

TABLE OF CONTENTS

Projective Overview 1, 2

Introduction 1
 Objectives and Goals 1
 Design theme objective and goals 1
 Existing Context 2
 Regional Context 2
 Water bodies 2
 Mountains 2
 Surrounding development/Built environment 2

Concept Development 3-11

Initial Theming Process 3
 Streetscape and Median Theming Survey 3
 Survey results 4
 Theme Development Process 5-11
 Theme Organization 5
 Alternative A: Spatial Organization Model 5
 Alternative B: Future Growth Model 6
 Preliminary Concept Theming 7
 River Curves Theme (option one) 8
 Waterway Theme (option two) 9
 Preferred Concept 10
 Aesthetic Details and Distribution 11

Design Philosophy 12, 13

General 12
 Theme Narrative/Design Philosophy 12
 Waterway Expression 12
 Structures, Walls, Architectural Sculpture Elements 12
 Landscape Design and Planting 13
 Gateways 13
 Other Theme Opportunities 13
 Right-of-way Design Development 13
 Iconography 13

Design Criteria 14-21

Median Development 14
 Urban/Commercial Character Area 15
 Streetscape elements 15
 Inert materials 15
 Walls 15
 Landscape 15
 Berms/Washes 15
 Architectural sculpture elements 15
 Colors 15
 Residential/Medium-density Character Area 16
 Streetscape elements 16
 Inert materials 16
 Walls 16
 Landscape 16
 Berms/Washes 16
 Architectural sculpture elements 16
 Colors 16
 Rural/Open Space Character Area 17
 Streetscape elements 17
 Inert materials 17
 Walls 17
 Landscape 17
 Berms/Washes 17
 Architectural sculpture elements 17
 Colors 17
 Landscape Materials 18
 Planting Palette List 18
 Wall Designs 19
 Wall and Architectural Sculpture Element Materials 20
 Paving and Inert Materials 21

Design Guidelines 22-27

Design considerations and opportunities 22
 Curbs and paving 22

Walls and signs 23
 Seating areas and bus stops 24
 Public art and integrated art 24
 Gateways and trailheads 25
 Lighting and banners 26
 Icon development and branding 27

Preliminary Cost Estimate 28-29

Appendix 30-39

Meetings – Agendas and notes 30-32
 Lake Pleasant Parkway Theme Development Map 33
 Landscape cost estimate comparison 34-35
 Survey results spreadsheet 36-38
 Acknowledgements 39
 Video Simulation CD-ROM
 (available separately)



INTRODUCTION

During recent plans to widen Lake Pleasant Parkway (LPP), the City of Peoria (COP) determined that it had an opportunity to define the parkway as a signature street, a calling card for the COP and region. The streetscape theme preparation was an opportunity to accentuate the natural beauty of the region and highlight the city's significance in the Valley. The purpose of this Design Theme Manual is to document the design criteria and guidelines for the development of landscape aesthetic treatments for the LPP in a clear format.

This manual begins with the Project Overview, delineating the objectives and goals for the document and the LPP's existing conditions. Concept Development makes up the next section of the manual, with descriptions of the initial theming process, streetscape and median theming survey, survey results, concept development process, theme organization, preliminary concept theming, and the preferred concept. The Design Criteria and Design Guidelines are established in the manual's third section. This section consists of the overriding design philosophy, median design criteria, and landscape aesthetic design guidelines for areas located outside the medians. Preliminary Cost Estimates for median development and typical landscape areas for the three Character Areas are outlined in the next section. The last section contains the Appendix with supporting materials such as meeting notes and the survey results.



Desert Landscape

OBJECTIVES AND GOALS

The objective of the Design Theme Manual is to establish and document aesthetic parameters as a set of criteria and guidelines for the newly renamed LPP. Implementation of this collection of criteria and guidelines—collectively, a design theme—will enable the LPP median to visually cohere throughout its entire length. Thus the median alone shall develop an aesthetic experience for the streetscape relating to the region and highlight the city's significance.

The COP encourages private development to execute this design theme when developing back-of-curb streetscape areas. Utilizing elements of the LPP design theme will reinforce a cohesive streetscape for the LPP corridor. However, should adjacent private developments implement a different identity and character for their properties, the LPP will sufficiently retain its own identity and visual character.

DESIGN THEME OBJECTIVES AND GOALS

- ✿ Create a signature roadway for the city, clearly identifiable as the LPP and the city of Peoria, with a bold statement implemented through creative design.
- ✿ Create a documented set of written standards, photographs, and graphics that clearly illustrate the LPP aesthetic criteria and physical components.



Median Walls

- ✿ Discourage mid-street pedestrian crossings through design and plant selection.
- ✿ Introduce opportunities and design parameters for the display and integration of public art into the streetscape and through artistic functional amenities of the public areas (e.g., bus stops, utility boxes, median bullnoses, lighting fixtures, sidewalks and crosswalks, trash/ash receptacles).
- ✿ Create a physical and visual link between the city center and Lake Pleasant.



Lake Pleasant/Recreation Connection

- ✿ Preserve, protect, or enhance important visual resources throughout the corridor for the present and future development of Peoria.
- ✿ Define a visual and physical presence through the proposed streetscape design theme and especially through medians designed with distinctive landforms, plantings, and architectural features.
- ✿ Create an icon capable of functioning successfully in multiple applications and indicative of both the LPP and the broader desired Peoria image.



Views to Distant Mountains

EXISTING CONTEXT

The existing LPP begins on the southern end at Beardsley Road as a four-lane undivided road connecting to 99th Avenue at the northern limits of Sun City, where it then extends north-northeast 13 miles to Carefree Highway, SR 74. Residential subdivisions comprise the majority of the existing moderately developed southern end of the LPP. The new Cibola Vista Resort and Spa represents entry of new development into open desert areas near West Wing Mountain. The northern half of the parkway is undeveloped and cuts through creosote flats to meet SR 74.

REGIONAL CONTEXT



CAP Canal

The variety of water bodies in the area surrounding LPP contributes to the terrain and scenic importance of the LPP to the city of Peoria and neighboring communities. The parkway transverses relatively flat lands bounded on the east by the New River and, on the west, by the Agua Fria River. Lake Pleasant, for which the parkway is named, is perched directly north of the LPP, less than 4 miles from its northern terminus. The proximity of these water bodies plays a significant role in the importance of the parkway as a link between populated areas and the natural scenic and recreational opportunities that these water bodies offer.

Pleasant, for which the parkway is named, is perched directly north of the LPP, less than 4 miles from its northern terminus. The proximity of these water bodies plays a significant role in the importance of the parkway as a link between populated areas and the natural scenic and recreational opportunities that these water bodies offer.

WATER BODIES

- ☼ Agua Fria River – west
- ☼ New River – east
- ☼ Lake Pleasant – north
- ☼ Central Arizona Project Canal – north (crosses LPP at northern 1/3 of parkway)
- ☼ Arizona Canal

Many mountain ranges are visible from the undeveloped stretches of parkway and contribute significantly to its scenic quality. Considerations for parkway setbacks are necessary to preserve views to these ranges.



Hills Near Parkway



Scenic Vista From Parkway

MOUNTAINS

- ☼ White Tanks – southwest (middle ground)
- ☼ Sierra Estrella – south (distant)
- ☼ Deem Hills – east (close)
- ☼ McDowell Mountains – east (distant)
- ☼ Four Peaks – east (distant)
- ☼ Hieroglyphic Mountains – north, northwest (distant)

The rates of population growth in Peoria—or, for that matter across the Valley—must be considered in establishing the LPP design theme. Without the existing and proposed developments surrounding the parkway there would be no need for this conscious and considered effort to define and guide the creative improvements to the LPP.



Subdivision Wall Without Landscape Buffer

SURROUNDING DEVELOPMENT/BUILT ENVIRONMENT

- ☼ Cibola Vista Resort and Spa
- ☼ Sun City West
- ☼ Vistancia
- ☼ Lake Pleasant Regional Park
- ☼ Ben Avery Shooting Facility
- ☼ Thunderbird Park



Pinnacle Peak Public Safety Facility

INITIAL THEMING PROCESS

An information-gathering process of meetings and a survey was used to identify the general direction of the design theme project. LSD assembled images and graphics into a survey booklet to elicit feedback from the COP staff regarding potential elements of the parkway theme and design guidelines.

STREETSCAPE AND MEDIAN THEMING SURVEY

In the survey, opinions on the several categories, described below, were recorded using a rating system of "inappropriate" to "very appropriate," with space included for written comments:

MUNICIPAL IMAGE

The municipal image is the memory of a city's face to the surrounding community. Photos of some cities with strong municipal images were provided as examples with the survey. A variety of images ranging from building types to western settlements were used to determine preferences of the participants.



