterial streetscape, while maintaining the minimum required setback from the right-of-way.
 - f. Incorporate planter and seating features in pedestrian areas throughout the site, including areas along the front of the buildings.
 - g. On-site landscaping exceeds 25% of the net site area.
 - h. One or more buildings are pushed to the street such that there is no intervening parking between the building and streetline.
 - i. Any other design amenity, which is otherwise not required but which meets the general intent for design innovation, may be substituted for any of the above additional quality standards.

B. Large Scale Retail (20-78-2.IV.B)

1. Any developments involving a Large-Scale Retail establishment as defined in the Peoria Zoning Ordinance shall provide the following, in addition to the other required Design Review elements:

a. Building mass shall be divided into smaller components, thereby creating functional public space and pedestrian friendly areas.

b. *Primary Themed Entry*

The Primary Themed Entry should contain a triangular landscaped area on each side. The triangular landscaped area should be defined by a) a minimum of one-hundred (100) feet along the property line adjacent to the arterial right-of-way and b) a minimum of one-hundred (100) feet along the face-of-curb along the entrance drive, and c) with both one-hundred (100) - foot or greater lines measured from the intersection of said property line and said face-of-curb as noted above.

Landscape triangle should include thematic elements such as water features, clock towers, public art, or other architectural features. (Figure 2.50)

c. *Main Circulation Aisle*

The main drive aisle connecting major public roadways to drive aisles interior to the site shall be considered the Main Circulation Aisle(s). Main Circulation Aisle(s) shall be delineated and distinguished from the surrounding parking area through the use of substantial hardscape treatment of entry throat, landscaping, decorative lighting, and other design elements, and shall provide no direct access to any parking spaces within one-hundred (100) feet of the entry. (Figure 2.51)

d. *Intersection Nodes*

Intersection nodes formed by the intersection of drive aisles shall be designed with decorative pavers or other architectural elements, intended to enhance awareness of the potential for conflicts between crossing vehicles.

e. *Shading*

Pedestrian routes located adjacent to building walls shall include functional shade; said shade will be provided by roof overhangs, landscaping, or other architectural materials, and will provide shade for the

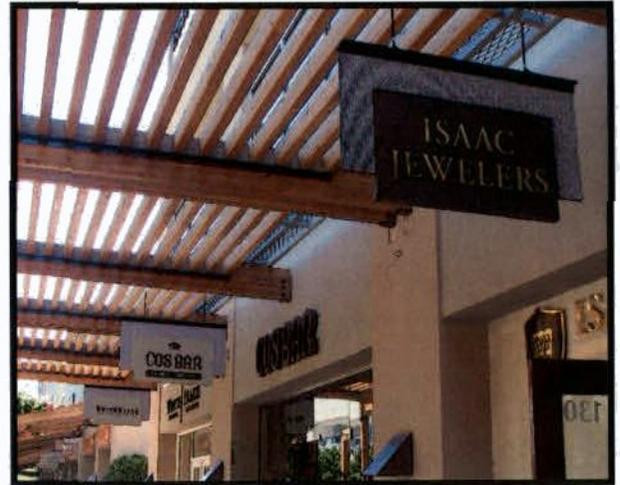


Figure 2.52—Pedestrian walkways alongside the building are shaded for comfort.



Figure 2.53—A covered principal pedestrian walkway leads from parking to a shopping center entrance.



Figure 2.54—Colored concrete and future shade trees define the pathway from parking area to building front.



Figure 2.55—A courtyard inside a Large-Scale Retail center provides seating and tables, as well as shade.

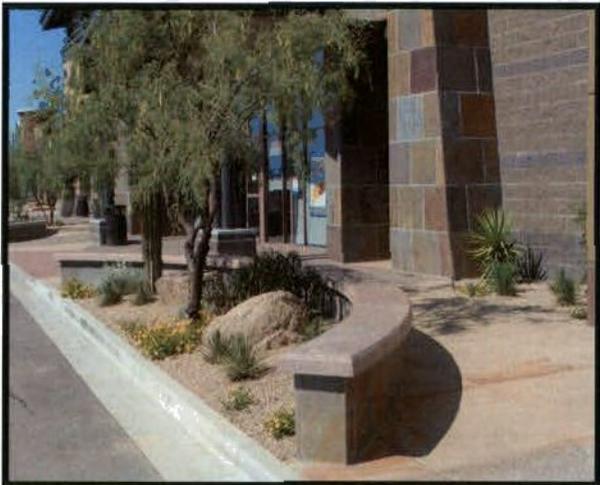


Figure 2.56—Building front landscaping creates a functional space utilizing seat walls and shade trees.



Figure 2.57—Cart storage is integrated into the building design.

entire width of the sidewalk along at least fifty percent (50%) of the building wall. (Figure 2.52)

f. *Principal Pedestrian Walkways*

Principal pedestrian walkways, consisting of (at minimum), a fourteen (14)-foot landscaped aisle, incorporating an eight (8) –foot walkway, defined with trees and shrubs, shaded with softscape or hardscape materials, and extending from the adjacent arterials to principal building entrances. A minimum of one such walkway will be provided per every three-hundred (300) feet of primary or business front footage, or as otherwise deemed to meet the intent of this ordinance. A minimum of one such walkway will connect to the main entrance of a principal building/tenant; other walkways will connect to other principal buildings/tenants or principal pedestrian areas (such as an outdoor plaza). (Figure 2.53)

g. *Interior Pedestrian Walkways*

Clearly defined pedestrian circulation systems shall be provided throughout the project site. Interior pedestrian walkways connecting parking areas to other pedestrian walkways or buildings, shall consist of (at minimum) a six (6) –foot walkway clearly delineated with decorative pavers or other architectural elements. Such walkways shall be designed to provide shade through landscaping and/or other materials. (Figure 2.54)

h. *Public Space*

Public spaces shall be provided in the form of plazas, courtyards, and/or outdoor eating areas, located adjacent to large-scale retail establishments and/or to other buildings on the site, at a ratio of a minimum of nine-hundred (900) square feet for each one-hundred thousand (100,000) square feet (G.F.A.) of Large-Scale Retail, as defined in the Peoria Zoning Ordinance. (Figure 2.55)

Public space shall contain benches , decorative pavers, trash receptacles, and other thematic elements to provide a distinct identity and sense of place. Seating areas and walkways connecting the refuge area to buildings or other walkways shall be shaded using trees, roof overhangs, canopies and/or similar. An arbor may be used in place of a tree. (Figure 2.56)

i. *Transit Facility*

When located on the site or in the right-of-way adjacent to the site, public transit facilities shall be architecturally

integrated into the design of the site through use of materials, color, and other architectural elements.

j. Bicycle Facilities

Facilities for bicycle parking shall be shaded and integrated into the site at the main entrance of the principal building.

k. Cart Storage

The design of shopping cart storage facilities shall complement the established architectural theme of center. (Figure 2.57)

C. Industrial (20-78-2.IV.C)

1. Wall forms shall be articulated with changes in massing, colors, and material. Such a change in horizontal walls shall occur every fifty (50) feet or less. (Figures 2.58 and 2.59)
2. Where appropriate, colonnades should be utilized along the street frontage façade to reduce the massing of tall buildings and add pedestrian scale.
3. Structures two-stories or higher should incorporate a step in the vertical plane to reduce the scale of the building. This step can be accomplished by stepping back the floors above the first or by projecting first floor elements or wall surfaces.
4. A full pitched roof over an entire industrial building is not realistic. However, where feasible and appropriate to the architectural style for the building, a pitched roof element should be provided over the entry and/or office portion of the structure. (Figure 2.60)
5. Pre-cast walls/tilt-up shall incorporate reveals, recessed panels, recessed windows, and/or molding to articulate the building exterior. (Figure 2.61)
6. All concrete pre-cast/tilt-up buildings shall be



Figure 2.58—Curved wall and tile pattern interrupt the horizontal wall plane to create visual interest.



Figure 2.59—Prominent entry is defined by variation in materials.



Figure 2.60—Multi-planar components introduce depth and direction.



Figure 2.61—Tilt-up panels incorporating protections with varying materials increases façade articulation.



Figure 2.62—Carwash exhibits characteristics of main building through the use of roof pitch and façade.

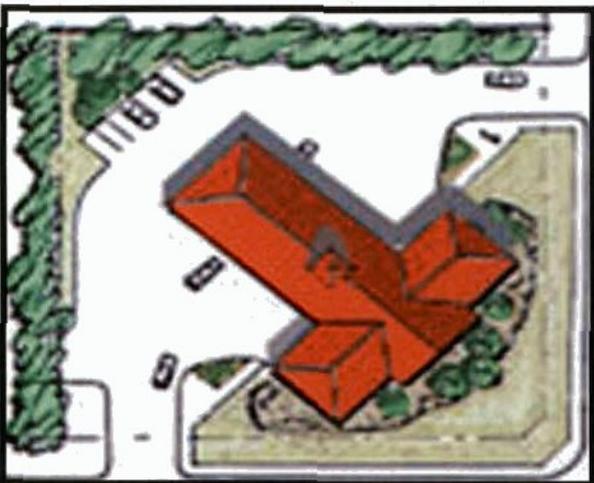


Figure 2.63—Reverse orientation of canopy minimizes its visual impact to the street and intersection.

designed to have an exterior appearance of conventional built structures utilizing surface treatments such as stucco, plaster, glass, stone, brick, or decorative masonry.

7. Expansive paved areas located between the street and the building should be avoided in favor of multiple small lots separated by landscaping and buildings.
8. To soften the transition between industrial and residential uses, elevations facing residential uses shall be substantially consistent with the architecture exhibited on the front elevation.

D. Gasoline Stations & Car Washes (20-78-2.IV.D)

1. All structures on-site (including canopies, kiosks, car wash facilities, gas pump columns, bollards protecting the gas pumps, gas pumps (excluding sign toppers), etc.) shall be consistent with and complement the architectural design of the primary building and overall project site. (Figure 2.62)
2. Bay door and car wash openings should be oriented to reduce visibility of these areas from public streets and should not face residential properties. Reverse orientation for canopy and service areas is encouraged. (Figure 2.63)
3. Column supports shall be of sufficient thickness to portray a visual sense of strength, balance, and traditional masonry proportions.
4. Light fixtures mounted under canopies shall be completely recessed into the canopy with flat lenses that are translucent and completely flush with the bottom surface (ceiling) of the canopy. The fascias of the canopy shall extend below the lens of the light fixtures twelve (12) inches. Unless overlapping a portion of the principal building,

gasoline canopies shall not exceed the height of the principal building.

5. Lighted bands, tubes, or corporate colors shall be limited to accent elements and not be primary design features.
6. All gas tank vent piping shall be screened from arterial streets and public view.
7. Car wash facilities should incorporate small plazas or other areas for patrons to comfortably wait while their vehicles are being washed.

E. Drive-Through / Drive-Up Facilities (20-78-2.IV.E)

1. The primary visual presence along major street frontage should be the building front, not the drive-through aisle or parking lots.
2. Drive-through menu boards shall be screened from street view, and architecturally integrated with building design through the use of common materials and colors.
3. All drive-through facilities, including drive-up lanes and stacking areas shall be adequately screened from street view by building orientation, landscaped berm or a screen wall measuring three (3) feet from grade of the driving lane, and situated so not to disrupt safe traffic flow. (Figure 2.64)
4. Drive-through lane, including entry and exit points, shall be separated from vehicular parking areas and access aisles through curbing, landscaping, etc. (Figure 2.65)
5. Drive-through windows and canopies shall be architecturally integrated into the building. (Figure 2.66)



Figure 2.64—Heavy landscaping screens the drive-through from street view.



Figure 2.65—Sidewalk and wrought iron fence divide the center's access aisle and drive-through lane.



Figure 2.66—The main building architecture is carried through to the drive-through canopy design.

Chapter 3: Single-Family Residential

Administration

Non-Residential

Single-Family

Multi-Family

Specific Areas

I. Intent (20-78-3.I)

The purpose of this Chapter is to guide the integration of new neighborhoods and housing types into the community fabric and safeguard and enhance the character of established neighborhoods. The design principles attempt to promote a deviation from the standard "cookie-cutter" format of garage dominant building lines and repetitiveness through a diverse palette of standard plans, articulation, color and material choices and neighborhood identity. Neighborhoods should contain safe and practical open space areas with context and connectivity to adjacent land uses and circulation modes.

A. Applicability (20-78-3.I.A)

1. New single-family detached or attached (up to 2 units per building) residential developments exceeding 10 lots.
2. New additions to existing residential units, including patio covers, carports, and RV covers, as specifically addressed herein.
3. New accessory structures exceeding nine (9) feet in height or three hundred (300) square feet in area, including RV covers, as specifically addressed herein.
4. New gazebos, ramadas, or similar structures as specifically addressed herein.



Figure 3.1—Detached single-family residential neighborhood.



Figure 3.2—Example of attached single-family residential type.

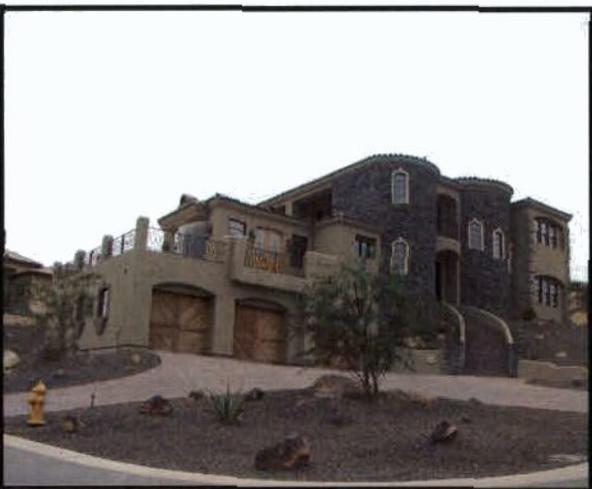


Figure 3.3—Example of Custom Single-Family Residential type.

B. Exemptions (20-78-3.I.B)

1. Custom-home developments are exempt from the Façade Articulation Exhibit provision per 20-78-3.II.C.1.f; However, each elevation shall demonstrate the minimum building plane articulation per 20-78-3.II.C.1.e.
2. Play structures, sport courts, private water features, and similar minor structural elements as identified by the Planning Manager.
3. Arizona Rooms

II. Single-family Residential Design Standards (20-78-3.II)

A. Subdivision Design (20-78-3.II.A)

1. LOT AND TRACT DESIGN (20-78-3.II.A.1)

- a. In an effort to promote safety, connectivity, and maintenance, tracts less than fifty (50) feet in width between lots (lot "fingers" or "wedges") shall be avoided unless mitigation is provided. Such mitigation may include pedestrian connections, low level accent lighting, view fencing, or combinations thereof on adjacent lots or other design solutions that provide function and attention to narrow, dead spaces. (Figure 3.4)
- b. In an effort to enhance lot functionality, pie-shaped lots should be designed to avoid acute angles less than forty-five (45) degrees. (Figure 3.5)
- c. Lot frontages shall be located a minimum of one hundred fifty (150) feet from arterial street intersections.

- d. To minimize glare from automobile lights and promote safe traffic maneuverability, lots should not be designed to center on a T-intersection unless adjustments or mitigation can be demonstrated. (Figure 3.6)
- e. Through lots/double frontage lots are prohibited. (Figure 3.7)
- f. Flag lots shall be permitted provided the following criterion are met:

1. Each flag lot shall have at least twenty (20) feet of street frontage and at least twenty (20) feet of width for the entire length of the flagpole.
2. The area of the flag pole portion of the flag lot shall not be included in the calculation of minimum lot area.
3. For flag lots, the Planning Manager or designee shall determine which property line shall constitute the front and rear lot lines for the purposes of compliance with yard and setback provisions of this ordinance.
4. The driveway providing access to the flag lot shall be placed as close as possible to an existing driveway on adjacent property.
5. No more than two (2) flag lots may be contiguous. (Figure 3.8)

2. CRIME PREVENTION THROUGH ENVIRONMENTAL DESIGN (20-78-3.II.A.2)

- a. To the extent feasible, windows, doors, or other openings should be provided on all sides of a façade to create visual interest and prevent dead spaces. (figure 3.9)



Figure 3.4—A pedestrian path and two-story homes provide activity and “eyes” on this narrow tract.

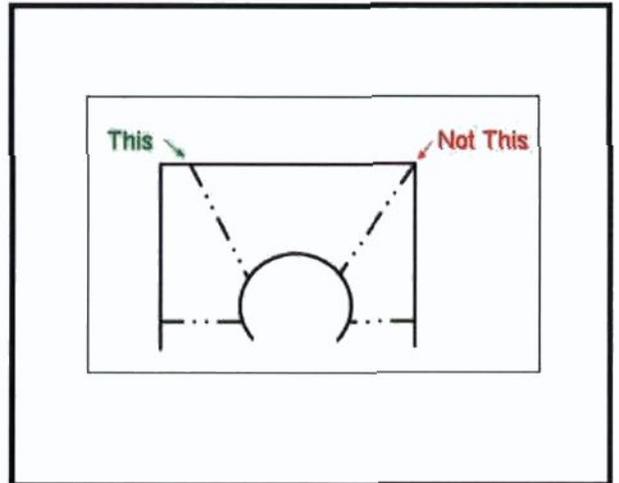


Figure 3.5—Avoid angles less than 45 degrees.