Open Vistas

VISUAL EXPERIENCE

This category sought opinions about what should be seen from the parkway—be it blank subdivision wall or mountain ranges in the distance with wide expanses of blue sky; i.e., what would convey the desired municipal and streetscape image or images.

LANDFORMS

Emphasizing or deemphasizing the natural landforms surrounding a community are methods that may be utilized to anchor a city to its environment. Images of large rolling hills, flat open stretches, and rocky foothills were options shown to determine the types of landforms that would best reinforce the desired municipal image.



Rammed Earth Wall

WALLS AND FENCES

Walls and fences are opportunities to vertically indicate ideas and images. Even their absence connotes certain impressions. They serve to express the themes of the streetscape. This section contained images of walls of various heights, materials, and opacity.

BOLLARDS AND PLANTERS

The use of bollards and planters can make important statements about the municipal image through their function as protective barriers, opportunities for art integration, and as connective design elements whose repeated use reinforces a larger theme.



Large Architectural Pots

STREET AMENITIES

The fixtures and seating used along a street can be used to reinforce themes and integrate art as well as provide for the citizens using the streetscape.



Lightpole Banners

LIGHTING

Street lighting is a necessary item within the streetscape and can be used to reinforce the chosen theming through the selection of fixture heads, pole design, color, banners, and applied artistic expressions. Street lighting can be used in the foreground to accentuate the streetscape image or it can be consciously chosen to recede into the background.

ARCHITECTURAL SCULPTURE ELEMENTS

Artistic expressions, either integrated into streetscape elements or as individual artist's creations, can enrich the experience of public spaces and can mark or signal a specific "place."



Art Bollard

PAVING MATERIALS, COLORS, AND TEXTURES

The sidewalks, walls, curbs, and bullnoses are opportunities to use similar forms and materials throughout the streetscape to reinforce the theme characteristics.

LANDSCAPE DESIGN

The landscape design for a streetscape encompasses the landforms and the scale and organization of plantings. This category sought to determine whether formal, informal, or naturalistic arrangements—or a combination of these—were best suited for the streetscape design theme.



Layered Planting Areas

PLANT MATERIALS

The plant species used in streetscape design are important elements in establishing an overall design theme. Photographs of a representative selection of plant species were used to assess respondents' sense of appropriateness of certain species, color preferences, and groups of plants for use in selecting a preliminary plant palette.

EXISTING CONDITIONS

Photographs were provided to illustrate the current conditions and thus functions as a reference point for understanding the scope of the task ahead.

A stakeholders meeting was held with several COP staff members and LSD. LSD presented the survey, explained the format, answered questions, and polled the attendees for additional ideas, concerns, expectations, or wishes they may have for the design direction. The meeting attendees were also encouraged to ask questions and express opinions regarding the overall theming and design possibilities.

SURVEY RESULTS

Seven COP participant survey forms were collected and tallied. The survey results provided numerical ratings for the images, and written comments in response to the photographs were documented where provided. LSD collected these written comments along with the verbal comments captured by the notes from the stakeholders survey meeting. LSD graphically illustrated the top-rated images, by category, to summarize the ideas garnered from the survey.

MUNICIPAL IMAGE

- Diverse building types are preferred
- Natural landscape and recreational activities strongly linked to city image are preferred
- Not a western ranching community

VISUAL EXPERIENCE

- Wide views, maintain view to the mountains
- Relationship of sky and land is an important relationship to maintain
- Blank subdivision walls are undesirable
- A bold statement within the medians is appropriate



Desert Landscape

LANDFORMS

- Large rolling hills and berms are preferred
- Big boulders and a variety of stone textures are preferred
- Expanses of lawn-covered slopes are undesirable

WALLS AND FENCES

- Rammed earth wall, natural materials are appropriate
- Brick and native rock accents are preferred
- Walls with visual interest and texture are desired

BOLLARDS AND PLANTERS

- Integrated with art is preferred
- Large planters raised above ground plane are appropriate

STREET AMENITIES

- Seating integrated into low walls preferred
- Architectural and artistic forms intrinsic to structures preferred

LIGHTING

- Pillar-style light bollards are appropriate
- Banner attachments to overhead lighting are appropriate
- Banners with artistic design changed seasonally rather than signs for event advertising is preferred



Artistic Bus Stop

ARCHITECTURAL SCULPTURE ELEMENTS

- Interpretive desert ideas are appropriate
- Bold, large-scale objects are appropriate
- Integrated into structures and amenities preferred

PAVING MATERIALS, COLORS, AND TEXTURES

- Brick and stone patterns preferred
- Warm, rich, earth tones are appropriate



Pavers

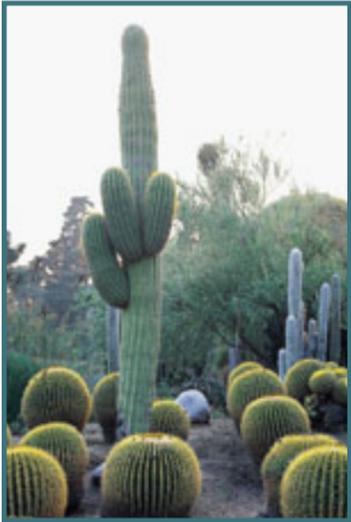


River Run Rock

- Bold textures are appropriate
- Strong, contrasting shapes preferred

LANDSCAPE DESIGN

- Layered plantings with several levels of interest preferred
- Scale planting to appropriate user type
- Pedestrian level – Small-scale, intimate plantings and pedestrian-friendly species
- Middle activity use level – Small shrub groupings and accents
- Through-traffic level – Large massings of ground covers, accents, and shrubs



Desert-Adapted Plants

PLANT MATERIALS

- Desert-adapted species preferred
- Bold shapes and silhouettes are appropriate
- Bright colors are appropriate
- Variety and changing interest are appropriate (e.g., blooms occur at different times)

THEME DEVELOPMENT PROCESS

THEME ORGANIZATION

The theme organization determines the spatial relationships of how elements of a theme concept are to be used. It defines everything from the frequency of artistic elements to the types of materials used on walls. The purpose of subdividing a design theme is to provide a hierarchy to the designed environment that can reinforce and differentiate parkway segments from each other.

ALTERNATIVE A: SPATIAL ORGANIZATION MODEL

A spatial division model of the LPP separates the streetscape into three distinct geographic zones. A southern one-third, a middle one-third, and a northern one-third make up the spatial divisions, with each zone having distinct, designed elements that still relate in some fashion to the adjoining zone(s). No distinction is made for the surrounding development type or regional context for amenities.

Southern Third – Williams Road to Pinnacle Vista Drive: urban emphasis, with the most existing infrastructure and surrounding development

- Theme elements concentrated within the median
- Heavy concentration of built elements reflects architectural patterns
- Plantings formalized, with higher concentration of flowering species

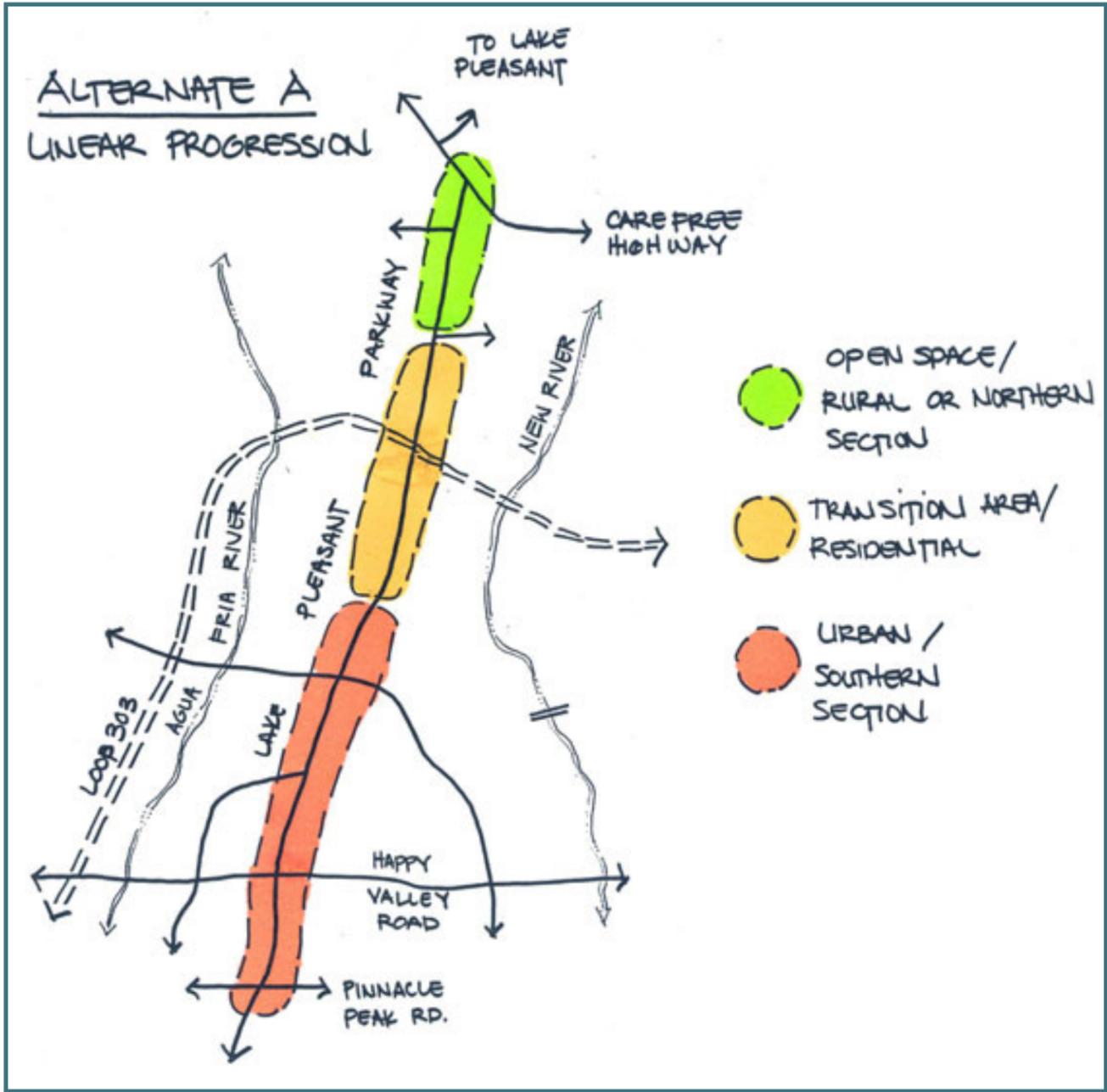
Middle Third – Pinnacle Vista Drive to the future Loop 303: transitional zone with elements of both Southern and Northern segments

- Theme elements concentrated within median and mostly used at major intersections
- Moderate use of built elements and softer, more organic lines and patterns
- Plantings softened in more natural groupings with less massings of flowering species

Northern Third – Future Loop 303 to SR 74: rural emphasis, with most areas remaining undeveloped or developed in a rural character

- Theme elements concentrated at intersections, leaving remaining areas informally laid out

- Minimal use of built elements, with organic forms and little patterning
- Plantings designed with a desert feel through use of salvaged plants and native species



THEME DEVELOPMENT PROCESS (CONT.)

ALTERNATIVE B: FUTURE GROWTH MODEL

The growth relationship model reflects the future development expectations and zoning patterns set forth by the City of Peoria General Plan Growth Map and is responsive to the surrounding site context. This model separates the LPP into three zone types that are sometimes repeated within the corridor in areas with similar future development expectations. The opportunities for open areas and connections to surrounding recreational amenities were factored into the delineation of zones.

Urban/Commercial zone - located in areas with retail and commercial development and often at major intersections

- Theme elements are modern and starkly contrast with natural forms of desert
- Built elements reflect regional architecture types
- Highest concentration of artistic elements and public art opportunities
- Planting formalized and of the highest density, with a high concentration of flowering species and street trees

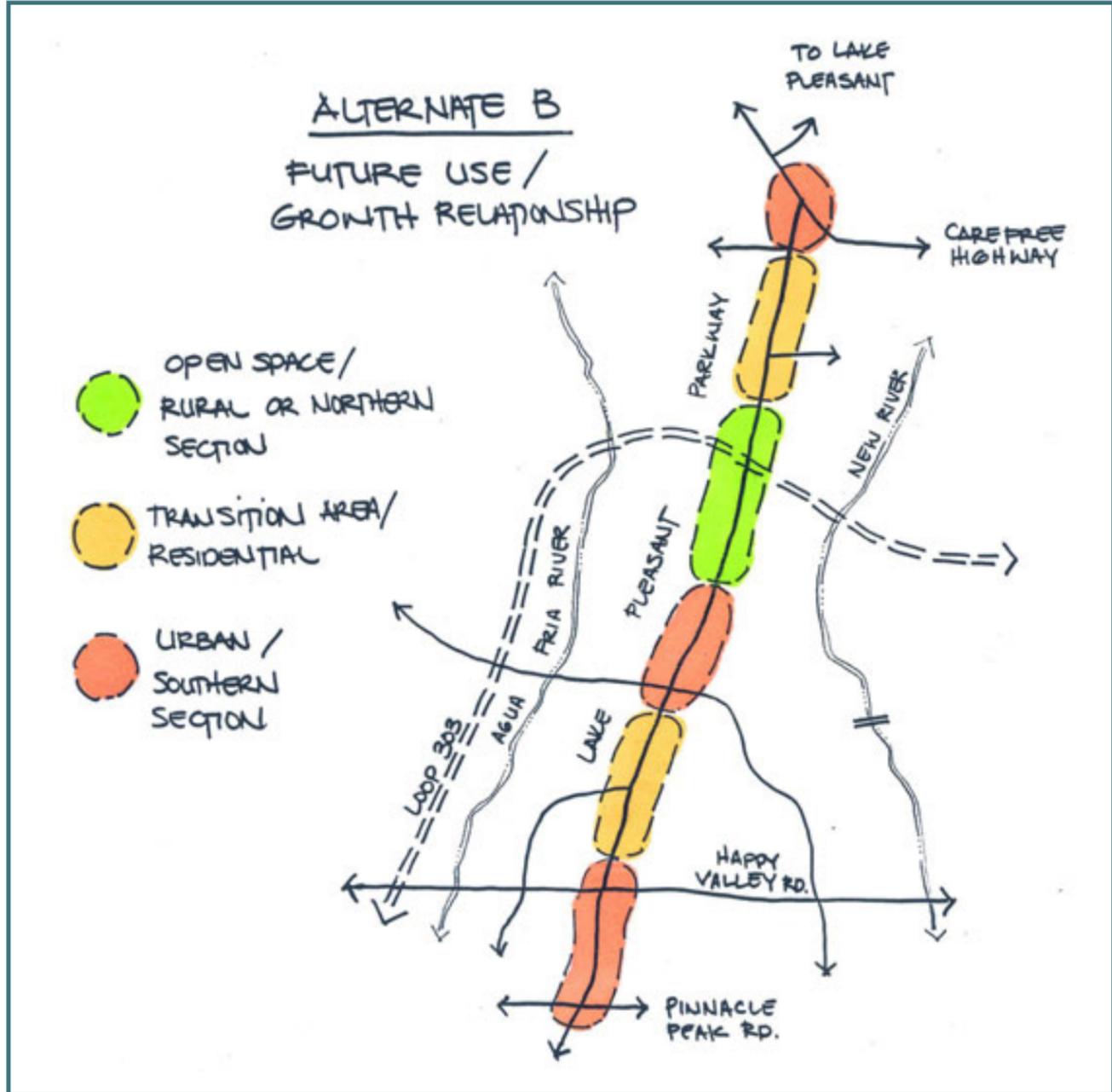
Residential/Medium-density zone - located along subdivisions and mixed-use areas

- Theme elements suggest organic forms with interpretative shapes of the surrounding desert landscape
- Built elements scattered and less densely used in median design
- Architectural sculpture elements and public art opportunities reduced to occasional occurrences at intersections or focal points
- Planting is mix of naturalized forms and bold accents and less dense, and with some trees

Rural/Open space zone - located in areas of recreational open space and low-density development

- Theme elements integrate organic shapes, mimicking surrounding desert forms
- Constructed theme elements minimally used in median areas

- Architectural sculpture elements and public art opportunities rarely used, and only at intersections or focal points
- Planting reflects the surrounding desert, with informal plantings, sparsely scattered, and trees rarely used



THEME DEVELOPMENT PROCESS (CONT.)

PRELIMINARY CONCEPT THEMING

Preliminary theme development for LPP was facilitated through brainstorming by LSD staff: on individual scale, in small group sessions, and through one large group ideation session. Information summarized in the survey results, images of desert waterways, and maps of the Peoria area provided inspiration for formulating streetscape theme ideas. Ideas pulled from previous LSD experience with Valley streetscapes, the Internet, and professional publications were also used to catalyze the brainstorming. From these ideation sessions COP staff selected two of the streetscape theme ideas for further exploration.

The preliminary theme concepts and additional illustrations of ideas for various design elements (including gateway, integrated art, and decorative walls) were presented to the COP Project Manager during an intermediate review session. In the review session, refinements to the concepts were suggested and ideas for additional graphics delineating ideas for theme organization graphics and design element alternatives were proposed.

THEME DEVELOPMENT PROCESS (CONT.)

PRELIMINARY DESIGN THEMES

RIVER CURVES THEME (OPTION ONE)

The river curves theme derives design elements from shapes of rivers and canals that pass through the city of Peoria: the New River, the Agua Fria River, and the CAP Canal. The waterways would be expressed using curvilinear lines transposed over the roadway to define edges of zones. Each zone would represent a segment of the Sonoran Desert landscape through plantings, ground cover, and earth forms. Thematic reinforcement would occur in the vertical plane with walls and artistic elements.