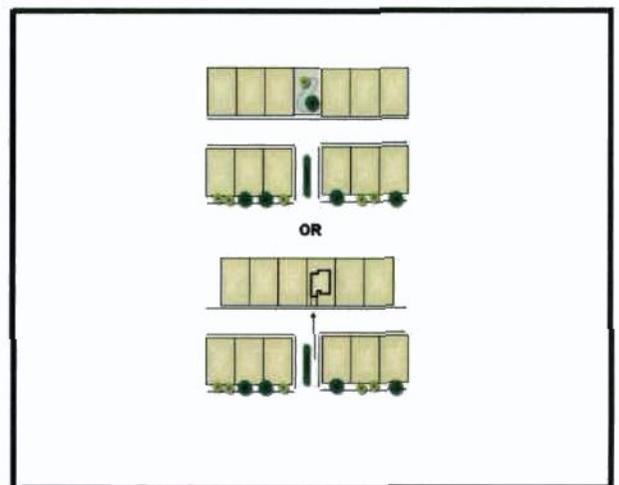


Figure 3.6—Lot adjustments and layout prevent alignment on T-intersections.

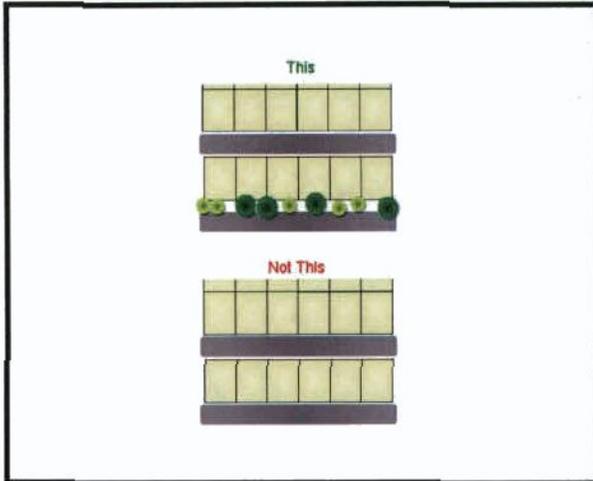


Figure 3.7—Avoid Through/Double Frontage Lots.



Figure 3.8—Lots 12 and 13 illustrate flag lots, as well as limitation against more than 2 contiguous flag lots.



Figure 3.9—Multiple “perspectives” on the street provide interest and reinforce neighborhood ownership.

- b. Streets should be designed to discourage cut-through traffic.
- c. Access (to the subdivision) should be limited without completely disconnecting the subdivision from adjacent subdivisions.
- d. Landscaping should not create blind spots or hiding spots.
- e. Open spaces and recreational areas should be located so that they can be observed from nearby homes. (Figure 3.10)

3. CIRCULATION (20-78-3.II.A.3)

- a. All local residential streets exceeding one thousand (1000) feet in length shall be curvilinear in design unless traffic calming measures are proactively utilized.
- b. All collector and non-residential collector streets exceeding 1000 feet in length shall proactively incorporate traffic calming measures. Examples of traffic calming measures suitable for local streets include, but are not limited to; roundabouts, traffic circles, landscape islands, or medians, chicanes, chokers, raised intersections, and/or crosswalks, or other approved measures. (Figure 3.11)
- c. To minimize potential conflict points, four-way street intersections at local/local intersections are discouraged.
- d. Pedestrian connections should be established throughout the community to provide connectivity and to reduce walking distances, especially at the ends of cul-de-sacs and along routes to destinations such as parks, schools, and services. (Figure 3.12)

4. STREETScape (20-78-3.1.A.4)

- a. Developments shall provide a sense of arrival at all primary themed entries into a subdivision. To achieve this objective, one or more of the following elements shall be incorporated into each primary themed entry:
 1. Hardscaped accented entry, including but not limited to, stamped or colored concrete and pavers as approved by the City Engineer or designee.
 2. Landscaped medians along the entry throat. (Figure 3.13)
 3. Entry monument, or monument sculpture, water feature, or other identifying element as approved by the Planning Manager. (Figure 3.14)
- b. The Development shall provide triangular landscaped entry areas at all primary themed entries into a subdivision. The size of the entry area should be no less than the visibility triangle required for the intersection plus the prescribed street frontage landscape buffer. For example, a 30' x 30' view triangle at an arterial or collector intersection would include a ten (10) foot landscape buffer (hence 40' x 40' triangle). This allows for trees or structures outside of the visibility triangle.
- c. Cul-de-sacs shall not terminate on blank walls or narrow landscape tracts less than ten (10) feet in width. (Figure 3.15)
- d. Detached sidewalks shall be provided adjacent to all arterial roadways and some collector roadways in accordance with the Infrastructure Guidelines. Detached sidewalks should be separated from the roadway by a minimum of four (4) feet. (Figure 3.16)



Figure 3.10—Recreation areas are within view from several nearby homes.



Figure 3.11—Landscaped traffic circle is one type of traffic calming measure.



Figure 3.12—Pedestrian path provides connection from cul-de-sac to collector roadway.



Figure 3.13—Landscaped median leading into the community.



Figure 3.14—Primary themed entry into a subdivision.



Figure 3.15—Non-enclosed cul-de-sac spills onto open space and recreation areas rather than walls.

- e. The sidewalks along arterial roadways should provide for a gentle meander to elevate the aesthetic and pedestrian quality. The sidewalk shall return to the back of the curb or right-of-way line no less than twenty (20) feet from any intersection to minimize the potential for pedestrian—automobile conflicts. (Figure 3.17)
- f. To the extent practical, all utilities and ground mounted mechanical equipment shall be vaulted or fully screened from view through landscaping placement and/or color treatment.
- g. Use of native vegetation and low water plants shall conform to the Phoenix Active Management Areas Low Water Using Plant List, and be planted in conjunction with an efficient water system.

5. USEABLE OPEN SPACE (20-78-3.II.A.5)

- a. All developments shall provide a minimum area of Useable Open Space in accordance with the schedule outlined in Table 1. (Figure 3.18)
- b. Useable Open Space areas shall be clearly designated on the Preliminary and Final Landscape Plans. Useable Open Spaces include, but are not limited to the following:
 1. Park or recreation areas (private or dedicated);
 2. Designated or reserved equestrian, hiking, local connectors, and multi-use trails. For the purposes of computation, a twenty (20) foot corridor width may be calculated, provided the corridor is improved with landscaping and pedestrian amenities such as benches (where appropriate) and pedestrian scale lighting (Figure 3.19);

3. Reserved or dedicated hillside preserves;
 4. Golf courses (area not to count more than fifty (50) percent toward minimum requirement);
 5. Active amenities including, but not limited to, racquet and sport courts or fields (i.e. basketball, volleyball, baseball, etc.), and substantial turfed area.
 6. Common pools, spas, and recreation centers; (Figure 3.20)
 7. Passive amenities including, but not limited to, substantial turfed areas, ramadas, barbecue areas, bench seating areas, and specified interpretive or demonstration facilities; (Figure 3.21)
 8. Retention basins and floodway areas when improved with active or passive amenities as described above.
- c. In an effort to create a seamless network of open spaces and pedestrian connections, subdivisions shall be designed to integrate and/or continue existing or approved open space areas, pedestrian walkways, and multi-use trails from adjacent developed and undeveloped areas.
- d. Shaded pedestrian refuge areas should be provided along pedestrian and multi-use walkways, paths, and trails.

B. Thematic Form (20-78-3.II.B)

The intent of this section is to promote distinctive neighborhood identity and discourage bland, ubiquitous communities.



Figure 3.16—Detached meandering sidewalk provides refuge and separation from the arterial roadway.



Figure 3.17—An offset sidewalk "softens" the streetscape and enhances visual interest and safety.

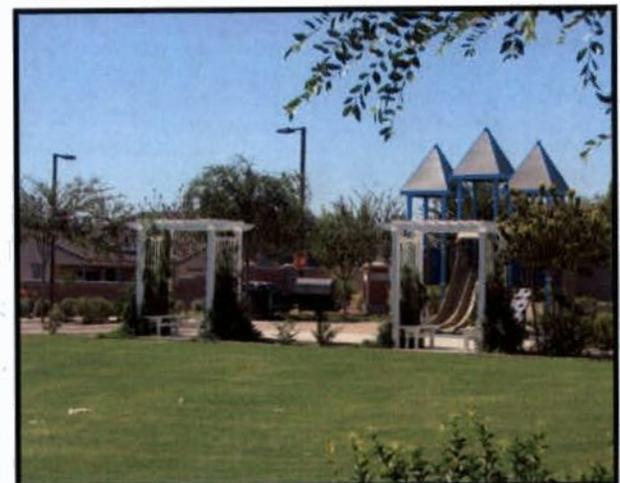


Figure 3.18—Open space area in the subdivision has been reserved for recreation.



Figure 3.19—A well signed and demarcated hiking trail provides an alternative type of open space amenity.



Figure 3.20—A common HOA pool serves as an additional type of useable open space amenity.

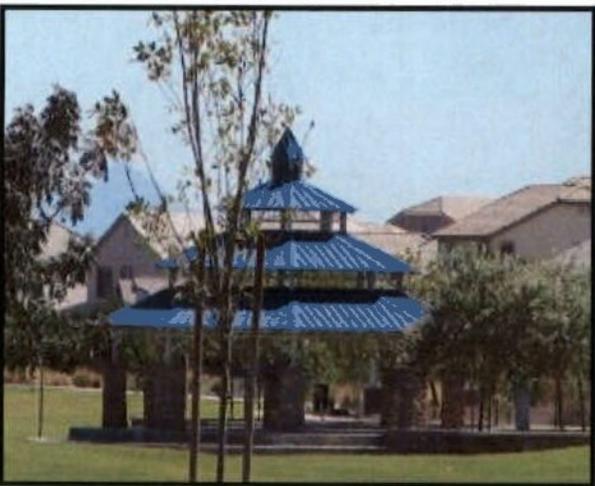


Figure 3.21—Ramada and green belt provides a passive open space amenity.

1. WALLS AND FENCES (20-78-3.II.B.1)

- a. Developments should provide pedestrian access to adjacent non-residential areas, including schools and churches, by incorporating wall and landscape penetrations into the pedestrian circulation element.
- b. Entry signage, perimeter walls, and other appurtenances in community owned areas shall be consistent with the thematic character of the development through the use of common integrative elements such as colors, materials, and architectural style.
- c. Perimeter walls shall be constructed of decorative masonry block intended to be utilized without additional finish. Such walls adjacent to streets with a classification of arterial or higher shall meet at least two (2) of the following requirements; perimeter walls adjacent to local or collector streets shall provide at least one (1) of the following mitigations:
 1. Integration with one (1) or more of the following: inlays, materials, or color accents, capping, decorative pilasters, wrought iron, planters or other approved feature that adds visual interest. Strong geometric patterns are strongly discouraged. (Figures 3.22, 3.23 and 3.24)
 2. Incorporation of raised planters containing hostile vegetation or minimal heights to discourage unwanted access to private yards. (Figure 3.23 and 3.24)
 3. Incorporation of decorative stone or masonry pillars with caps. (Figure 3.23 and 3.24)
 4. Incorporation of decorative wrought iron trellises or artistic features in context with the area.

5. Other alternative design as approved during the Preliminary Plat Review Process.

d. Walls adjacent to retention areas, trails, parks, or useable open space areas shall incorporate regular undulation or variation in materials. View fencing is strongly encouraged. (Figure 3.25)

e. Chain link (cyclone) fences shall be prohibited, except in the AG, SR-35 and SR-43 Zoning Districts. Wood or metal fences may be considered if integrated into an identifiable theme. Such theme may include rural/agrarian with the use of wooden picket fencing or a metal pole corral style open fencing. All perimeter subdivision walls in AG, SR-35 and SR-43 Districts shall be masonry.

2. LIGHTING (20-78-3.II.B.2)

a. Primary-Themed Entry features should use backlit decorative lighting to enhance walls and signage.

b. All street lighting along arterials shall be screened or shielded to minimize glare and illumination onto residences.

c. Decorative theme lighting, accent lighting or lighted bollards shall be placed along pedestrian connections and in useable open space areas to improve visibility and safety.

3. OTHER THEMATIC ELEMENTS (20-78-3.II.B.3)

a. Drainage crossings shall utilize decorative railings consistent with the thematic character of the development. The determination whether a railing is warranted for a specific crossing shall be made by the City Engineer or designee.



Figure 3.22—Theme wall integrates color accents, multiple textures, decorative pilasters and capping.



Figure 3.23—Integrated raised planters provide further visual interest and reduces perceived wall massing.



Figure 3.24—Integration of multiple textures and planter box.



Figure 3.25—View fencing opens views to open spaces.



Figure 3.26—Clustered mailboxes containing similar thematic character to the development.



Figure 3.27—This streetscape features varied housing styles.

4. CLUSTERED MAILBOXES (20-78-3.II.B.4)

- a. Clustered mailboxes should be consistent with the thematic character of the development through the use of common integrative elements such as color palette, building materials, and roof pitch. (Figure 3.26)
- b. Clustered mailboxes should be placed along side lot lines or open space tracts and located a minimum of one hundred (100) feet from a collector or arterial street intersection. The intention is to provide adequate separation between mailbox queuing and subdivision entry.

C. Architectural Form (20-78-3.II.C)

The intent of this section is to promote substantial design elements in housing to foster diverse, attractive and integrative neighborhoods.

1. DESIGN PROFILE (20-78-3.II.C.1)

- a. To promote architectural diversity, all developments shall implement the applicable prescriptions outlined in Table 2. In the implementation of Table 2, each standard plan shall be provided with a minimum of three (3) elevations.
- b. No more than two (2) consecutive lots (adjacent side lot lines) shall share the same standard plan and elevation. (Figure 3.27)
- c. To mitigate the impact from monotonous building elevations, design solutions integrating facade articulation, variable building angles, eaves, parapets, and the strategic placement of windows and doors shall be employed in each elevation.

d. Each elevation shall integrate architectural embellishments including, but not limited to, recessed windows or window projections, articulated facades and decorative moldings, or pilasters for the purpose of creating shadow lines. (Figure 3.28)

e. Not more than sixty (60) percent of any front elevation shall occur on the same plane. Plans with two planes in the front elevation shall provide a minimum undulation of four (4) feet between said planes; plans with three (3) or more planes in the front elevation shall provide a minimum undulation of two (2) feet between said planes. To demonstrate compliance with this section, a Façade Articulation Exhibit shall be provided with the Design Review submittal. (Figure 3.29 and 3.30)

f. Roof mounted HVAC & evaporative cooler equipment shall be prohibited. (Figure 3.31)

2. MATERIALS AND COLORS (20-78-3.II.C.2)

a. All developments should integrate multiple exterior materials including brick, stone, and masonry as appropriate with the architectural style of the home. (Figure 3.32)

b. All developments shall provide a diversity of roof colors, materials, and styles within the subdivision in accordance with the schedule outlined in Table 2. (Figure 3.33)

c. Hillside lots as defined in Article 14-22A ("Hillside Development Overlay District") are subject to the color and reflectivity restrictions contained therein.

d. With the exception of windows and similar features, the use of reflective surfaces is prohibited.



Figure 3.28—This elevation features rounded corners, stone, and various decorative elements.

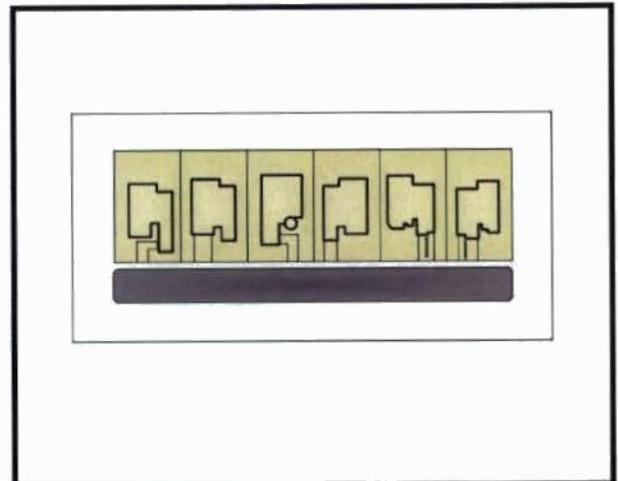


Figure 3.29—Façade Articulation Exhibit demonstrates a "staggered" street profile.



Figure 3.30—Structure exhibits "staggered" façade.



Figure 3.31—Residences without roof-mounted HVAC provides for a cleaner, more visually appealing look.



Figure 3.32—Home provides interest through orientation, use of stone, corbels, rich base color, & framing.



Figure 3.33—A diversity of roof types, materials and styles are in evidence.

- e. Exposed metal chimney flues inconsistent with the architectural character are prohibited. Exposed flues (B-vent) are acceptable only with fireplaces without a chimney.