Theme Parameters

This theme was divided into the three zones identified as: Urban/Southern segment with the three rivers primarily contained within the median area, Residential/Middle segment rivers expanding into the right-of-way landscape areas at nodes and important intersections, and Rural/Northern segment with the rivers utilizing the entire right-of-way. In each

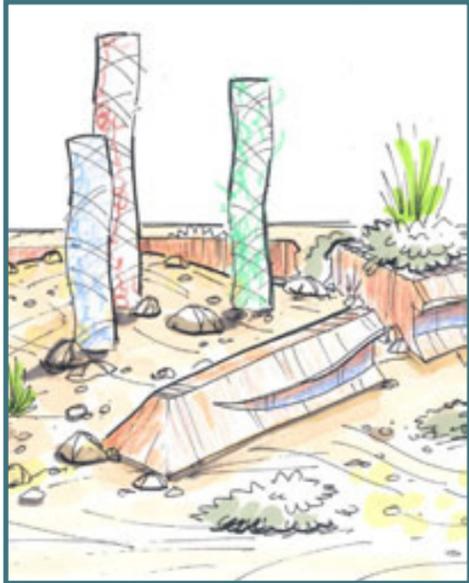
segment the three rivers curved through the ground plane with the spaces between rivers containing differentiated plant palettes

Forms

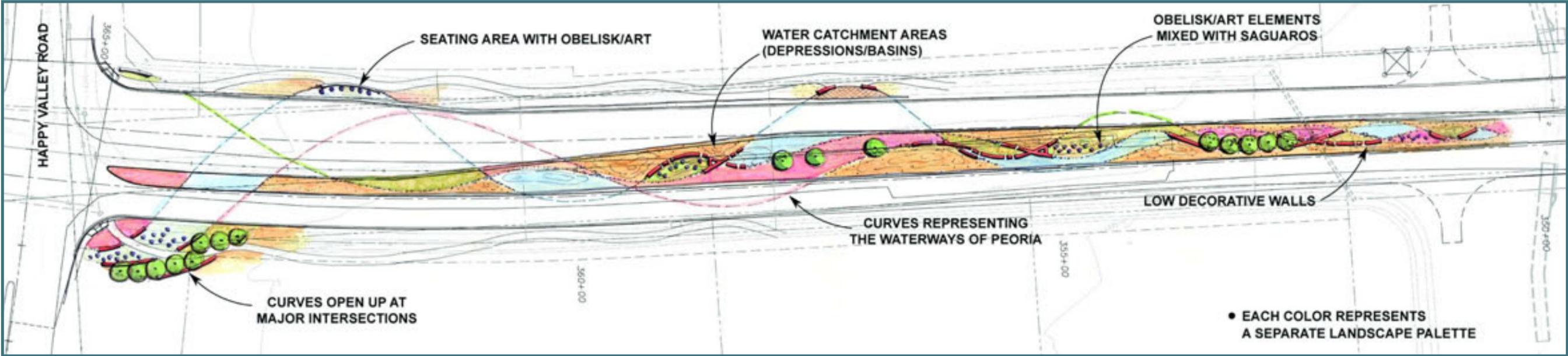
- curved, "river" boundaries between zones
- walls generally arranged in groupings and at curve intersections
- berms and depressions increase sense of verticality

Design element relationships

- berms and walls occur together in groupings
- curves expand into parkway and back-of-curb in newer and rural areas
- planting character areas each contain indicator species for the Sonoran Desert area represented
- each zone emphasizes the use of a different planting each character area



Median Landscape



Concept A: River Curves Theme

THEME DEVELOPMENT PROCESS (CONT.)

WATERWAY THEME (OPTION TWO)

The waterway theme would create a representative waterway in the median through the use of elements symbolizing washes, rivers, and canals. The theme uses the rock and sand textures of wash and river; earth forms of mounds, wash channels, and depressions; walls and structures of canals; and waterside plant groupings to place the concept of a desert waterway as the focal point for the parkway. The entire LPP would, without physically using water, tell the story of waterways in the desert, bringing the characteristics of desert watercourses to people driving along the parkway.

Theme Parameters

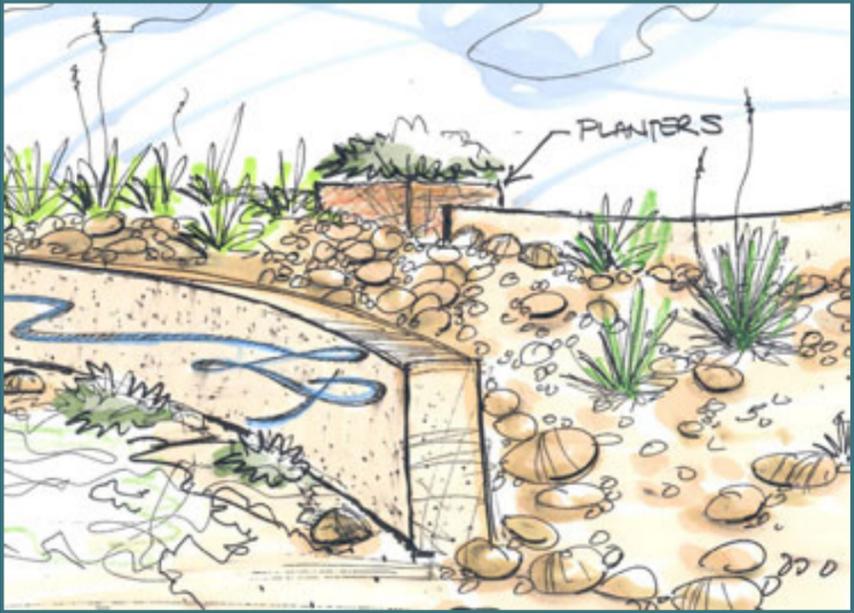
This theme was divided into the three zones: an urban waterway representing canal structures, a transition waterway shaped to resemble channelized wash areas and urban rivers, and a rural waterway flowing and winding through the median with the characteristics of a natural wash.

Forms

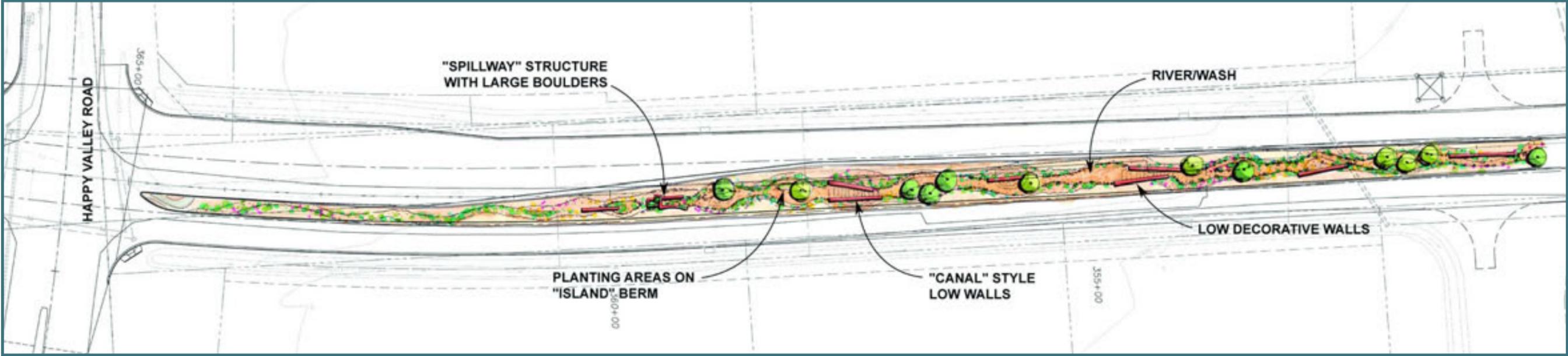
- walls and ground surfacing define waterway edges
- depressions accentuate wash forms
- berms emulate canal forms and island shapes

Spatial relationships

- earth forms look man-made in urban areas and mimic natural forms in rural areas
- Character Areas have highest concentration of native species planted in the rural areas and more adapted low water-use species in the urban areas.



"Spillway" Area



Concept B: Waterway Theme

THEME DEVELOPMENT PROCESS (CONT.)

PREFERRED CONCEPT

The COP staff selected the Waterway Theme (Option Two) overlaid on the Alternative B Future Growth Model as the preferred concept as presented by LSD during the theme development process.

DESIGN CONCEPT: WATERWAY THEME

The preferred concept combines the waterway theme developed in the preliminary concept phase and applies it to the preferred theme organization model, that was developed with suggestions and comments received during the stakeholders meeting and through refinement of the theme ideas generated in the internal working sessions. The concepts of the theme organization model use Character Areas that are expressed through the design of the medians. Right-of-way areas can provide opportunities to reinforce the design themes further.

THEME ORGANIZATION: ALTERNATIVE B - FUTURE GROWTH MODEL

- Urban/Commercial Character Area
- Residential/Medium-density Character Area
- Rural/Open space Character Area

LANDSCAPE CONCEPTS
• WATERWAY THEME •

URBAN / SOUTHERN SECTION

- STRAIGHT WALLS W/ ARCH. THEMES PARALLEL TO ROAD
- STREET TREES : LINEAR OR OFFSET
- BERMS W/ DEFINED EDGES & MAN MADE FORMS
- RIP-RAP & DG HARD/SHARP EGGED IN NON-LOCAL COLORS

RESIDENTIAL / TRANSITION SECTION

- STRAIGHT WALLS W/ STYLIZED THEMES ANGLED TO ROAD
- GROUPINGS & SCATTERED TREES
- BERMS W/ SOFTENED EDGES
- BOULDERS & RIP RAP MIX , DG IN LOCAL COLORS

RURAL / OPEN SPACE / NORTHERN SECTION

- CURVILINEAR WALLS W/ FORMS OF MOUNTAINS & WAVES
- SCATTERED TREES - FEW - MORE SAGUAROS
- BERMS SOFTLY ROUNDED IN NATURAL SHAPES
- BOULDERS OF LOCAL SURFACE ROCK & DESERT PAVEMENT SALVAGED LOCALLY

CONCEPT B

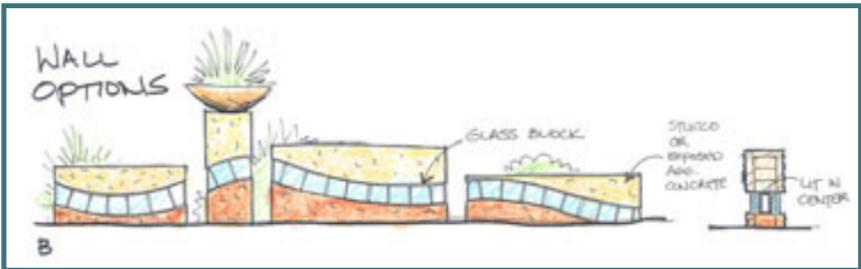
THEME DEVELOPMENT PROCESS (CONT.)

AESTHETICS

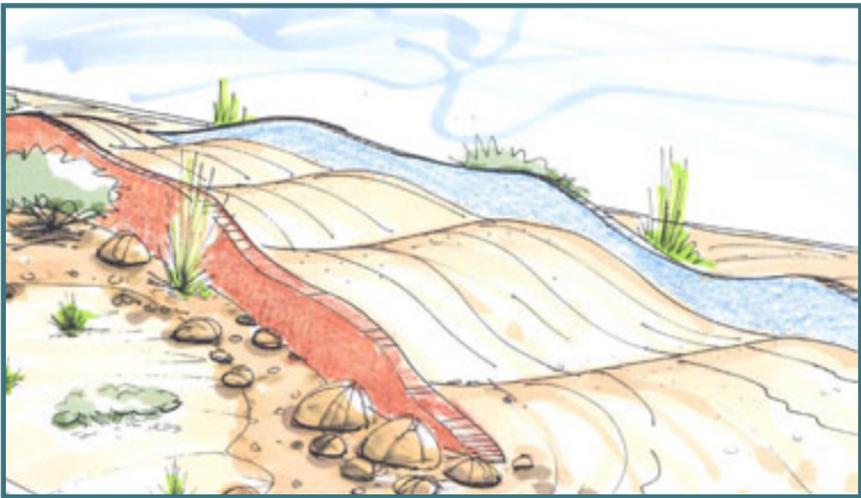
The aesthetics for a design theme fine-tunes the concept by considering design at a detailed level of creativity. This is the arena for selecting colors, materials, and textures. The aesthetics also define the style of the objects that make up the design theme—be it modern architectural, earthy organic forms, or a juxtaposition of both.

Element Details and Distribution

- Walls – Used in medians to provide vertical separation between traffic lanes, provide vertical surface to accentuate theme ideas and artistic elements, and provide nighttime interest with the use of lighting

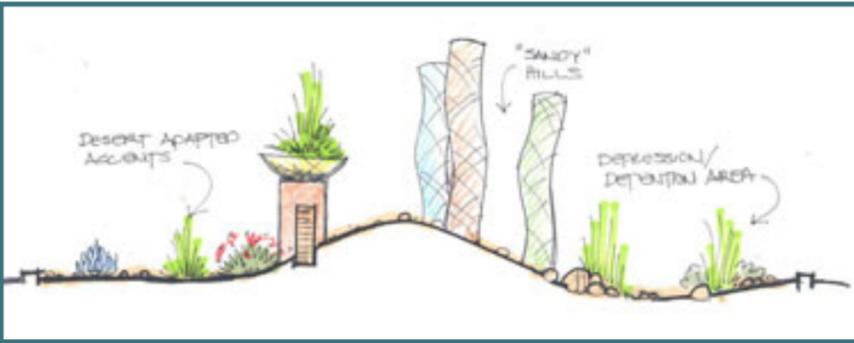


Median Wall Elevation



"Canal" Style Median Walls

- Architectural sculpture elements – Concentrated in areas of heavy use and visual prominence, such as intersections, trailheads, bridge abutments, seating areas, and gateways. They can be incorporated into streetscape amenities like bus stops or stand-alone focal points for public art displays.



Median Cross Section

- Plantings – The species used and the density will depend on the zone; each will have a planting palette with desert-adapted selections, with the densest plantings in urban areas and the least impact occurring in the rural areas.
- Gateways – Intersections are the best opportunities to create a sense of entry along the LPP. Use one or both sides of an intersection to incorporate the theme elements of art, landforms, plantings, and lighting in large presentations.
- Materials – The materials used throughout the theme determine the overall look and feel. Rock and riprap enhance the zone characteristics, with native stone types being used in the rural areas and nonnative types with broken man-made shapes in the urban context. Walls with stucco finish lend themselves to being easily shaped into architectural or naturalistic forms and easily take on different textures to reinforce the ideas expressed in each Character Area zone.

- Amenities – The streetscape amenities are the furniture, lighting, trash receptacles, and bike racks that provide functional convenience for the users. Though serving a functional purpose, they are opportunities to integrate art and icon elements. Amenities can be selected to reinforce the design theme through their shapes, colors, and materials.



Panel Style Banners

GENERAL

The design philosophy is the underlying principle for the design criteria as well as for the guidelines for other aesthetic opportunities within the parkway. The Design Criteria offer measurable parameters for the development, while the Design Guidelines are ideas and instances offered as examples without quantitative specifics.

THEME NARRATIVE/DESIGN PHILOSOPHY

Peoria has a unique collection of desert water bodies within its boundaries, including rivers, canals, large washes, and a large recreational lake. The idea behind the waterway theme is to elevate the water's edge and the shapes and forms of waterways to the everyday consciousness of the people of Peoria and to interject local water body types into desert living without using flowing or standing bodies of water to achieve this design goal. There are several important precepts in developing the waterway theme to achieve that goal.

WATERWAY EXPRESSION

The first precept in the theme is to use the shapes of a wash edge, canal structures, and river edge along with landforms and plantings resembling the conditions along waterways to express the theme within the parkway. The median islands provide the best opportunity to realize this design idea. The long stretches of linear landscape area provide an ideal location for a wash and canal design to play out. Landscape berms and long depressions in the median grade can be used in exaggerated shapes to play up the vertical changes in the ground plane and to provide visual thematic interest.

STRUCTURES, WALLS, AND ARCHITECTURAL SCULPTURE ELEMENTS

The second precept within the theme is to use structures and walls to carry architectural and artistic embellishments, thus providing additional visual interest along the parkway. The walls will also enhance the vertical grade changes used in the theme's first precept. They are planned to include lighting for nighttime interest as well. Other structures can include artistic elements and planters to increase the verticality—or sense of verticality—of the median design. Architectural sculpture elements introduced to the theme reflect the waterway theme through the use of shapes, textures, and materials of waterways and the ways mankind uses water. Sinuous curves following the pattern of a river or wash moving through the land, shapes of sails on the water, the patterns formed by water at the lake edge, the shape of sails silhouetted against the land, and the shapes of canal embankments are some of the ideas expressed in the architectural sculpture elements proposed. Textures and materials like riprap, gabion-like structures, wash sands, ripstop sail fabric, or native surface stone are used because they are either native to the washes and rivers surrounding the parkway or are materials used generally in the harnessing of desert watercourses. Functions of the waterways as well as these physical manifestations of waterway use are all possibilities for artistic inspiration in the theme development.