3. GARAGES AND DRIVEWAYS (20-78-3.II.C.3)

- a. Driveways shall be no less than eighteen (18) feet in length (exclusive of curb and sidewalk) for forward-facing garages.
- b. Driveways for three-car or larger forward-facing garages shall incorporate alternative paving design elements including, but not limited to, rock salt finishes, broom, float and trowel finishes, stamped concrete, concrete engraving, concrete stains, intricately sawed / grooved patterns, concrete pavers, and colored concrete to soften the appearance of large impervious surfaces. (Figure 3.34)
- c. The forward facing (from street view) elevations of a side entry garage shall appear as livable area by utilizing windows, wainscot, or other design elements compatible with the design of the structure. (Figures 3.32 and 3.35)
- d. Front-facing garages shall be offset no less than four (4) feet from the plane of a living area or other principal structure (porches) and the third or fourth car garage shall be setback a minimum of two (2) feet behind the main garage plane. The intent here is to soften garage dominance and provide for horizontal articulation. (Figure 3.36)
- e. Grouped driveways shall be separated by no less than twelve (12) inches. The use of decorative hardscape or colored paving may be used to provide the minimum prescribed separation. (Figure 3.37)

- f. When grouped driveways cannot be utilized, the Developer should consider the use of alternative garage types such as, single-bay tandem, side entry, or rear loaded (Figure 3.38)

D. Existing Development (20-78-3.II.D)

The intent of this section is to preserve or improve the integrity and character of existing communities by promoting consistency for proposed additions or alterations to existing buildings.

1. ACCESSORY BUILDINGS (20-78-3.II.D.1)

- a. Accessory buildings exceeding nine (9) feet in height or three hundred (300) square feet in area shall generally conform to the principal building in terms of colors, exterior building materials/finishes, and architectural style.

2. ROOM ADDITIONS AND BUILDING MODIFICATIONS (20-78-3.II.D.2)

- a. Additions to principal buildings shall utilize like colors, materials, and architectural style. (Figure 3.41)

3. COVERED PATIOS (20-78-3.II.D.3)

- a. Covered Patios shall be compatible to the principal building in terms of color and building materials. Roofing materials are not required to match that of the principal building. (Figure 3.39)
- b. Covered Patios shall be constructed on an approved concrete or similar foundation or slab.
- c. Patio/porch enclosures shall be reviewed by the Building Safety Division to determine whether or not it is considered a room addition or an "Arizona Room". For the purposes of this



Figure 3.34—Alternative driveway paving softens the appearance of large impervious surfaces.



Figure 3.35—Side-entry garage appears as livable area from street view.



Figure 3.36—Recessed third car garage softens perceived "garage dominance" from the street view.



Figure 3.37—Brick inlay reduces perceived massing for a grouped driveway.



Figure 3.38—Rear-loaded garages enhance the streetscape by reducing garage dominance.



Figure 3.39—This lattice patio cover provides an artful alternative.

Ordinance, “Arizona Rooms” are not required to match the existing structure in color, materials, or architectural style.

4. ATTACHED CARPORTS AND RV COVERS (20-78-3.II.D.4)

- a. Attached carports and RV covers shall generally conform to the principal building in terms of color and building materials. Roofing materials are not required to match that of the principal building. (Figure 3.40)
- b. Attached carports and RV covers shall be used only for the storage and protection of automobiles and recreational vehicles and shall not be used for ‘outdoor’ workshops or storage of miscellaneous household products.
- c. Attached carports and RV covers shall be constructed on a dust-free surface. However, all vertical members shall be secured to an approved base/foundation.

5. DETACHED RV COVERS (20-78-3.II.D.5)

- a. Detached Recreational Vehicle Covers are permitted, provided that:
 - b. Lot size shall be eighteen thousand (18,000) square feet or larger.
 - c. Limit of one (1) RV canopy per qualifying residential lot.
 - d. Structure shall be painted to match the dominant color of the principal structure. Wood structures shall not be permitted. Alternative building materials, or combinations thereof, may be considered provided the intent of this section is met.

e. Fabric or mesh sidewalls may be permitted provided they are securely attached to the structure. Materials shall be maintained through the life of the structure.

6. **GAZEBOS, RAMADAS, AND SIMILAR STRUCTURES** (20-78-3.II.D.5)

- a. Structure shall not be for storage purposes.
- b. Roof is constructed of lattice material or shall match roof material on principal building.
- c. Play structures, barbecues, chimneys (not attached to principal building), sport courts, and other like structures are exempt from Design Review.



Figure 3.40—This attached carport conforms to the principal building design and roofline.

TABLE 1: USEABLE OPEN SPACE

Lot Size	Minimum Amount of Useable Open Space Required
Lots less than 10,000 square feet	9% of gross project area
Lots 10,000 to 18,000 square feet	7% of gross project area
Lots in excess of 18,000 square feet	5% of gross project area

*If development includes a mixture of lot sizes, the percentage of Useable Open Space required shall be based on the percentage of lots in each lot size category.

Table 1

TABLE 2: STANDARD PLANS, MATERIALS AND COLORS

	Single-Family Units (Per Plat)			
	10 - 49	50 - 99	100 - 199	200+
Number of Roof Colors Re-quired	Minimum of 3 different roof colors.	Minimum of 4 different roof colors.	Minimum of 4 different roof colors.	Minimum of 5 different roof colors.
Number of Roof Styles¹ Re-quired	Minimum of 2 different roof styles.			
Number of Roof Material Types² Required	Minimum of 2 different roof material types.			
Number of Standard Plans Required	Minimum of 2 standard plans.	Minimum of 3 standard plans.	Minimum of 4 standard plans.	Minimum of 5 standard plans.

1 Roof styles include, but are not limited to, pitched, hip, flat, gambrel and mansard.

2 All asphaltic shingles shall be considered one material type. Clay and concrete tiles shall be considered one material type. However, alternating tile types (i.e. flat tile and mission tile) may be considered as different roof material types.

3. A minimum of three (3) elevations is required per standard plan (20-78-3.II.C.1.a)

Table 2

Chapter 4: Multi-Family Residential



I. INTENT

The purpose of this Chapter is to integrate safe and well-designed multi-family housing developments into the community. There are a diverse array of multi-family housing types including garden apartments, duplexes, large apartment, and condominium complexes. These housing types provide options for a growing segment in the community. Such areas typically serve as transitional land uses in close proximity to shopping, employment, and transit facilities. Such areas are also highly-visible, particularly along arterial roadways. The design guidelines attempt to deviate from a sea of repetitiveness through color and material options, strong architectural design and detailing, landscaping and site enhancements and the provision of safe and practical recreational opportunities.

A. APPLICABILITY (20-78-4.A)

The Multi-Family Residential Design Review Guidelines shall apply to the following multi-family residential development in all zoning districts:

1. All proposed multi-family residential development including attached single-family residential development (three (3) or more units per building), for new construction.
2. Alterations or additions to any existing building.



Figure 4.1—Staggered design elements provide interest along the street.



Figure 4.2—Building orientation, awnings and window and balcony placement promote energy efficiency.



Figure 4.3—Building orientation creates "parking nodes" as opposed to larger parking fields.

II. MULTI-FAMILY RESIDENTIAL DESIGN STANDARDS (20-78-4.II)

A. SITE DESIGN (20-78-4.II.A)

1. SITE LAYOUT AND ORIENTATION (20-78-4.A.1)

- a. The site design should incorporate natural amenities and features into the development template such as topography, prominent view corridors, washes, and significant vegetative stands to enhance the character of the development.
- b. The building placement and orientation along the street line should be staggered, angled, or curved to provide modulation and interest. (Figure 4.1)
- c. The building design shall account for predominant sun and environmental factors through building orientation, window and door placement, landscaping, awnings, canopies, window treatment (i.e. glazing) and other appropriate design solutions. (Figure 4.2)
- d. On-site parking areas shall be broken into smaller "nodes" and separated by landscape buffers and/or building placement to reduce the visual and environmental impact of large, impervious parking fields. (Figure 4.3)
- e. The transition between multi-family residential and existing single-family residential areas shall be enhanced to achieve maximum compatibility through design solutions including but not limited to, building orientation, use of clerestory windows for upper stories, wall height, and landscape buffering.

2. ACCESS AND CIRCULATION (20-78-4.A.2)

- a. Vehicle circulation and parking should be internalized within the development. Circulation design (scale and location) should be compatible with the adjacent neighborhood.
- b. The development should provide pedestrian access to adjacent non-residential areas by incorporating wall and landscape penetrations into the pedestrian circulation element.
- c. The development shall provide a continuous walkway from each abutting right-of-way (ROW) to the leasing office. The walkway shall be separated from all vehicular traffic movements except where drive aisle crossings are necessary.
- d. The on-site pedestrian circulation system shall link the various site amenities and components (i.e. parking fields, play areas, clubhouse, pools, recreation center, refuse enclosures). (Figure 4.4)
- e. Decorative materials should be used to clearly delineate pedestrian walkways. The use of hardscaping for walkways is encouraged when it is not in conflict with ADA requirements.
- f. Pedestrian walkways/paths traversing on-site vehicle drive aisles shall be distinguished with an alternative hardscape material such as, pavers, Streetprint™ and patterned, stamped, or colored concrete. Exposed aggregate is not generally viewed as an acceptable material.
- g. On-site vehicular circulation should be designed to reduce pedestrian/vehicle conflicts through the incorporation of appropriate traffic calming measures.



Figure 4.4—Open spaces and site amenities are linked by a lighted pedestrian path.



Figure 4.5—Shaded pedestrian refuge area provides respite and opportunities for gathering.



Figure 4.6—This alternative parking canopy continues the thematic character of the development.



Figure 4.7—Recessed garage doors provide additional façade articulation.



Figure 4.8—Palm-lined median provides a "splashy" entry into the development.



Figure 4.9—Handsome covered entry punctuates the thematic character of the development.

- h. Site design should encourage alternative modes of transportation. Such design considerations include connections to existing off-site trails/paths and bikeways, bicycle parking and storage areas, and designs facilitating the use of mass transit.
- i. The on-site pedestrian circulation system shall incorporate regular and evenly distributed placement of shaded, well-lit bench seating and other pedestrian refuge areas. (Figure 4.5)
- j. Dead end drive aisles are discouraged.

3. PARKING AREAS (20-78-4.A.3)

- a. Carports and detached garages shall incorporate curved and pitched roof elements of a design similar to the principal structure on the site. (Figure 4.6)
- b. Garage doors should appear to be set into the walls rather than flush with the exterior wall. (Figure 4.7)
- c. Ground-level parking structures should be screened from view by two (2) or more of the following:
 1. Walls containing architectural details, such as banding or similar architectural embellishments incorporated into the building design.
 2. Trees and shrubs.
 3. Grillwork and/or Greenscreens incorporating decorative metal artwork or panels.

- d. Raised planting areas, with a minimum interior dimension of five (5) feet should be used to separate double-loaded parking areas.

4. PROJECT ENTRY AND CHARACTER

(20-78-4.A.4)

- a. A combination of ornamental landscaping, landscaped medians, water features, architectural monuments, decorative walls, signs, and/or enhanced paving shall be incorporated into the primary themed entry as accent features. Three (3) or more of the following elements shall be incorporated into the Primary Themed Entry of the development:
 - 1. Landscaped median. (Figure 4.8)
 - 2. Identifying building form such as a covered entry. (Figure 4.9)
 - 3. Unique pedestrian scale lighting or bollards.
 - 4. Prominent architectural features or monuments, such as a trellis or arbor. (Figure 4.10)
 - 5. Ornamental gates and/or decorative walls.
 - 6. Water features. (Figure 4.11)
 - 7. Other feature (s) as approved through Site Plan Review.
- b. Project entry features shall be reflective of the overall architectural identity and character of the project.
- c. The entry throat into the development shall be



Figure 4.10—Entry feature incorporates water feature, trellis, accent landscaping and a decorative gate.



Figure 4.11—This entry incorporates a decorative water feature into a landscaped roundabout.



Figure 4.12—The entry throat into the complex is distinguished with hardscaping.

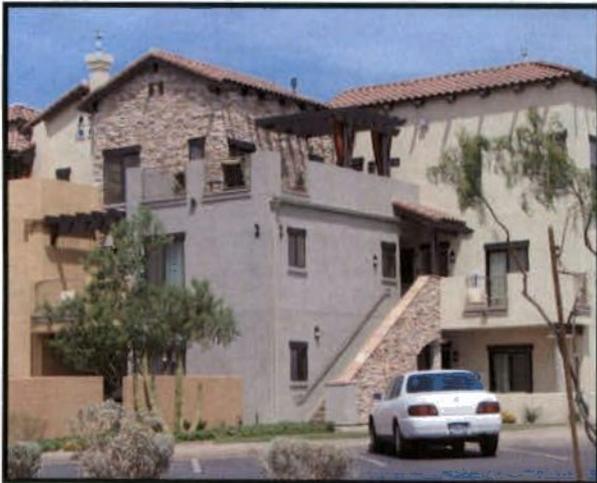


Figure 4.13—Multiple sightlines provide “eyes” onto the parking area.



Figure 4.14—Pathways are well delineated, lighted and visible from units.



Figure 4.15—Enclosed passive open space areas are lighted and visible from balconies and windows.

distinguished with hardscape materials such as pavers, Streetprint, and patterned, stamped or colored concrete. Exposed aggregate is not generally viewed as an acceptable material. (Figure 4.12)

5. CRIME PREVENTION THROUGH ENVIRONMENTAL DESIGN (20-78-4.A.5)

- a. Entrances into parking lots should be defined by landscaping, architectural design, or monitored by a guard.
- b. All doors that open to the outside should be well lit and visible from the street, parking area, or neighboring units.
- c. Parking areas, pedestrian walkways, elevators, stairwells, and recreation areas should be visible from multiple perspectives. (Figure 4.13)
- d. Refuse enclosures should not create blind spots or hiding areas.
- e. Buildings should be sited so that the windows and doors of one unit are visible from another.
- f. All four (4) facades should have windows.
- g. Building entrances should be accentuated by architectural elements, lighting, and/or landscaping.
- h. Landscape design should not preclude visibility or surveillance capabilities to common areas and units.
- i. For safety purposes, exterior doors should be designed with a solid core, peep holes, deadbolt

locks, and reinforced with strike plates.

- j. "Hostile Vegetation" such as Ocotillo, Cacti or other plant types that discourage pedestrian movement or vandalism should be placed under windows or adjacent to long, remote expanses of perimeter walls.
- k. Pedestrian walkways should be safe, visually attractive, and well defined by landscaping and lighting. (Figure 4.14)
- l. All areas including pedestrian walkways/paths, active play areas, and open space shall be adequately lighted and designed to assure safety and security. All lighting shall be properly shielded from adjacent properties. (Figure 4.15)
- m. Common open spaces should be conveniently located for the majority of units. Children's play areas should be visible from as many units as possible. (Figure 4.16)

6. OPEN SPACE (20-78-4.A.6)

- a. The development shall provide both passive and active recreation areas such as barbeque grills, swimming pools, tennis courts, and exercise courses. (Figure 4.17)
- b. Active play areas should be located in centralized locations. Care should be taken to mitigate noise generation when locating adjacent to lower density uses and on-site residential units.
- c. Common useable open space and recreation areas shall be provided at a prescription of one hundred fifty (150) square feet per bedroom. All such areas shall be unencumbered and be a minimum width of twenty (20) feet.



Figure 4.16—Common pool is visible from units and through wrought iron fence.



Figure 4.17—This exercise course provides an alternative active recreational amenity.



Figure 4.18—Private balconies are provided for each unit.



Figure 4.19—Clubhouse promotes social interaction and fitness.



Figure 4.20—Pool with cool decking and shaded areas.



Figure 4.21—Highly visible tennis court.

- d. Private balconies shall be provided for each unit. Such space shall have an unencumbered area of at least twelve (12) feet in diameter for ground floor units and eight (8) feet in diameter when provided by a balcony for upper floors. (Figure 4.18)
- e. Useable open space areas and on-site amenities should be distributed equitably throughout the development. (Figure 4.19)
- f. The minimum amount of project amenities shall be provided per the following schedule:

<u>Total Units</u>	<u>Amenities Required</u>
Less than twenty (20) units	One (1) amenity
20-99 units	Three (3) amenities
100-179 units	Four (4) amenities
180-259 units	Five (5) amenities
260 units +	Six (6) amenities plus second pool

The following amenity options are representative of the size, type and scale of amenities acceptable to meet the schedule above. The listing is not intended to represent all amenity options. Alternative amenity options may be considered during the Site Plan Review process.

1. Swimming pool, Ramada and cool deck perimeter; (Figure 4.20)
2. Jacuzzi area with Ramada;
3. Ramada or trellis feature with barbeque and shaded seating areas;
4. Tot lot / Jungle Gyms;

5. Regulation size volleyball court;
6. Other hard surface multi-use court (60' x 60' minimum);
7. Lighted regulation size tennis court and/or lighted regulation size racquetball court; (Figure 4.21)
8. Weight room;
9. Par course;
10. Demonstration garden;
11. Putting green.