LANDSCAPE DESIGN AND PLANTING

The landscape design and plant palette is the third precept for the design theme. Starting with desert-adapted plant species and, especially, using species that grow or resemble plants growing along water bodies in the Sonoran Desert. Each Character Area of the design theme has a separate plant palette with specific plants to be used as indicator species to help identify that area. The landscape design for each Character Area is defined by a separate set of criteria: the urban areas use flowering groundcover and bold accent plants in massings and in fairly dense formal plantings, whereas the rural areas use sparsely scattered native and naturalized species to mimic the surrounding desert more closely. The residential and medium-density areas are opportunities to blend these concepts as a transition; some plant massings are used—mostly at nodes and intersections while using more natives with moderate spacing and informal groupings. Trees also play an important role in defining the Character Areas, primarily in their limited numbers and their function to screen undesirable views. All segments will have some trees, though none use trees as the dominant feature and all use trees in densities far below the COP landscape guidelines. In the urban areas street trees are appropriate, preferably in small groupings and of species used along canals. In the transitional medium-density areas, trees are primarily desert natives and used to screen views or to indicate of an activity node. The rural areas use the least number of trees; functions are limited to defining “places” and to screening. Only species native to the region are used in rural areas.

GATEWAYS

The use of gateways along the LPP is the fourth precept of the theme. These areas are the least defined by design criteria for several reasons. To begin, they are intended for areas behind the future curb, in areas not currently owned by COP. They are dependent on the artistic theme elements selected for the LPP. Gateways are also dependent on the future locations of road crossings and could shift as planning is realized through construction document development. Using dominant materials from the waterway median design on a magnified scale along with concentrated artistic elements are the underlying guides to linking the gateways to the overall parkway theme. Texture, form, color, height, and light are the primary ways the gateways differentiate themselves from the background developments.

OTHER THEME OPPORTUNITIES

RIGHT-OF-WAY DESIGN DEVELOPMENT

Areas of design outside the parkway median are opportunities to reinforce the design theme. Bus stops, seating areas, barrier walls, and landscape areas are a few of those design elements that lend themselves to incorporating the waterway theme. Integrating art into functional forms or accenting the right-of-way with public art and architectural sculpture elements are ways to connect the parkway across lanes of traffic. Employing the same principles of the design theme landscape forms, materials, and planting palette behind the curb would have the greatest impact in linking both sides of areas: the median and back-of-curb. Guidelines are provided in this manual to assist in right-of-way designs.

ICONOGRAPHY

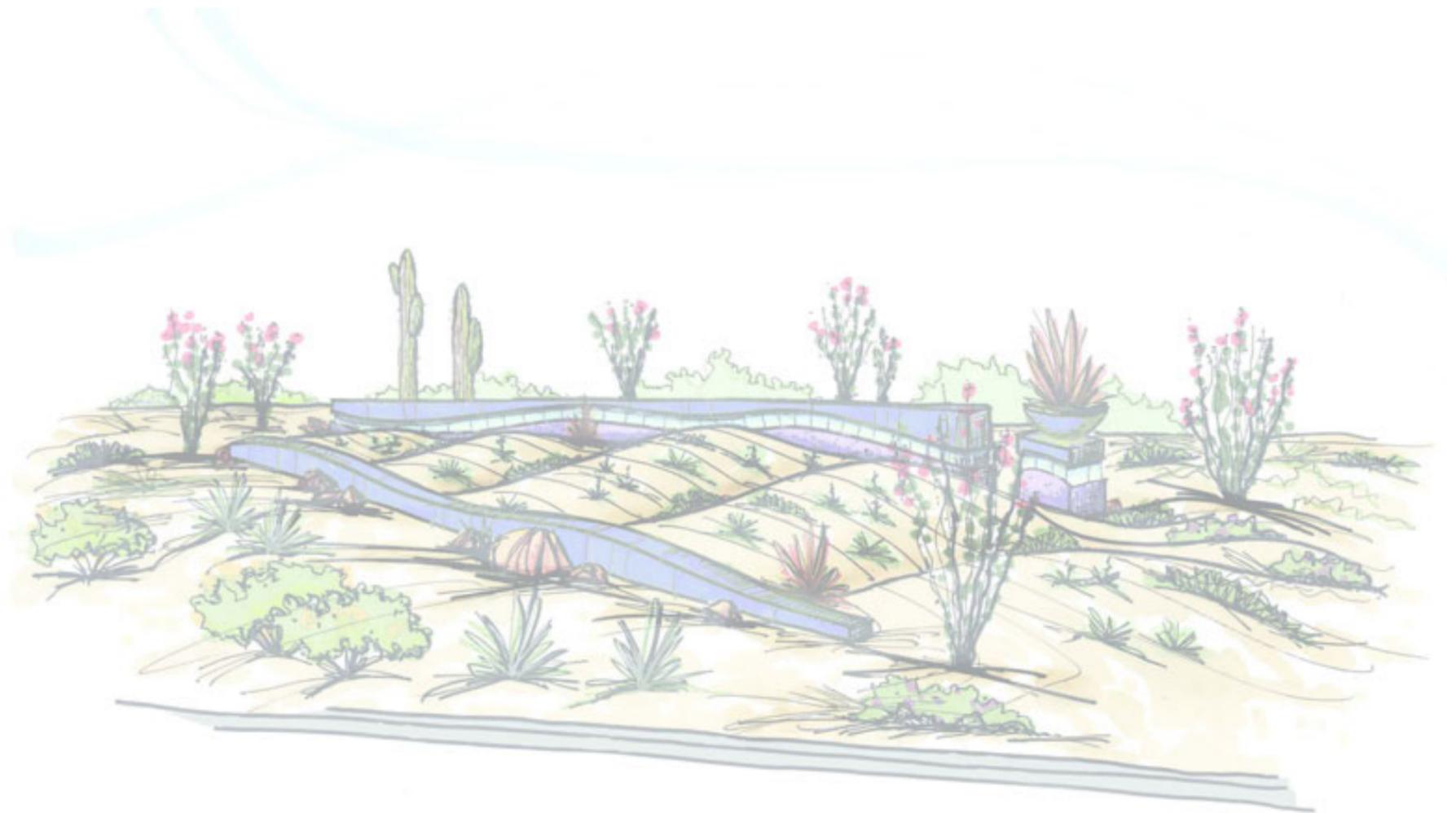
Creating a memorable symbol for the parkway is an additional goal for the theme development—be it a set emblem as in a stand-alone graphic or a symbolic shape repeated in several materials. Several options for the icon have been proposed in this document for consideration and further development. Many draw forms from the LPP theme or use elements from the theme coupled with artistic elements. The goal is to generate an icon that can function throughout the city of Peoria as a timeless identifier.

DESIGN CRITERIA

The design criteria are the measurable and quantifiable parameters set forth to ensure that the intent of the design philosophy is carried out in the streetscape development for areas controlled by the COP. The criteria established herein supercede the COP design development standards for landscape and materials.

MEDIAN DEVELOPMENT

The landscape area within the median islands is the only area along the LPP directly within COP design control and, therefore, must have a cohesive visual presence throughout its entire length. The design criteria established in this document are intended to ensure that the LPP will sufficiently portray its identity and visual character. The theme organization, the design concept, and the design philosophy are integrated in the development of the parameters.



URBAN/COMMERCIAL CHARACTER AREA

STREETSCAPE ELEMENTS

Inert materials

- Decomposed granite (1/2" minus)
- Cobble (1"- to 3"-diameter)
- Riprap (6"- to 18"-diameter)
(see materials palette on page 21)

Walls

- 12" to 30" (maximum)-clear height
- 30 l.f. to 50 l.f. of wall to every 100 l.f. of median wide enough to allow walls (see typical plan)
- Placed in grouped series of two to five segments
- Straight runs of walls parallel to traffic lanes, often overlapping each other to form canal-like runs
- Architectural forms, level or angled surfaces and edges
- Materials (combined or used alone)- stucco, glass block, tile, or solid surface acrylic
(see wall designs on page 19 and materials on page 20)

Landscape

Medians are planned and designed for a future third traffic lane expansion in both travel directions; use setbacks projected from future curb lines to locate trees, accents, and large shrubs. (see landscape materials and preliminary planting palette on page 18)

Trees

- 2 to 3 per 100 l.f. – 20'-minimum spacing
- Application – screen undesirable views
- Setbacks – 5' from future back-of-curb

Shrubs

- Setbacks – 5' from back-of-curb
 - ▲ Large (5' mature height) – 2 plants per 100 l.f.
 - ▲ Medium (3' mature height) – 15 plants per 100 l.f.
- Setbacks – 3' from back-of-curb
 - ▲ Small (2' mature height) – 12 plants per 100 l.f.