7. LANDSCAPING (20-78-4.A.7)

- a. Landscaping shall be provided at the foot of buildings to soften the transition between paved areas on the ground plane and building materials on the vertical plane. (Figure 4.22 and 4.23)
- b. Potted plants, ornamental landscaping, and architectural features should enhance courtyards, plazas, and other gathering areas. (Figure 4.24 and 4.25)
- c. Flowering trees and shrubs should be used to provide color and accentuate entrances and activity areas. (Figure 4.25)
- d. Landscaping should be used to define areas such as building entrances, key activity hubs, focal points, and the street edge. (Figure 4.26)



Figure 4.22—Landscaping at the foot of the building to soften the appearance.



Figure 4.23—Varying heights of landscaping softens the façade and reduces massing.



Figure 4.24—Attractive trellis feature, natural materials and landscaping to accentuate a courtyard.



Figure 4.25—Trellis and colorful landscaping artfully accentuate courtyard activity area.



Figure 4.26—Landscaping used as a means of “wayfinding” directing residents to the clubhouse.

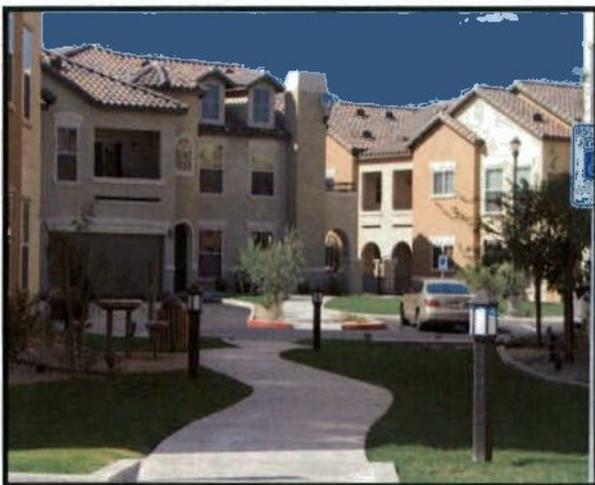


Figure 4.27—Decorative bollards provided along a pedestrian connection.

- e. Use of native vegetation and low water plants shall conform to the Phoenix Active Management Areas Low Water Using Plant List, and be planted in conjunction with an efficient water system.

8. LIGHTING (20-78-4.A.8)

- a. Lighting shall be provided within outdoor spaces to provide visual interest as well as a security function.
- b. Decorative theme lighting, accent lighting, or lighted bollards shall be placed along pedestrian connections and in useable open space areas to improve visibility and safety. (Figure 4.27)
- c. Gated entry features shall use integrated decorative lighting to enhance walls and signage.
- d. Lighting design shall be compatible with the building architecture, with fixtures of a consistent type and size within the development. (Figure 4.28)

9. WALLS AND FENCES (20-78-4.A.9)

- a. Entry signage, refuse enclosures, perimeter walls, and other appurtenances shall be consistent with the thematic character of the development through the use of common integrative elements such as colors, materials, and architectural style. (Figure 4.29)
- b. Walls adjacent to retention areas, trails, parks or other useable open space areas shall incorporate regular undulation or variation in materials. View fencing is strongly encouraged.

c. Perimeter walls shall integrate at least three (3) of the following treatments:

1. Integration with one or more of the following: inlays, materials or color accents, capping, decorative pilasters, wrought iron, planters, or other approved feature that adds visual interest. Strong geometric patterns are strongly discouraged. (Figure 4.29)
2. Incorporation of raised planters.
3. Incorporation of decorative stone or masonry pillars with caps.
4. Incorporation of decorative wrought iron trellises or artistic features in context with the area.
5. Other alternative design as approved during the Site Plan Review Process.

10. REFUSE ENCLOSURES (20-78-4.A.10)

a. Refuse enclosures shall be:

1. Internalized and oriented away from arterial streets and abutting residential areas.
2. Architecturally compatible through use of common colors, materials, and design. (Figure 4.30 and 4.31)
3. Placed in convenient, proximate locations for tenant convenience and pickup service.

11. CLUSTER MAILBOXES (20-78-4.A.11)

a. Cluster mailboxes shall be:



Figure 4.28—Uniform application of wall sconces add interest to the building architecture.



Figure 4.29—Wall is consistent with the thematic character and allows surveillance into the parking areas.



Figure 4.30—Refuse enclosure continues the complex theming.



Figure 4.31—Trash enclosure blends into the complex with color and design treatments.



Figure 4.32—Mailbox utilizes consistent roof pitch and materials, trim, and base color.

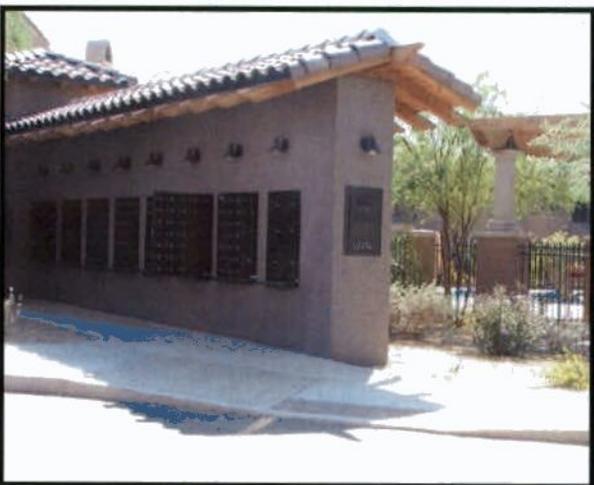


Figure 4.33—Mailbox appears as an extension of the building.

1. Located in a central and well-lit area and located no closer than three (3) feet from any residential privacy wall.
2. Cluster mailboxes should be screened or designed in a manner consistent with the thematic character of the development through the use of common integrative elements such as color palette, building materials, and roof pitch. (Figure 4.32 and 4.33)

12. UTILITY AND MECHANICAL EQUIPMENT (20-78-4.A.12)

- a. Noise generating equipment should be located away from residential units, public spaces, and pedestrian areas.
- b. Backflow preventers for landscape irrigation and domestic water shall not be located at visually prominent locations and shall be well-screened with shrubs, berming, or low-screen walls.
- c. Roof access ladders and roof drains/dowspouts shall be internalized within the building.
- d. Mechanical equipment, ground and roof mounted, shall be screened from public view. (Figure 4.34)
- e. Utility screening requirements including Service Entrance Sections are located in Chapter 5 Supplementary Standards.

B. ARCHITECTURAL FORM (20-78-4.B)

1. BUILDING MASS/ARTICULATION (20-78-4.B.1)

- a. The visual impact of large monolithic structures shall be minimized by creating a cluster of smaller buildings or the appearance of a series of smaller buildings. (Figure 4.35 and 4.36)
- b. All buildings shall integrate variations in exterior walls in depth and direction. Use building pop-outs, arches, and upper-story balconies to break up massing.
- c. Minimize the bulk and appearance of structures through the use of sloping rooflines consisting of varying roof heights, directions, and shapes. Incorporate varied building heights at the street. Avoid massive straight rooflines with flat appearances. (Figure 4.37)
- d. Multi-story buildings (exceeding two (2) stories) should step back from the street and adjacent properties. Incorporate a tiered design with one (1) or two (2)-stories in the front and increasing to multiple stories in the rear. (Figure 4.36 and 4.38)
- e. Building placement and orientation shall vary for design interest and visual relief.
- f. The use of landscaping as a design element is encouraged to reduce building mass. Landscaping should accentuate and compliment architecture. Explore the use of berms or raised planters against a large structure.
- g. Multiple design solutions shall be integrated into the building to provide visual interest and variation. Such design solutions may consist of projecting private balconies, building wall recesses, pop-outs, varied wall planes, dormers, mansard roof, decorative window treatment, brick or stone veneers, color changes, and other architectural elements. (Figure 4.39)
- h. Horizontal façades longer than thirty (30) feet



Figure 4.34—Utility is screened from street view by a wall and landscaping.



Figure 4.35—Three-story complex reduces visual impact through articulation, balconies, and color contrast.



Figure 4.36—Building integrates varying shapes and colors to reduce massing.



Figure 4.37—Varying roof height and direction coupled with color and form reduces the apparent bulk.

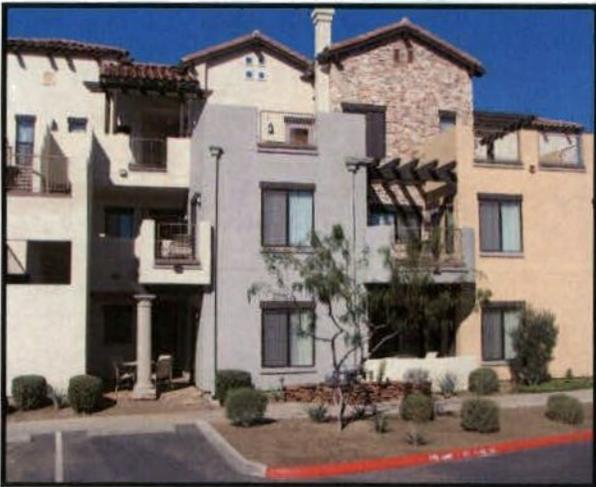


Figure 4.38—Third-story units are stepped back.



Figure 4.39—Building integrates balconies, arches, dormer windows, rounded corners and popouts.

shall be articulated into smaller sections utilizing at least four (4) of the following methods: (Figure 4.40 and 4.41)

1. Varied building heights.
2. Different materials used on first floor.
3. Different window types.
4. Different colors.
5. Offsets.
6. Projecting roofs (minimum of twelve (12) inches).
7. Recesses.
8. Bay windows.
9. Variable roof forms or orientation.
- i. Building facades should incorporate design elements that enhance the pedestrian environment.
- j. Deep roof overhangs are encouraged to create shadow and add depth to facades.
- k. Shade structures and screening shall be architecturally and functionally integrated with the established design theme.
- l. Garages and storage/utility areas shall be architecturally integrated into the established design theme. (Figure 4.42)

m. Exterior stairways shall be designed with at least one ninety (90) degree angle turn from floor to floor.

n. Downspouts shall be internalized.

2. FENESTRATION (20-78-4.B.2)

a. Windows shall employ design details as appropriate to the architecture, such as mullions, arched windows, shutters/faux shutters, window surrounds, awnings, and canopies to break the scale of the façade into smaller components. (Figure 4.43)

b. Unifying architectural elements shall be used to carry a window pattern across a façade, such as a common sill or header line. (Figure 4.44)

c. Shaped frames and sills shall be used to enhance openings and add additional relief. They should be proportional to the glass area framed (i.e. a larger window should have thicker framing members). (Figure 4.45)

d. Windows shall be provided in facades facing streets, comprising at least twenty (20) percent of the façade area.

e. Windows and balconies should be designed so that visual and auditory intrusions on private outdoor space of other units or adjacent development are minimized. (Figure 4.45)

f. Window and door type, material, shape, and proportion shall complement the architectural style of the building.



Figure 4.40—Building integrates arches, stone veneer, varying roof planes, articulation and distinctive colors.



Figure 4.41—Massing reduced by brick accents, ornamentation, distinctive colors, capping, and articulation.



Figure 4.42—Garages are integrated into the design character.



Figure 4.43—Window surrounds, sills and proportional faux shutters add interest to the windows.



Figure 4.44—Prominent and substantial windows advance the architectural lines across the façade.

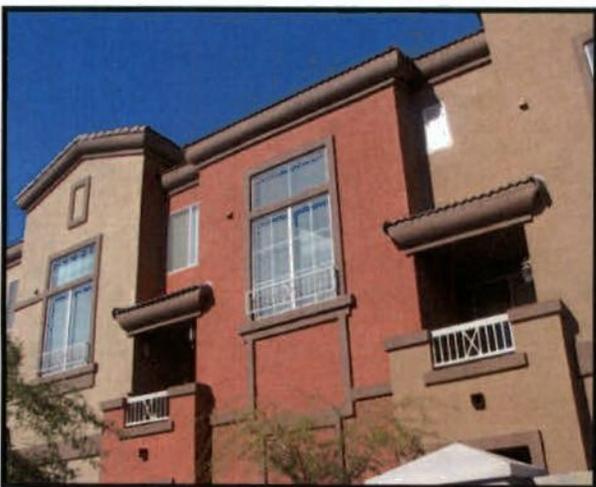


Figure 4.45—Shaped frames, sills, mullions and ornamentation add relief and interest to the façade.

3. COLORS AND MATERIALS (20-78-4.B.3)

- a. The color and material palette(s) shall be appropriate to the context. (Figure 4.46)
- b. Where appropriate to the architectural style, materials and textures shall vary between the base and body of a building to break up large wall planes and add visual base to the building.
- c. Material changes shall occur at intersecting planes, preferably at inside corners of changing wall planes or where architectural elements intersect, such as a chimney, pilaster, projection, or pop-out.
- d. All developments shall provide a minimum of three (3) distinct color and material palettes. Each palette shall be distributed evenly throughout the project. (Figure 4.47 and 4.49)
- e. All developments shall provide substantial accent materials, such as stone, brick, tile or other similar materials to add texture and visual interest to all building elevations. Accent materials shall not be limited to typical wainscot height (3-4 feet) and may include all or most of the following: (Figure 4.48)
 1. Stone clad or concrete columns as patio/porch supports.
 2. Integrated corbels (wood or treated synthetic materials) placed under eaves at corner locations or throughout project.
 3. Stone wainscot at varied heights with accented caps.
 4. Integrated lighting sconces beyond individual porch lights.

5. Scored stucco areas where color changes or the addition of accent materials is not practical.
 6. Pop-outs and other projections of materials other than wood frame and stucco. (Figure 4.50)
 7. Decorative wrought iron accents in the form of stairway railings, gates into entry and amenity areas and patio/porch accents.
 8. Large raised planters or pots placed in key areas to break up long walkways or parking areas.
 9. Other creative accent materials and/or methods presented and approved during the Site Plan and Design Review Process. (Figure 4.50)
- f. All accessory structures (i.e. garages, parking canopies, gazebos, etc.) shall conform to the dominant design theme of the development. For example:
1. Stone clad or concrete columns under gazebos and along pool areas.
 2. Decorative wrought iron pool fencing and gates.
 3. Timber trellises/gazebos with integrated corbels.
 4. Iron or thematic wood/timber accents such as trellises, 'brows', or other interest items around or above garage doors.
 5. Color matched parking canopies. (Figure 4.51)
 6. Garage door colors that allow doors to blend in.



Figure 4.46—The use of warm and subdued colors is appropriate to this desert environment locale.



Figure 4.47—Multiple color palettes are utilized.



Figure 4.48—Substantial use of stone, strong cornice treatment, capping, and other details add prominence.



Figure 4.49—A strong, bold palette with varying shapes and architectural lines creates "splash" and interest.



Figure 4.50—The use of popouts, projections, capping and strong color distinction liven a side façade.



Figure 4.51—Alternative parking canopies are consistent with the thematic character.

Chapter 5: Supplementary Standards



I. INTENT (20-78-5.1)

The Design Review Manual provides overarching principles and guidelines for all development types located within the City. However, there are some areas in the City that warrant special attention due to the presence of unique built and environmental characteristics, heritage, uses or visibility. The Lake Pleasant Parkway Corridor enjoys spectacular vistas and serene desert lands. This Chapter attempts to recognize these environmental qualities and initiate the corridor as a signature parkway with theming playing on the presence of Lake Pleasant. A different character area is the historic heart of the City - Old Town. The interests in this area are different. The Manual promotes the appropriate integration of new development in context with its historic character. Therefore, to these ends, Chapter 5 is intended to provide supplemental standards to address the unique needs in these areas. The relevant standards in this Chapter then will be used in conjunction with the applicable standards contained in Chapters 2-4 of this manual.

Finally, this Chapter contains overlay standards for all development types in regards to the location and screening of utility boxes, pedestals, and related appurtenances. The concentration of such facilities at high profile locations can detract from the aesthetic quality of the community. The standards and details are located in one Chapter for ease of use.

This Chapter is therefore organized as follows:

1. Old Town
2. Lake Pleasant Parkway Corridor
3. Utility Cabinets

II. Old Town

II. Old Town



II. OLD TOWN

A. INTRODUCTION

The Old Town area is of special interest to the City as it represents the City's history and charm. Development in Old Town must be consistent with the existing flavor. The City's vision for this area is a commercially-based core with supporting residential uses. Together, these uses will create a walkable community and comfortable streetscape taking advantage of the existing narrow streets, mature trees and dense development pattern. Though restoration of existing structures is preferred, new development is certainly welcome provided it works in concert with the City's vision for Old Town.

The dominant character of Old Town should be that of a retail-oriented, commercial environment with an active street edge that is inviting to pedestrians, while also accommodating to automobiles. Development should include a mix of building types including older structures and compatible, newer ones. Each building should reflect the style and character of its own time, but should also contribute to a sense of visual continuity for the area.

B. APPLICABILITY

These guidelines shall apply to the design and construction of new commercial/retail, office, multi-family residential, or mixed-use development in all zoning districts in the Old Town area. These guidelines shall also affect exterior alterations, additions to or the rehabilitation of any existing buildings in Old Town. The guidelines are broken down by concept, applying to *all* land uses in the Old Town area unless otherwise stated. Map 5.1 depicts the affected area of Old Town.

Old Town Special Area





Figure 5.1—Adjacent buildings share features such as brick facades and arched windows.



Figure 5.2—Primary building entrances are oriented directly toward the street and sidewalk.

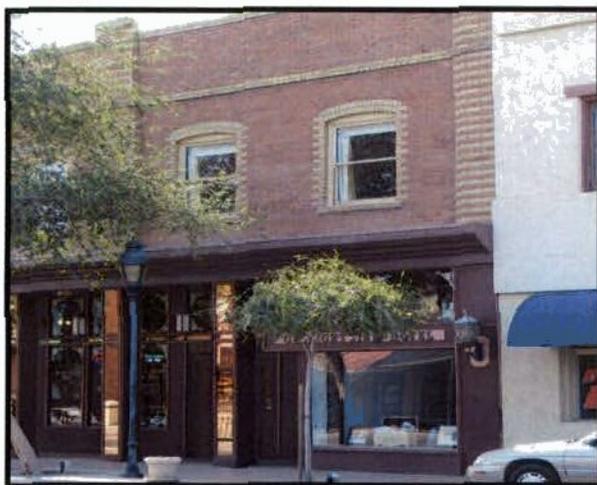


Figure 5.3—Building entrances in proportion with the overall façade.

C. SITE DESIGN (20-78-5.II)

1. SITE LAYOUT AND ORIENTATION (20-78-5.II.C.1)

- a. Avoid designs that significantly interrupt the predominant scale of the street.
- b. Development shall be compatible with area buildings in scale, massing, architectural features, solid-to-void relationships, and materials. (Figure 5.1)
- c. Primary building entrances shall be oriented directly toward the street and sidewalk. (Figure 5.2)
- d. Building entrances should appear historically accurate.
- e. Building entrances shall be in scale with the overall façade. (Figure 5.3)
- f. Entrances to residential units shall be oriented toward open spaces whenever possible. First floor entries oriented toward alleys, parking lots, or streets with heavy traffic should be avoided.
- g. Buildings should be oriented in a manner that promotes strong physical connections between public spaces and neighboring development.
- h. New buildings shall be oriented parallel to their lot lines in order to respect the Old Town street grid.
- i. The area between a building and its front setback/build-to line should be used for outdoor dining, entryways, sidewalk sales, and other

pedestrian friendly activities.
(Figure 5.4 and 5.5)

2. ACCESS AND CIRCULATION (20-78-5.II.C.2)

a. General

1. Streets shall be designed for safe and comfortable pedestrian activity, incorporating amenities such as trees, furniture, and lights.
2. The use of special paving and landscape treatments shall provide distinctive accents to areas with a unique identity, such as important intersections and notable historical and cultural landmarks.

b. Sidewalks

1. Sidewalks and pathways shall be designed to promote physical continuity and connection.
2. Ample width and design for universal access shall be provided along all pathways and sidewalks.
3. Clear pedestrian passage shall be maintained along public sidewalks without conflicts with utility equipment, street trees, street amenities, or other potential interferences. (Figure 5.6)

c. Transit Stops

1. Transit stops should be designed as iconic and functional amenities. (Figure 5.7)
2. Transit stops shall be provided with seats and shelter to increase safety and comfort.



Figure 5.4—Outdoor dining area encourages activity and projects "life" onto the street.



Figure 5.5—An outdoor dining space is created between a building and the sidewalk.



Figure 5.6—A sidewalk provides accessibility and opens to a well-lit seating area with trees



Figure 5.7—Artful transit stop provides function and thematic interest.



Figure 5.8—Street furniture creates an inviting respite for pedestrians.



Figure 5.9—Awnings and canopies on old flat facades provide pedestrian coverage and interest.

3. PARKING (20-78-5.II.C.3)

- a. Parking lots/structures shall be located behind buildings or interior to a block to support pedestrian-oriented streets.
- b. Parking lots shall be screened. Such screening shall be consistent with the surrounding area.

4. PEDESTRIAN AMENITIES/PUBLIC PLACES (20-78-5.II.C.4)

- a. Adequate bicycle parking facilities shall be provided at appropriate locations commensurate with the impact of the use.
- b. Street furniture, including benches, refuse receptacles, and planters/seat walls should be placed at frequent intervals along sidewalks. (Figures 5.8 and 5.25)
- c. Street furnishings, such as drinking fountains, public restrooms, bicycle racks, and kiosks should be integrated, where appropriate, into the Old Town development template.
- d. Awnings over entrances, decorative features on walls, landscaped planters, trees, and other similar amenities shall be used, wherever possible, to enhance pedestrian areas.
- e. Buildings adjacent to the sidewalk should provide overhead cover in the form of canopies, awnings, and overhangs where there is an insufficient or immature street tree canopy. (Figures 5.9 and 5.26)
- f. Gathering areas with fountains, artwork, seating, and enhanced landscaping should be provided in centralized locations. (Figure 5.10)

5. OTHER THEMATIC ELEMENTS (20-78-5.II.C.5)

a. Public Art

1. Public art should be considered in areas with a unique identity or to celebrate the entrance to a public place (e.g. gateways, public places, buildings, etc.)
2. Public art that celebrates local history and culture shall be encouraged. (Figure 5.11)
3. Public art shall be made accessible, drawing people through public spaces, but not impeding pedestrian flows.

- b. Covered mailboxes that look like an add-on and not integrated into the building's architecture are prohibited.

6. LANDSCAPING (20-78-5.II.C.6)

- a. Landscaping shall be used to define and delineate public streets.
- b. Landscaping shall not be used as a substitute for building enhancements.
- c. Landscaping should accentuate architecture utilizing trees with ornamental tree grates, shrubs, flowers, and ground cover.
- d. Tree canopies should allow sufficient room for growth without impacting building elements or impeding pedestrian and other modes of circulation.
- e. Street trees should define the street and sidewalk, emphasizing the consistent use of tree



Figure 5.10—A water fountain serves as a centralized gathering place.



Figure 5.11—Public art celebrating local history is surrounded by seat walls.



Figure 5.12—Street trees are used to accentuate pedestrian crossings and provide shade.



Figure 5.13—Pedestrian-scale street lights coordinate with a clock feature and bollards.



Figure 5.14—Decorative kiosk directing pedestrians to landmarks and attractions.

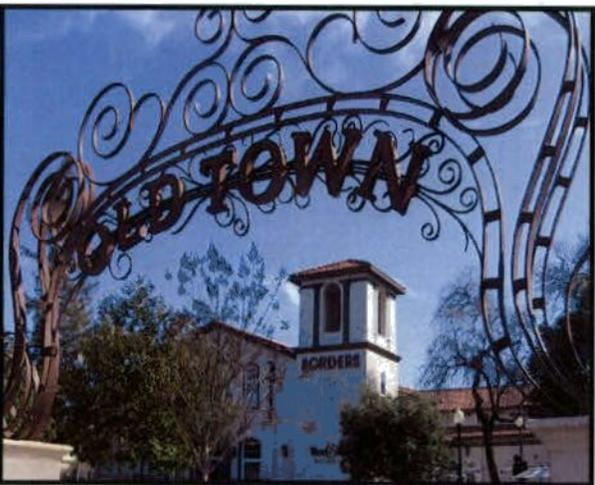


Figure 5.15—Thematic signage announcing entry into a special district.

species, size and spacing along a street to create a pleasant rhythm, and reinforcing the space of the street. (Figure 5.12)

- f. Tree grates should be used in areas with considerable commercial and pedestrian activity to increase sidewalk width, reduce safety hazards, and prevent compaction of the root-ball.

7. LIGHTING (20-78-5.II.C.7)

- a. Building mounted light fixtures shall be compatible with the building architecture.
- b. Street light fixture design shall be coordinated with other street amenities to unify areas with a distinct identity. (Figure 5.13)
- c. Low-level lighting of buildings and landscaped areas is encouraged.

8. REFUSE ENCLOSURES (20-78-5.II.C.8)

- a. Refuse enclosures shall be compatible with the overall architectural design of a project.

9. SIGNAGE (20-78-5.II.C.9)

- a. Signs and graphics should be used to aid pedestrian and vehicular orientation and way finding, and to identify special districts, historical and cultural landmarks, and local attractions. (Figure 5.14)
- b. Signage shall coordinate with the architectural character.
- c. Thematic signage should be used in key locations to define areas of interest. (Figure 5.15)

D. ARCHITECTURAL FORM (20-78-5.II.D)

1. BUILDING MASS (20-78-5.II.D.1)

- a. New construction shall appear similar in mass and scale to historic structures in the area.
- b. All structures should have exposed foundation walls and bases, whenever possible.
- c. Multi-Family Residential
 1. Additions to existing residences shall be positioned in a manner so as to not dominate the existing building. (Figure 5.16)

2. BUILDING FORM (20-78-5.II.D.2)

- a. Secondary or non-entrance building façades exposed to public view should compliment the primary façade.
- b. The use of projecting private balconies, building wall recesses, building pop-outs, varied wall planes, arched dormers, mansard roofs, decorative window treatments, brick or stone veneers, color changes, and other architectural elements is encouraged. (Figure 5.17)
- c. Storefronts should be continuous along the streetscape to encourage pedestrian movement. (Figure 5.18)
- d. Windows and doors should make up the majority of ground floor façades, creating inviting pedestrian scale streetscapes. Windows and balconies should be used on upper floors to create depth and articulation. (Figure 5.19)



Figure 5.16—Newer residential unit is stepped back without dominating the existing streetscape.



Figure 5.17—Building integrates projecting balconies, brick veneer, and awnings.

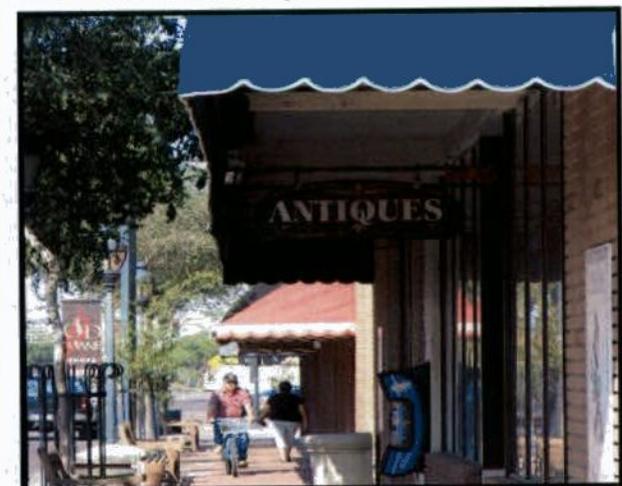


Figure 5.18—Continuous storefronts encourage pedestrian movement.



Figure 5.19—Balconies define pedestrian scale and add building articulation.



Figure 5.20—Through adaptive reuse, old-schoolhouse is utilized as a historical museum.



Figure 5.21—Buildings of various ages maintain a sense of continuity.

- e. Awnings/canopies shall appear integrated into the design of the building they are a part of in scale, form, material, and details.

3. ARCHITECTURAL CHARACTER (20-78-5.II.D.3)

a. Existing Buildings

1. The distinguishing qualities and features of a historic or architecturally significant building, structure, or site shall be maintained.
2. The recycling of historically and architecturally significant Old Town buildings shall be encouraged through adaptive reuse. (Figure 5.20)

b. New Buildings

1. The replication and contemporary interpretation of older historic styles for new buildings shall be encouraged. Contemporary interpretations of traditional buildings, which are similar in scale and overall character to those seen historically in Old Town and the region shall be encouraged. (Figure 5.21)

E. COLORS/MATERIALS (20-78-5.II.E)

1. OVERALL DESIGN STANDARDS (20-78-5.II.E.1)

- a. Materials shall appear similar to those used historically in the area (brick, concrete block, etc).
- b. Façade elements shall be aligned along the block, including window sills, moldings, and building cornices. (Figure 5.22)

2. RAIN GUTTERS (20-78-5.II.E.2)

- a. Rain gutters shall follow the architectural lines of the building or be encased within the building walls. (Figure 5.23)
- b. Pedestrians shall be protected from water run-off from roofs and balconies through the use of rain gutters, or the proper location of walkways.

3. WALL MATERIALS & COLORS (20-78-5.II.E.3)

- a. Upon renovation of historic structures, original materials shall be repaired and retained whenever possible.

4. WINDOWS & DOORS (20-78-5.II.E.4)

- a. Windows shall be trimmed with painted wood, anodized metal, shutters, wrought-iron, or a material similar to that historically used in the area.
- b. Window dimensions shall be similar to those traditionally used (dividing frame elements such as mullions, etc.). (Figure 5.24)
- c. Windows shall have multiple panes, divided lights, exposed lintels, trim casing, and/or recessed frames.
- d. Large areas of glass shall be recessed or placed in shadow of projected overhangs.
- e. Windows and doors on ground level storefronts shall utilize transparent glass to insure visibility and minimize glare.



Figure 5.22—Façade elements are substantially aligned along the block.



Figure 5.23—Rain gutters follow the architectural lines of the building.



Figure 5.24—Window proportions are consistent.



Figure 5.25—Thematically consistent seatwall and signage are integral pedestrian ingredients.



Figure 5.26—Building awnings provide proportion and scale and serve as an amenity for pedestrians.

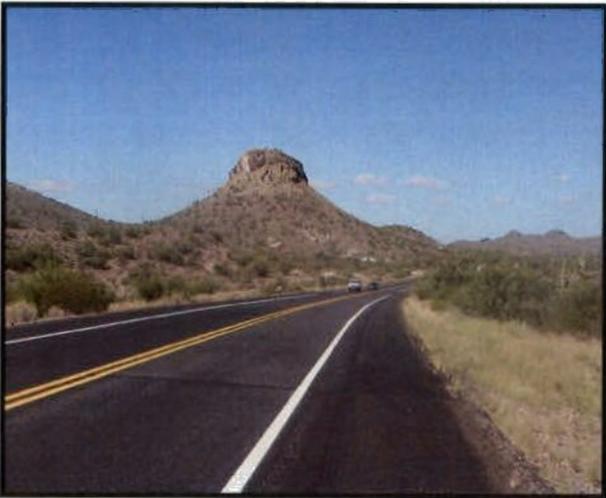
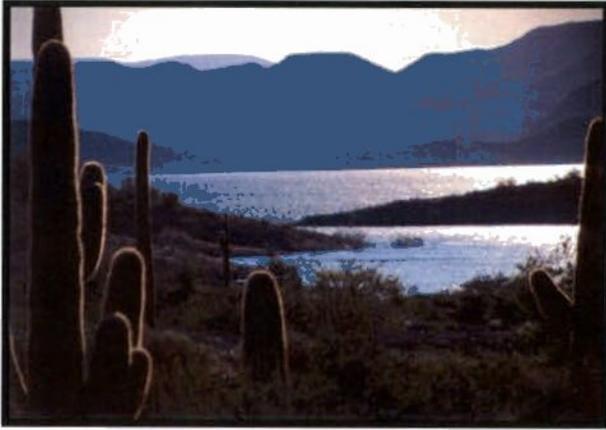


Figure 5.27—Distinguished walkway promotes safer crossing conditions for pedestrians.



III. Lake Pleasant Parkway

III. Lake Pleasant Parkway



III. LAKE PLEASANT PARKWAY

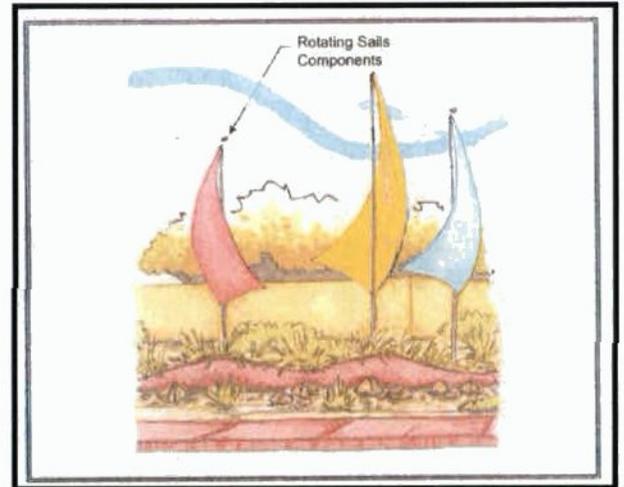
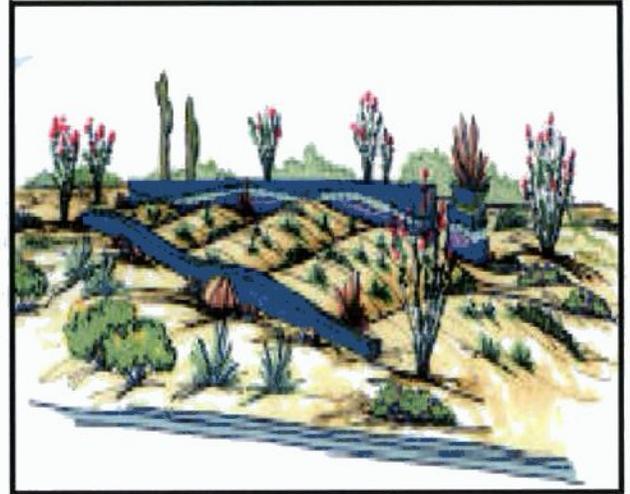
A. INTRODUCTION (20-78-5.III.A)

During recent plans to widen the Lake Pleasant Parkway, the City of Peoria determined that it had an opportunity to define the parkway as a signature street. The streetscape theme preparation was an opportunity to accentuate the natural beauty of the region and highlight the city's significance in the Valley. The purpose of this chapter is to document the design expectations for the development of landscape aesthetic treatments for the Lake Pleasant Parkway in a clear format. This chapter advances and encases the concepts accepted by the City Council in the 2005 *Lake Pleasant Parkway Design Theme Manual*.