Accents

- 10 plants per 100 l.f.
- Setbacks – 5' from future back-of-curb

Groundcover and grasses

- 100 plants per 100 l.f. (24"-spacing)
- Application – plant in massings of 25 plants or more
- Setbacks – 3' from back-of-curb

Berms/Washes

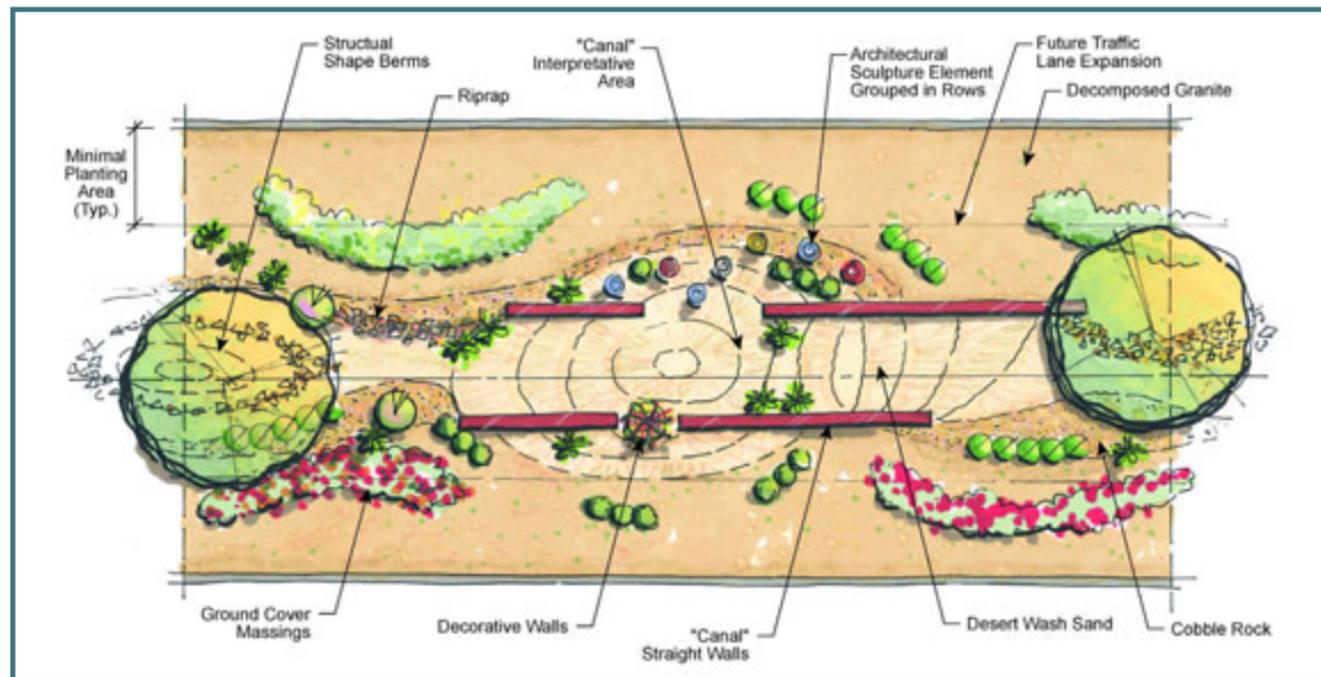
- Formed from engineered shapes and defined gradients and edges with a clear-cut separation of the elevated/depressed sections relative to the median curb elevation
- 30" – Maximum grade change above or below median curb elevation

Architectural sculpture elements

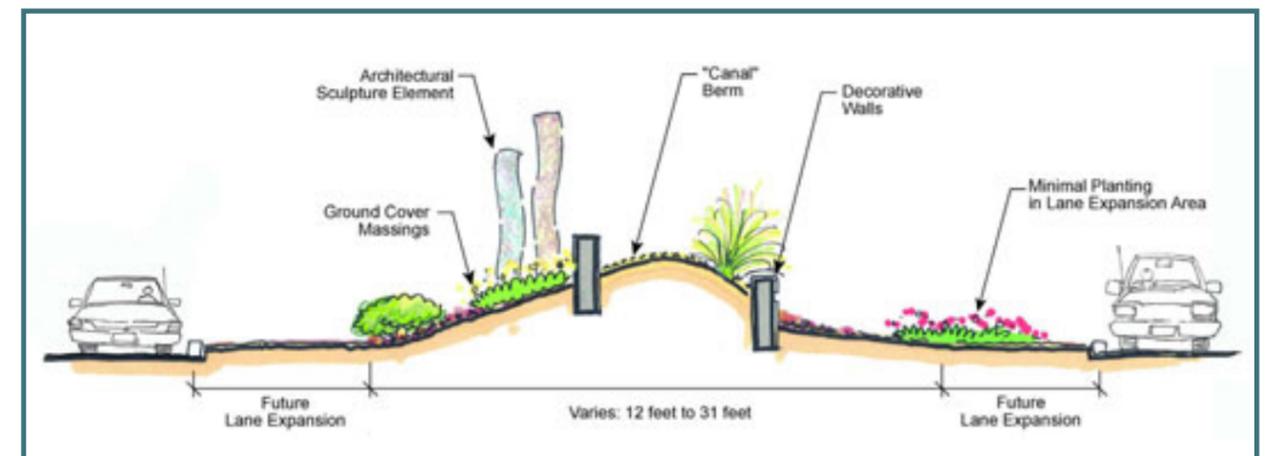
- Distributed throughout median areas; cluster in groups of three to seven for every 500 l.f. of median wide enough to allow architectural sculpture elements (see typical plan)
- Setbacks – 5' from future back-of-curb

Colors

- Inert materials – browns with accents of blues, greens, and grays (see materials palette on page 21)
- Walls – warm earth tones, gold, rust, red, and yellows; accented with greens and blues. Bright solid areas of these tones. (see wall colors on page 20 and alternate palettes on page 19)



Typical 100' Plan



Typical Cross Section

RESIDENTIAL/MEDIUM-DENSITY CHARACTER AREA

STREETSCAPE ELEMENTS

Inert materials

- Blended 50% decomposed granite (1/2" minus) and 50% salvaged soil
- Blended 50% cobble and 50% river rock (1"- to 3"-diameter, both)
- Blended 50% riprap and 50% native boulders (6"- to 18"-diameter, both)
(see materials palette on page 21)

Walls

- 12" to 30" (maximum)-clear height
- 20 l.f. to 40 l.f. of wall to every 100 l.f. of median wide enough to allow walls (see typical plan)
- Scattered placement and in groups up to three segments
- Slight curves allowed and angled placement to traffic lanes, overlapping occasionally
- Man-made forms inspired from natural forms, curved and wave top of wall designs, softened edges

- Materials (combined or used alone)- stucco, glass block, tile, or solid surface acrylic
(see wall designs on page 19 and materials on page 20)

Landscape

- Medians are planned and designed for a future third traffic lane expansion in both travel directions; use setbacks projected from future curb lines to locate trees, accents, and large shrubs. (see landscape materials and preliminary planting palette on page 18)

Trees

- 2 per 100 l.f. - 20'-minimum spacing
- Application - screen undesirable views
- Setbacks - 5' from future back-of-curb

Shrubs

- Setbacks - 5' from back-of-curb
 - ▲ Large (5' mature height) - 3 plants per 100 l.f.
 - ▲ Medium (3' mature height) - 18 plants per 100 l.f.
- Setbacks - 3' from back-of-curb
 - ▲ Small (2' mature height) - 10 plants per 100 l.f.

Accents

- 7 plants per 100 l.f.
- Setbacks - 5' from future back-of-curb

Groundcover and grasses

- 50 plants per 100 l.f. (24"-spacing)
- Application - plant in massings of up to 20 plants per group
- Setbacks - 3' from back-of-curb