Implementation of this collection of standards and guidelines – collectively, a design theme - will enable the Lake Pleasant Parkway median to visually cohere throughout its entire length. The median treatment alone will elevate the aesthetic experience for this signature roadway. This chapter promotes the extension and integration of this design concept to the edges as private development occurs. Clearly, as each development may have its own character and thematic profile, the "expression" of this design concept may fluctuate. The goal is to create the iconography that will function throughout the City of Peoria as a timeless identifier. The principles and guidelines contained in this chapter are intended to meet the following goals and objectives:

1. Create a signature transportation corridor for the City with a strong identity implemented through creative design;

2. Create a set of written guidelines clearly outlining the Lake Pleasant Parkway aesthetic criteria and physical components;
3. Discourage unwanted mid-street pedestrian crossings through design and plant selection;
4. Introduce opportunities and design parameters for the display and integration of public art into the streetscape and through functional amenities in public areas (e.g., bus stops, utility boxes screening, lighting fixtures, sidewalks and crosswalks, trash receptacles);
5. Create physical and visual link between the City and Lake Pleasant;
6. Preserve, protect and enhance important visual resources throughout the corridor;
7. Define a visual theme through medians designed with distinctive landforms, plantings and architectural features; and
8. Create an icon capable of functioning successfully in multiple applications, indicative of both the Parkway and the broader Peoria image.

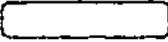
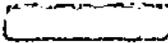


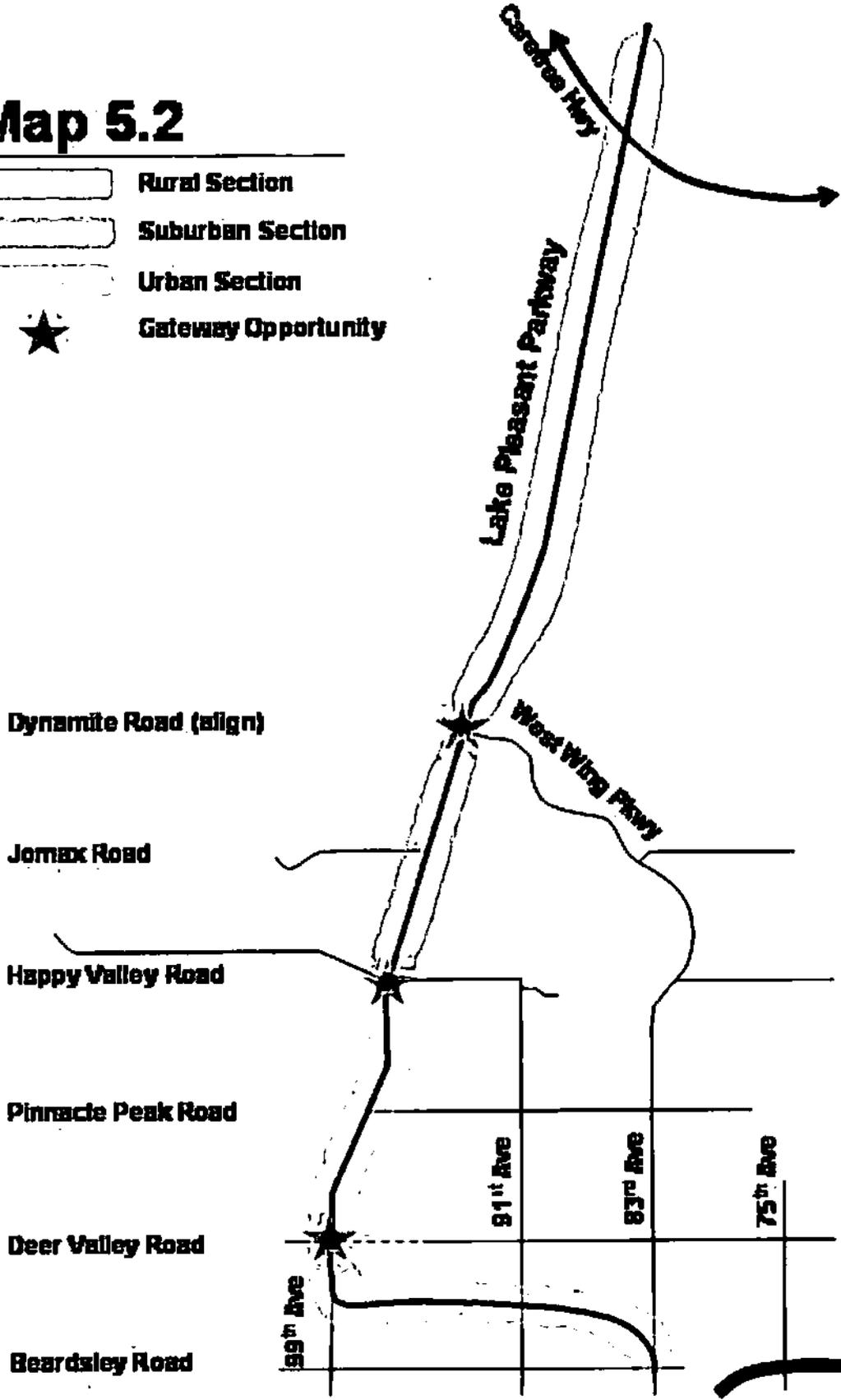
B. APPLICABILITY (20-78-5.III.B)

The principles and guidelines in this chapter will apply to the median and edge treatments for the Lake Pleasant Parkway corridor as depicted on Map 5.2. The design theme will be implemented through Capital Improvement Projects (CIP) and new development applications.



Map 5.2

-  Rural Section
-  Suburban Section
-  Urban Section
-  Gateway Opportunity





C. EXISTING CONTEXT (20-78-5.III.C)

The Parkway currently touches portions of Fletcher Heights, Camino A Lago, Lake Pleasant Pavilions, Lake Pleasant Town Center, Pleasant Valley Ranch and Cibola Vista Resort and Spa among other existing and proposed residential and commercial developments. The Lake Pleasant Parkway corridor passes through various types of terrain. It begins in a highly urbanized area in the southern portion of the corridor and continues north through undeveloped creosote flats and hillside areas along the northern half of the Parkway. As the Parkway heads north, additional view corridors have been maintained on both sides of the right-of-way to ensure the preservation of the mountain views looking north of Lake Pleasant.



D. DESIGN STANDARDS (20-78-5.III.D)

Design standards are parameters set forth to ensure that the design philosophy is carried out in a satisfactory manner. The standards established below shall supersede other City Code and Design Review requirements for median design and development within Lake Pleasant Parkway corridor.

This section of the Design Review Manual is intended to guide the landscape, wall, and artwork design located in all medians along Lake Pleasant Parkway to ensure a cohesive design theme along the entire corridor. In addition to guiding median design, this section is also used to influence the design of landscaping, public art, and walls for all private development adjacent to the Parkway.



1. WALLS (20-78-5.III.D.1)

- a. Walls shall be used in medians to provide a vertical surface to accentuate thematic concepts and artistic elements.

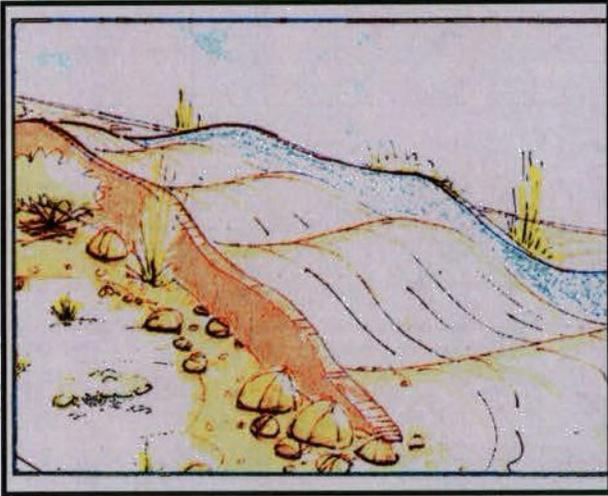


Figure 5.28—"Canal" style median walls

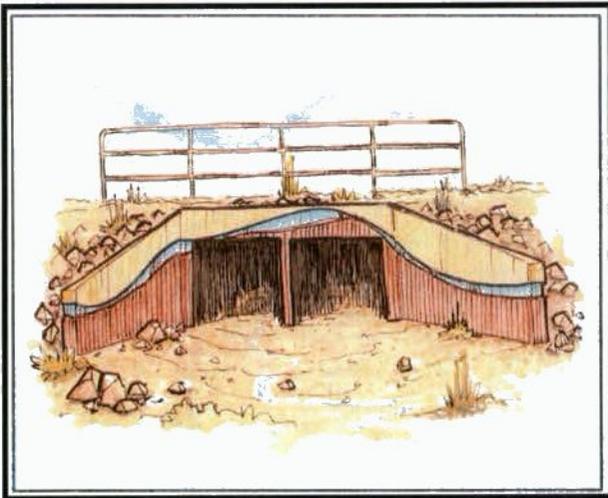


Figure 5.29—Headwall perspective

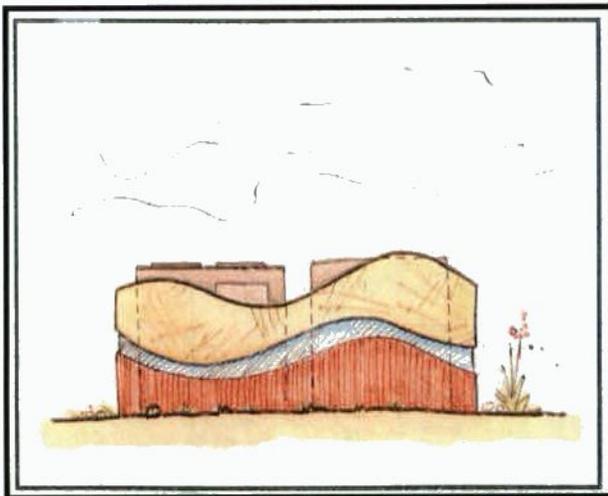


Figure 5.30—Utility box screen wall

- b. Walls shall exhibit warm earth tones, gold, rust, red, and yellows, accented with greens and blues.
- c. Walls shall be accented with materials such as glass block, stone, tile or other suitable materials to add visual interest and texture changes.
- d. Median walls shall be twelve (12) to thirty (30) inches in maximum visible height, but in no case shall the wall exceed thirty-six (36) inches in height above nearest curb.
- e. Median wall sections shall be between twenty-five (25) and forty (40) feet in length and shall be curvilinear in design. Walls or wall patterns shall undulate vertically to represent 'wave' theme. Physical separation of wall segments is strongly encouraged.
- f. Provide nighttime interest with the use of low level lighting.
- g. Drainage outflow aprons or headwalls should be made from materials used in the median "wash". At a minimum, these items shall be dyed/stained or painted to reflect the predominant color found in the area, however the preferred design will incorporate stone facades and enhanced railing.

2. BERMS (20-78-5.III.D.2)

- a. Berming shall occur along all walls within the medians and edge treatment.
- b. Berms shall be formed in a manner that will support plant growth and reduce the likelihood of scouring and/or the unintentional movement of landscape materials. Ideally, berming will enhance the 'wave' theme by creating the illusion of the wall diving in and out of the terrain.

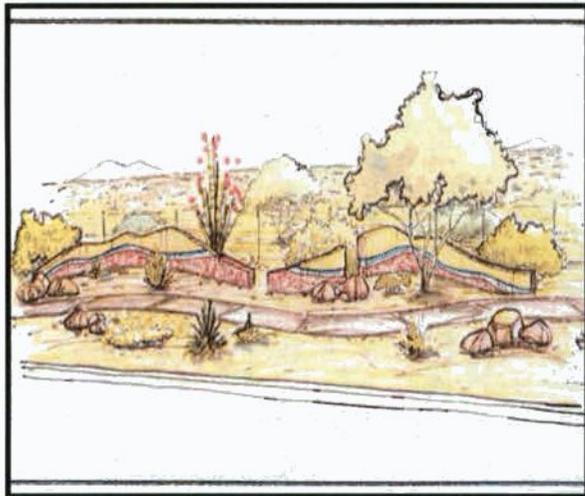


Figure 5.31—Low decorative walls at New River Dam Embankments.

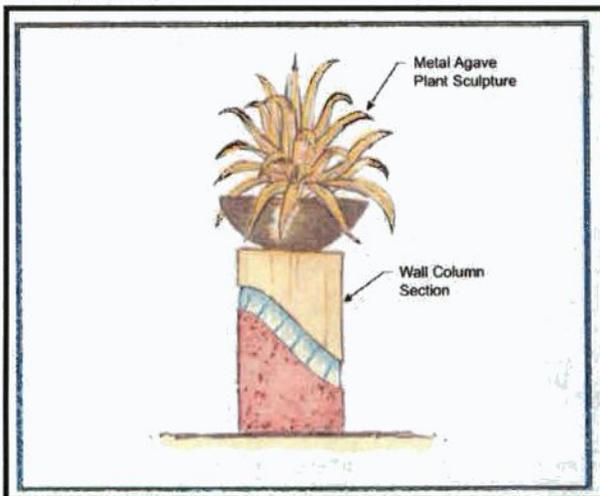


Figure 5.32—Decorative planter

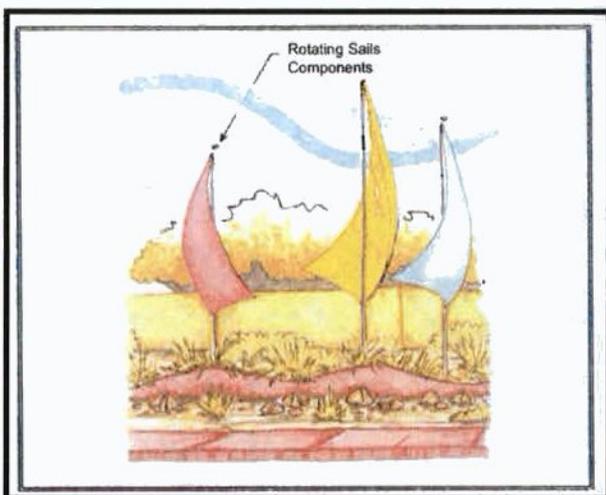


Figure 5.33—Interpretive sails

- c. There shall be a thirty (30) inch maximum grade change above or below median curb elevation.

3. ARCHITECTURAL SCULPTURE ELEMENTS (20-78-5.III.D.3)

- a. Elements shall be concentrated in areas of heavy use and visual prominence.
- b. Elements shall be incorporated into streetscape amenities such as bus stops, pedestrian rest areas, or simply as stand-alone focal points for public art displays.

- c. Sculpture elements shall be located outside of sight visibility triangles, rights-of-way, and utility easements. Typically, these elements should be located five (5) to ten (10) feet from the nearest right-of-way line. Deviations from this spacing may be supported depending on specific site conditions.

4. LANDSCAPING (20-78-5.III.D.4)

- a. The species and density of landscape materials shall meet or exceed the right-of-way planting standards found in the Zoning Ordinance. Median right-of-way landscape area shall be included as part of the required right-of-way landscaping calculation for properties adjacent to Lake Pleasant Parkway.

- b. Plant species shall be selected from the Arizona Department of Water Resources (ADWR) Landscape Planting Materials list. This list includes a preliminary plant palette of drought tolerant/low water use plants with bold forms and colors.

- c. All required right-of-way landscaping shall be located outside of the ultimate right-of-way.

Interim rights-of-way shall be landscaped with shrubbery, groundcover, and dust proofing with the understanding that future improvements will eliminate this landscaping.

- d. Inert materials shall include: decomposed granite, cobble, riprap, salvaged soil, native boulders, river rock, rock mulch, and desert wash sand.

5. PUBLIC ART (20-78-5.III.D.5)

Artistic expressions, either integrated into streetscape elements or as individual creations, should be used to enrich the experience of public spaces and to mark or signal a specific "place".

- a. Opportunities for public art include:
 1. Seating areas and bus stops.
 2. Gateways areas.
 3. Subdivision entries.
 4. Trailheads.
 5. Public facility entries.
- b. Theme ideas should include:
 1. Sails or interpretive sail shapes.
 2. Watercourse shapes/forms.
 3. Interpretative desert plant shapes or replicated plants.
 4. Gabion pillars used to influence water flow or to enhance wall placement and bus stop design.
 5. Wind spirals – kinetic art or the appearance of kinetics, without becoming distracting or unsafe.

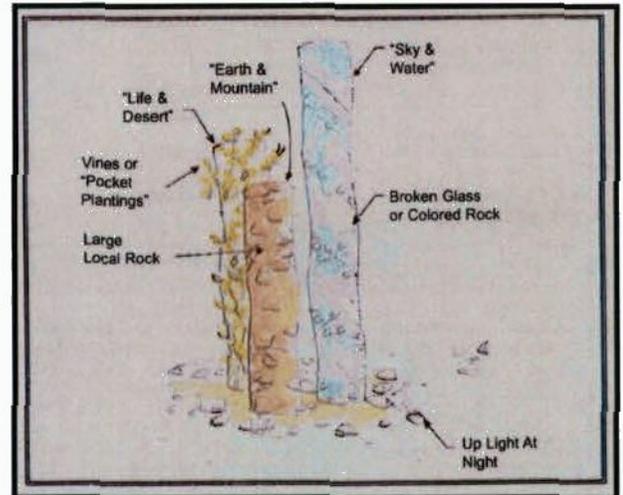


Figure 5.34—Gabion pillars

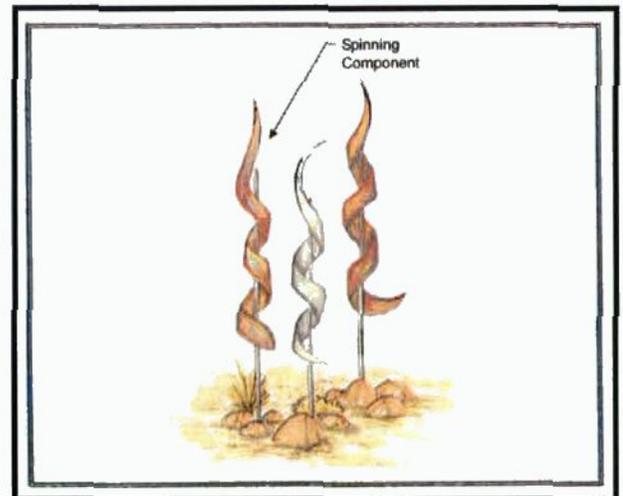


Figure 5.35—Vertical wind spiral

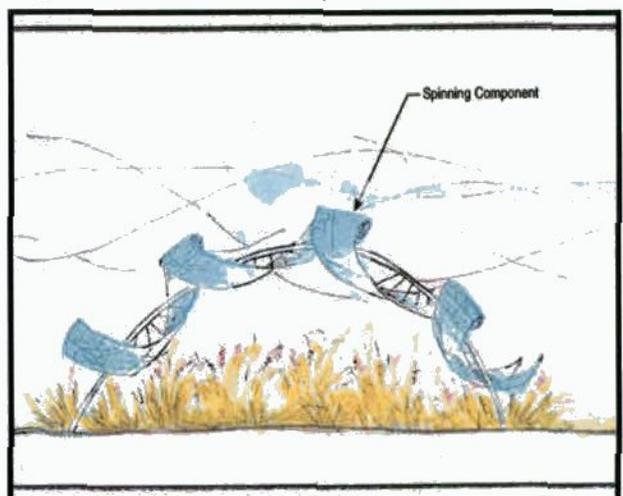


Figure 5.36—Wind spiral



Figure 5.37—Layered mountains gateway

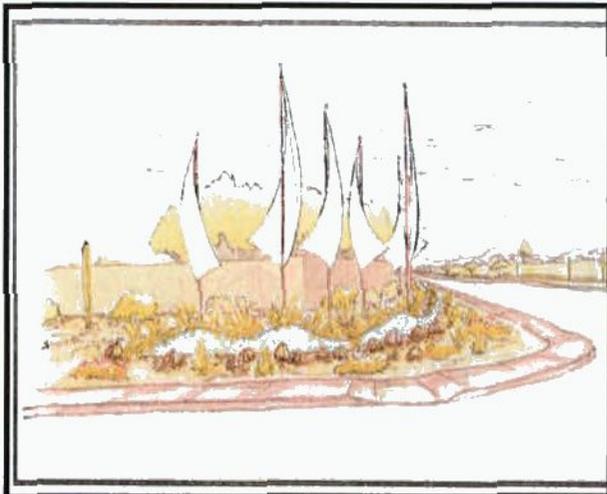


Figure 5.38—Sail gateway



Figure 5.39—"spillway" & pillars gateway

6. GATEWAYS AND TRAILHEADS (20-78-5.III.D.6)

Gateway opportunities for Lake Pleasant Parkway should reinforce the design theme in areas of the right-of-way. Gateway and trailhead areas welcome the public and punctuate the streetscape theme, linking the pedestrian and vehicular user. A gateway, corner treatment, or trailhead should combine the large visual scale of median requirements with the detail interest and creativity of the artistic requirements. Possible gateway locations are identified in Map 5.2.

- a. All sides of an intersection should be used to incorporate the theme elements of art, landforms, plantings, and lighting.
- b. To provide a sense of arrival at all themed gateway or trailhead areas, the following elements should be incorporated:
 1. Entry monument or sculpture, water feature or other identifying element consistent with the area and/or the theme established through a particular zone of the Parkway.
 2. Themed Landscaping.
 3. Public Art.
 4. Signage.
 5. Seat and/or Decorative Walls.

7. LIGHTING AND BANNERS (20-78-5.III.D.7)

To the maximum extent possible, these elements should share the theme of the particular Parkway zone in which they are located and should be of a scale that is compatible with the pedestrian environment.

- a. Decorative lighting should include:
 1. LED's incorporated in walls.
 2. Tree and shrub lighting.
 3. Low-level bollard lighting in pedestrian areas.
- b. Pole lighting should include:
 1. Paint colors that enhance/reinforce the theme palette.
 2. Banner opportunities, such as panel or sail banners.

8. STREETSCAPE AMENITIES (20-78-5.III.D.8)

- a. Streetscape amenities should include furniture, lighting, trash receptacles, and bike racks to provide functional convenience for Parkway users.
- b. Streetscape amenities should integrate art and iconic elements.
- c. Streetscape amenities must be selected to reinforce the design theme through their shapes, colors, and materials.
- d. Right-of-way landscaping and groundcover for all projects along Lake Pleasant Parkway should continue adjacent landscaping themes to the extent feasible.

E. CHARACTER AREAS DESIGN CONCEPTS AND GUIDELINES (20-78-5.III.E)

The primary design concept for Lake Pleasant Parkway is based on the Parkway's namesake—Lake Pleasant. This concept presents itself in the

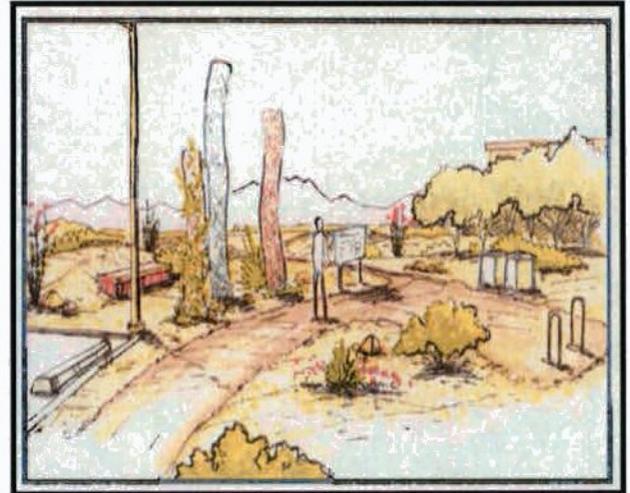


Figure 5.40—Small gateway at trailhead

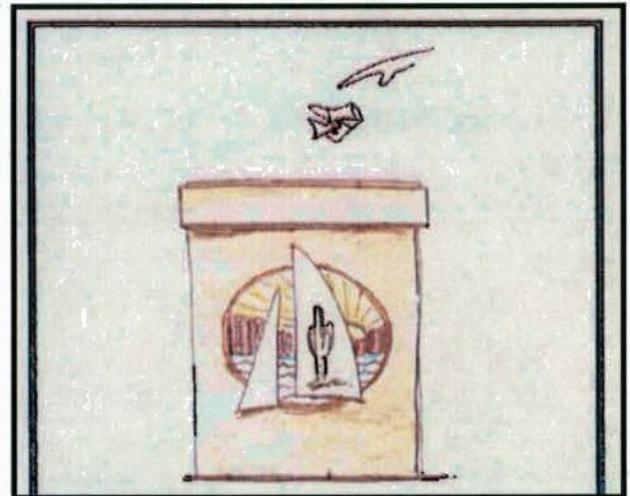


Figure 5.41—Trashcan with sail/sunset icon

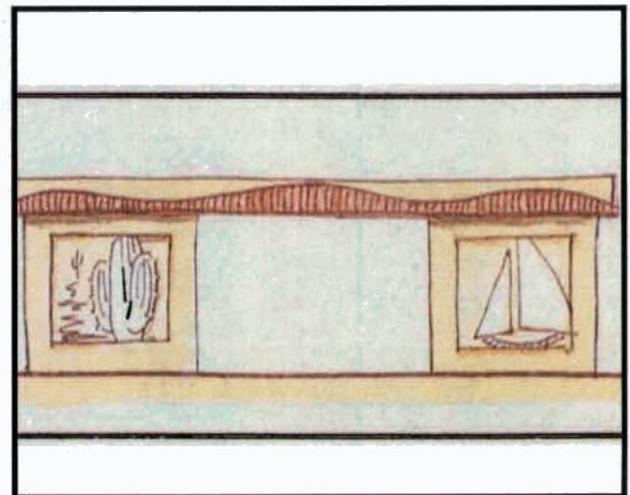
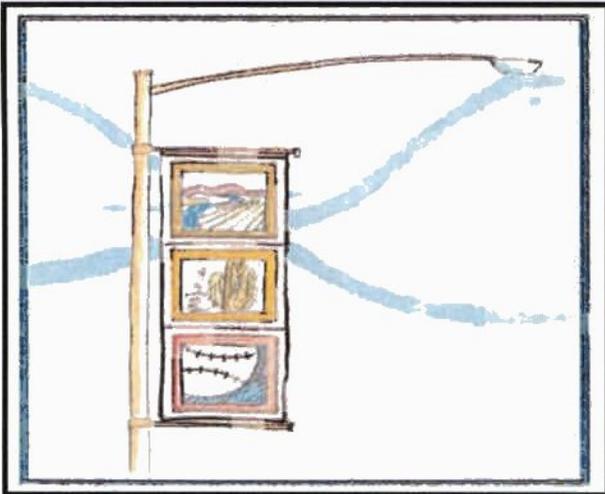
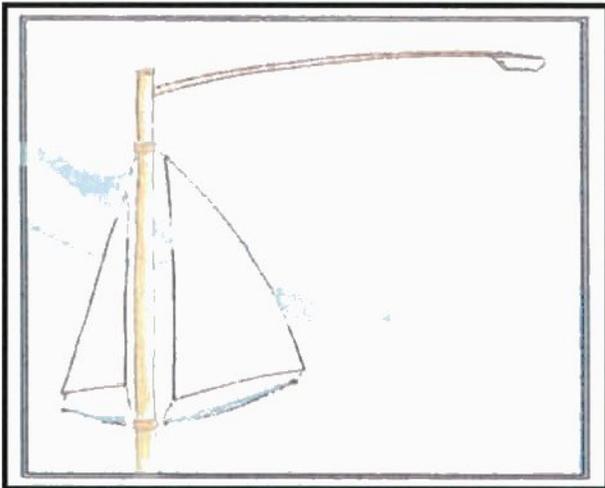


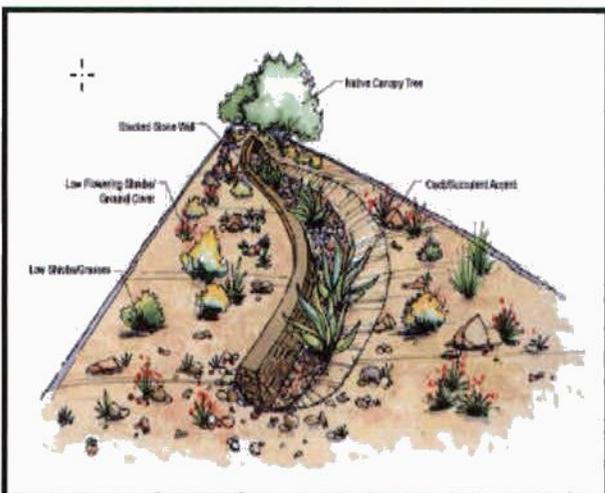
Figure 5.42—Bench with window frame blocks



5.43—Window frame panel banner



5.44—Sail banner



5.45—Typical urban median planting with decorative low walls

form of underlying 'waterway' or 'wave' elements in wall design, landscaping, artistic endeavors, and hardscape patterns and materials.

The City has established a series of zones along the Parkway which were created based on existing and proposed land uses and densities. These zones have been labeled 'Urban', 'Suburban' and 'Rural' in concert with the zones established in the General Plan Land Use Element. Medians and right-of-way landscape areas will be treated accordingly depending upon the zone in which they occur. The following design guidelines are intended to illustrate the vision for each zone along the Parkway and may be expanded depending upon the nature of each individual project.

1. URBAN ZONE (20-78-5.III.E.1)

Through the General Plan Land Use Element defines the Urban Zone along the Parkway, all Commercial nodes as identified on the General Plan Land Use Map, will be subject to the Urban Zone design standards as provided below. The established view corridor setback criteria will remain as stated in the General Plan.

- a. Thematic elements shall be contemporary in nature, but shall provide a range of materials, colors and forms that maintain the 'wave' theme. Use of stone, split face block, brick, or other materials associated with texture is discouraged. Materials such as smooth block, glass block, tile, smooth finish stucco, or similar materials are encouraged in the Urban Zone. Deviations from these materials may be considered if a severe contrast with surrounding architecture is demonstrated.
- b. Built elements such as walls, bus stops, and pedestrian amenities shall reflect regional architecture types.
- c. This zone shall contain the highest concentration of artistic elements and public art.
- d. Plantings shall be formalized and of the highest

density, with a high concentration of flowering species and street trees.

e. Walls shall be:

1. Placed in grouped series of segments in lieu of a single long segment.
2. Less curvilinear and possibly straight runs of walls parallel to traffic lanes, often overlapping each other to form canal-like runs.
3. Architectural forms, with level or angled surfaces and edges.
4. Reflecting architectural themes with bright solid areas of colors.
5. Enhanced with low-level lighting, possibly tinted to complement the colors of the wall itself.

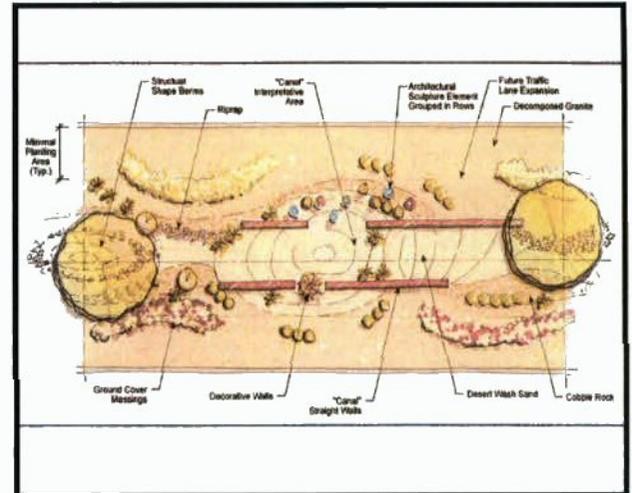
f. Berms shall be constructed with defined edges and/or man made forms.

g. Architectural sculpture elements shall be distributed throughout median areas in clusters of five (5) to seven (7) elements for every five hundred (500) linear feet where medians are wide enough to allow them. If unable to provide these quantities in the median, the option to provide in the adjacent right-of-way landscape areas is strongly encouraged where appropriate.

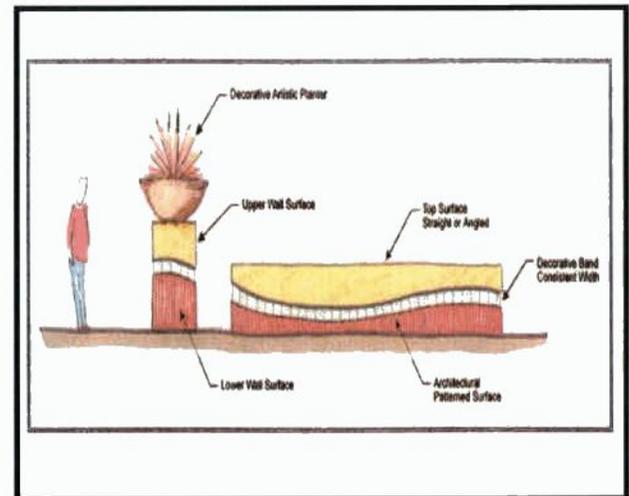
h. Inert materials shall include decomposed granite, cobble and riprap. Colors shall be primarily browns with assorted accents of blues, greens, and grays.

2. SUBURBAN ZONE (20-78-5.III.E.2)

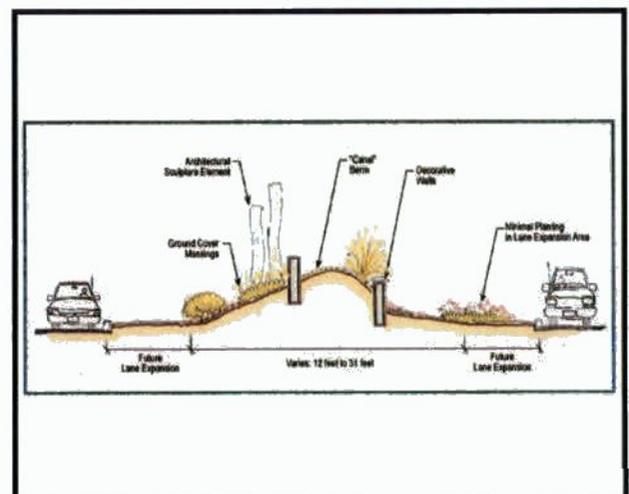
Located between Happy Valley Road and Dynamite Boulevard.



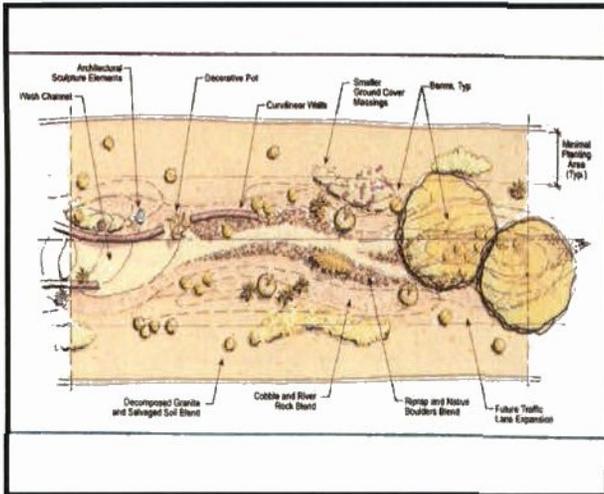
5.46—Typical urban 00' plan



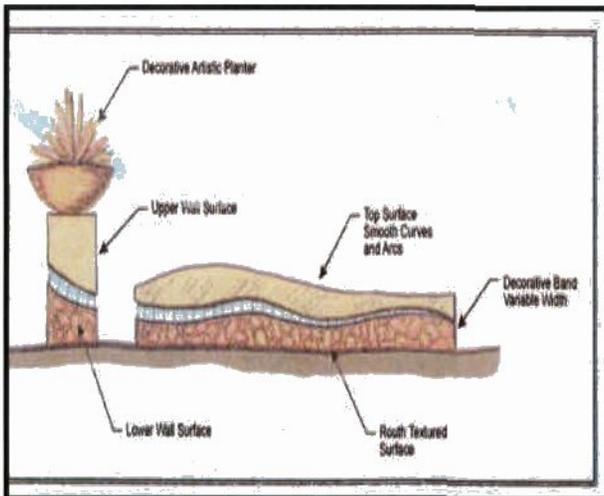
5.47—Urban character wall design



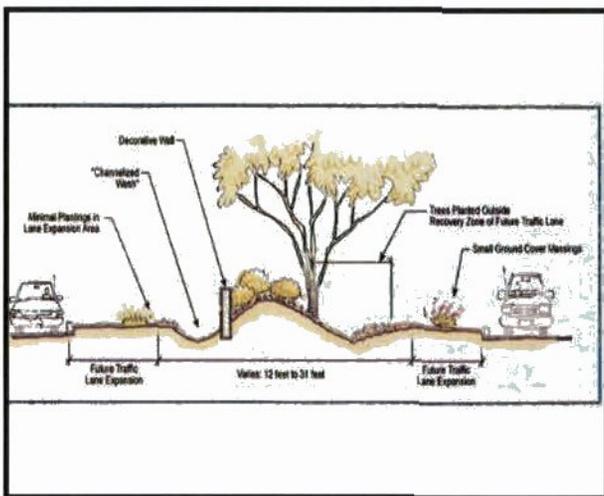
5.48—Typical urban cross section



5.49—Typical suburban 100' plan



5.50—Suburban Character wall design



5.51—Typical suburban cross section

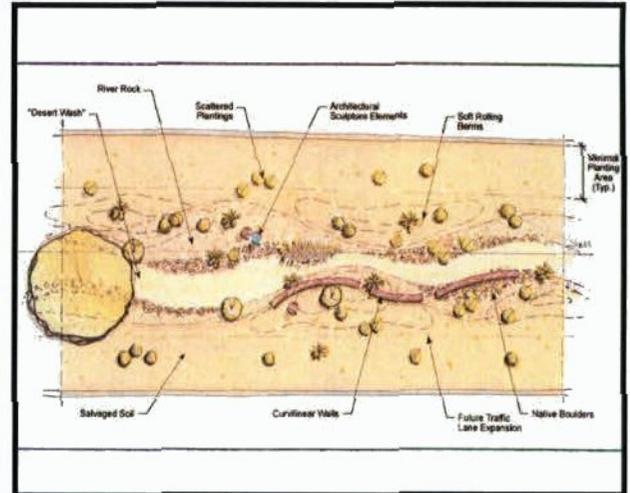
- a. Theme elements shall suggest organic forms with interpretative shapes of the surrounding desert landscape.
- b. Built elements shall be less dense in median design than what is found in the Urban Zone.
- c. Architectural sculpture elements and public art opportunities shall be provided at a rate of three (3) to five (5) per five hundred (500) linear feet where medians are wide enough to allow them. If unable to provide these quantities in the median, the option to provide in the adjacent *right-of-way* landscape areas is strongly encouraged where appropriate.
- d. Plantings shall be a mix of naturalized forms and bold accents, but shall be planted with lower-level and less clustered plantings than in the Urban Zone.
- e. Walls shall be:
 1. More curvilinear in form than in the Urban Zone and shall begin more pronounced vertical undulation.
 2. Constructed in overlapping segments to offer visual relief and opportunities for the placement of artwork.
 3. Man-made forms inspired from natural forms with the 'wave' theme incorporated into wall elevation through use of materials and colors. Materials with a more natural feel and/or coarse textures may be transitioned into wall design.
 4. In washes of colors having accent areas of bold solid tones.

- f. Berms shall have softened edges.
- g. Inert materials shall include decomposed granite and salvaged soil, cobble, river rock, and blended riprap with native boulders. Colors shall be those native to the area surrounding Lake Pleasant Parkway, mixed with richer browns accented with blues, greens, golds, reds, and grays.

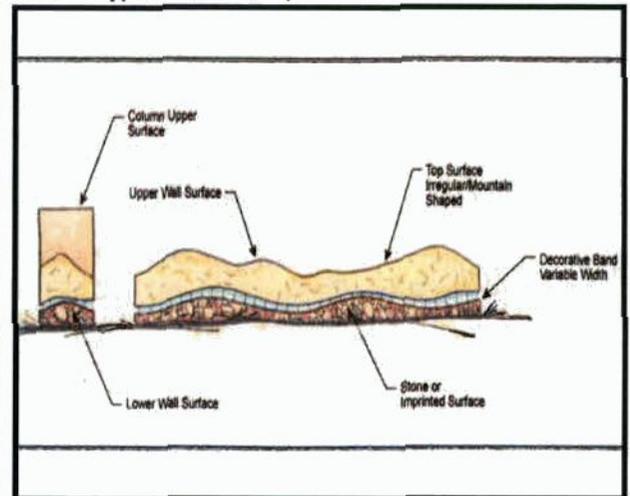
3. RURAL ZONE (20-78-5.III.E.3)

Located north of Dynamite Boulevard.

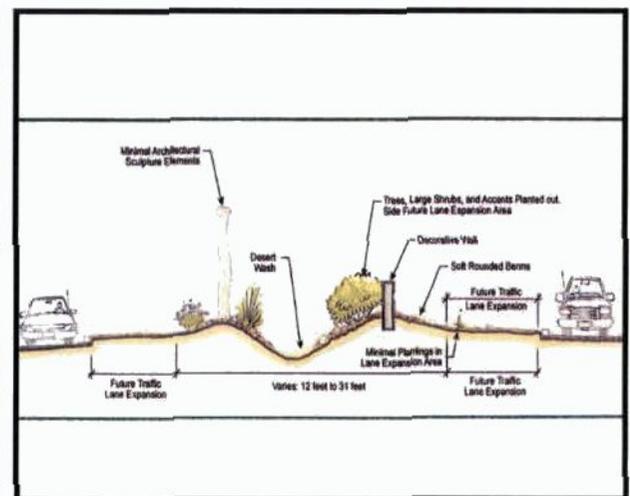
- a. Theme elements shall integrate organic shapes, that mimic the surrounding desert forms.
- b. Constructed theme elements shall be placed at ratios that are less dense, but equally as meaningful as what is provided in the Suburban Zone.
- c. Architectural sculpture elements shall be provided at a rate of three (3) to five (5) per seven hundred and fifty (750) linear feet where medians are wide enough to allow them. If unable to provide these quantities in the median, the option to provide in the adjacent right-of-way landscape areas is strongly encouraged where appropriate. In this zone only, public features may be concentrated at intersections or other focal points.
- d. Plantings shall reflect the surrounding desert, with less formal groupings and more natural plant densities and species.
- e. Walls shall be:
 1. The lowest of the three (3) zones in order to maximize views and minimize visual distraction in the area.



5.52—Typical rural 100' plan



5.53—Rural character wall design



5.54—Typical rural cross section

2. Designed to maximize vertical and horizontal undulation in conjunction with integrated berming.
 3. Separated into multiple segments with sections overlapping by five (5) to ten (10) feet.
 4. Constructed of coarse textures and natural materials including stone, rock-filled gabions, block, and other similar materials.
- f. Saguaros should be provided in exchange for median and right-of-way trees at a ratio of one (1) mature specimen saguaro for two (2) mature trees to serve as visual indicators of place.
 - g. Berms shall be softly rounded in natural shapes.
 - h. Inert materials shall include salvaged soil, river rock and native boulders. Colors shall be stone native to the area surrounding Lake Pleasant Parkway.

IV. Utility Cabinet Screening

IV. Utility Cabinet Screening

IV. UTILITY CABINET SCREENING

A. INTENT (20-78-5.IV.A)

The purpose of this section is to establish placement and screening criteria for utility cabinets and infrastructure to reduce the visual impacts created by the location of equipment along arterial and collector roadways.

B. STANDARDS (20-78-5.IV.B)

1. PLACEMENT STANDARDS (20-78-5.IV.B.1)

The developer is responsible to work with the utility companies to coordinate the location of utility cabinets according to the provisions listed below. If required, an on-site easement shall be provided by the developer. The City shall comment on the location and screening methods of all utility cabinets in standard comment form and will prepare a standard Condition of Approval / Stipulation upon site plan approval, which must be satisfactorily addressed during building permit review. The City shall have the authority to modify the placement of the utility cabinets, walls, berming and landscaping to provide sufficient screening from public rights-of-way, public access easements, roadway easements, and private streets.

The utility cabinets shall be placed in the designated areas as indicated below. Deviation from these areas may be approved by the Planning Manager or designee provided sufficient justification is afforded by the applicant.

a. **Utility Cabinet Placement**

Non-Residential / Multi-Family

1. Landscape Buffer or Side/Rear Setback area. (Figure 5.55)

Utility cabinets shall be located in landscape buffers or side / rear building setback areas. The location shall be outside of retention and street frontage landscape areas and must be accessible by service vehicles.

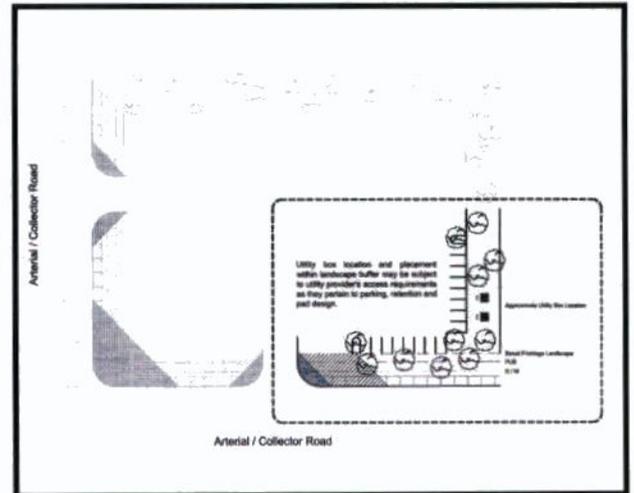
2. Street Frontage Landscape Area (Figure 5.56)

Utility cabinets of any height shall not be located within sight visibility triangles (per Engineering Department detail) associated with driveways and road intersections. Cabinets exceeding thirty (30) inches in height shall be located beyond the first one hundred and fifty (150) feet of street frontage and primary driveways measured from face of curb of the intersection and at least fifty (50) feet from any sight visibility triangle. Cabinets less than thirty (30) feet in height may be located within the first one hundred and fifty (150) feet of the site, but may not be located in a 75' x 75' triangle at the intersection of two (2) arterials. For roads that are recognized as scenic roadway corridors or requiring a special setback / landscape buffer, utility cabinets shall be placed along a side street, access aisle or completely outside of the setback/ landscape buffer area. In all locations, cabinets must be outside of retention areas and must be accessible by service vehicles.

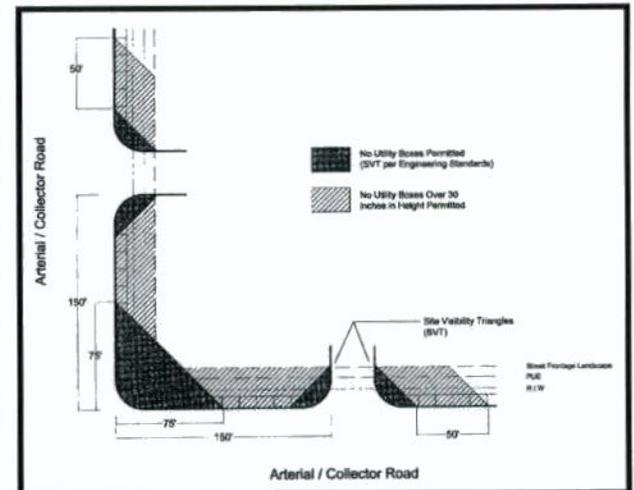
Single-Family Residential

1. Residential Development (Figure 5.57)

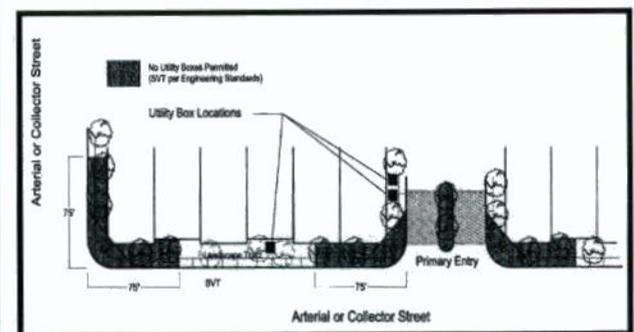
To the maximum extent possible, utility cabinets are to be located on streets classified as residential/ local streets only. The cabinets shall be located in PUE's in areas contained within the side / rear yard landscape tracts. In the event that placement along collector or



5.55—Landscape Buffer of Side/Rear Setback Area.



5.56—Street Frontage Landscape Area.



5.57—Residential Development.



Service Entrance Section screened with landscaping and an architecturally compatible wall.



Trellis and dense landscaping provide ample screening of Service Entrance Section



Screen wall conceals a Service Entrance Section on an industrial building

arterial streets is unavoidable, a modified version of Option #2 above will be applied. For all primary entrances, no cabinets shall be located along the collector or arterial street within seventy five (75) feet of the entry. Flexibility will be granted for the distance requirement along the entry 'throat' into the development due to variations in minimum lot depth. Cabinets located along collectors and arterials shall be inset into required wall undulations, provided accessibility is maintained. In all locations, cabinets must be outside of retention areas and must be accessible by service vehicles.

2. SCREENING DESIGN STANDARDS (20-78-5.IV.B.2)

a. Utility Cabinets

Utility cabinets shall be screened by architecturally integrated walls that complement the architecture of the surrounding development, berming and / or dense landscaping. Walls cannot limit accessibility to the cabinet.

The exterior of all utility cabinets shall utilize earth tone colors to match the predominate color of the surrounding groundcover material / screenwalls.

b. Exterior Wall Equipment

Exterior wall equipment, including electrical meters, electrical distribution cabinets, service entrance sections (SES) shall be incorporated in the design of the building, whether in a separate utility room or in an alcove to allow the equipment to be flush with the building façade.

Exterior wall equipment that is contained in an alcove located on the street side of the building and shall be fully screened from view through the use of screen walls, landscaping, trellis, green screens, or a combination thereof.



Exterior wall equipment is located in alcove in loading area for protection and screening.