Berms/Washes

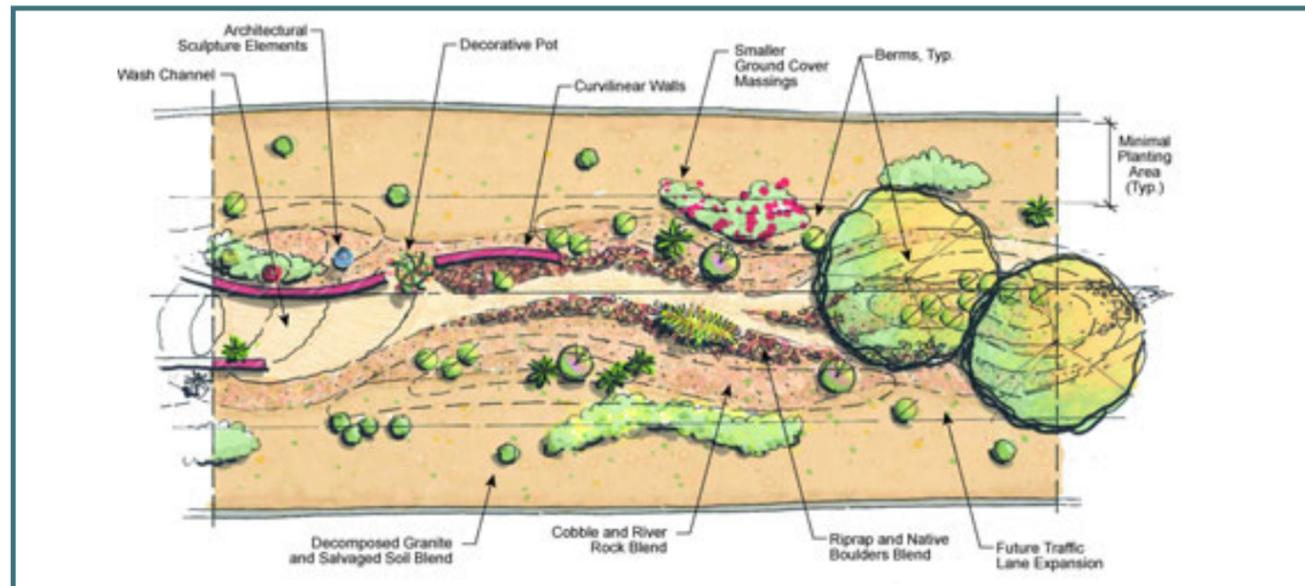
- Formed from slightly softened man-made shapes and less engineered gradients between the elevated/depressed sections relative to the median curb elevation
 - ♦ 30" - Maximum grade change above or below median curb elevation

Architectural sculpture elements

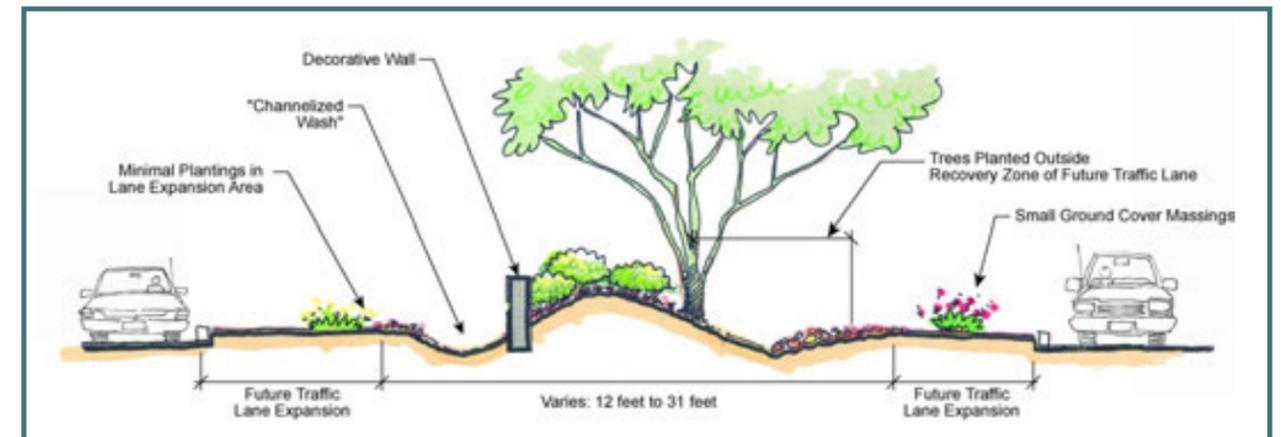
- Clustered in groups of three to seven elements at places of interest and near major intersections; in median areas wide enough to allow architectural sculpture elements (see typical plan)
- Setbacks - 5' from future back-of-curb

Colors

- Inert materials - native stone colors to the area surrounding LPP mixed with browns accented with blues, greens, and grays. (see materials palette on page 21)
- Walls - warm earth tones, gold, rust, red, and yellows; accented with greens and blues. Washes of these tones with accent areas of bright solid tones.
(see wall colors on page 20 and alternate palettes on page 19)



Typical 100' Length Plan



Typical Cross Section

RURAL/OPEN SPACE CHARACTER AREA

STREETSCAPE ELEMENTS

Inert materials

- Salvaged soil
- River rock (1"- to 3"-diameter)
- Native boulders (6"- to 18"-diameter)
(see materials palette on page 21)

Walls

- 12" to 30" (maximum)-clear height
- 20 l.f. to 40 l.f. of wall to every 100 l.f. of median wide enough to allow walls (see typical plan)
- Scattered placement and in groups up to three segments
- Slight curves allowed and angled placement to traffic lanes, overlapping occasionally
- Man-made forms inspired from natural forms, curved and wave top of wall designs, softened edges
- Materials (combined or used alone)- stucco, glass block, tile, or solid surface acrylic
(see wall designs on page 19 and materials on page 20)

Landscape

Medians are planned and designed for a future third traffic lane expansion in both travel directions; use setbacks projected from future curb lines to locate trees, accents, and large shrubs. (see landscaping materials and preliminary planting palette on page 18)

Trees

- 1 per 300 l.f. – maximum spacing 1,000 l.f.
- Application – visual indicator of "place"
- Setbacks – 5' from future back-of-curb

Shrubs

- Setbacks – 5' from back-of-curb
 - ▲ Large (5' mature height) – 3 plants per 100 l.f.
 - ▲ Medium (3' mature height) – 13 plants per 100 l.f.
- Setbacks – 3' from back-of-curb
 - ▲ Small (2' mature height) – 15 plants per 100 l.f.

Accents

- 7 plants per 100 l.f.
- Setbacks – 5' from future back-of-curb

Groundcover and grasses

- 20 plants per 100 l.f. (24"-spacing)
- Application – plant in massings of up to 10 plants per group
- Setbacks – locate along "wash" edges only

Berms/Washes

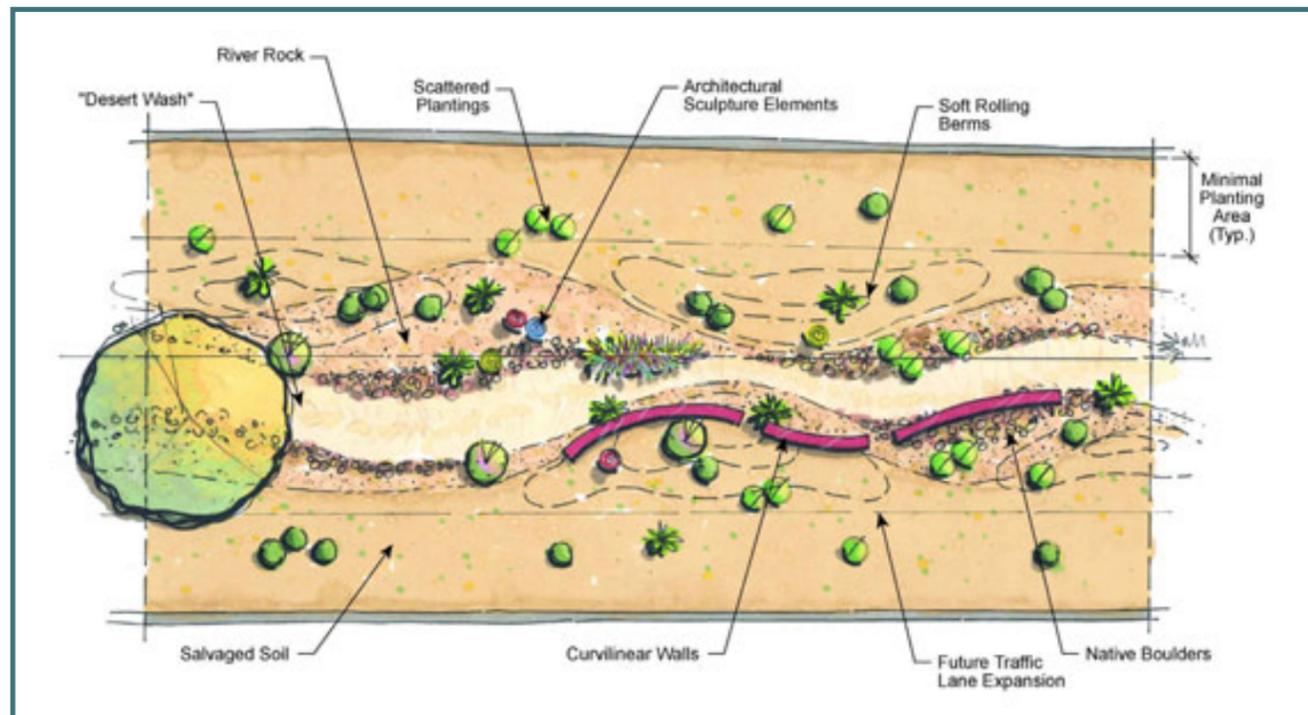
- Formed from softly rounded shapes and naturalistic looking gradient of the elevated/depressed sections relative to the median curb elevation
- 30" – Maximum grade change above or below median curb elevation

Architectural sculpture elements

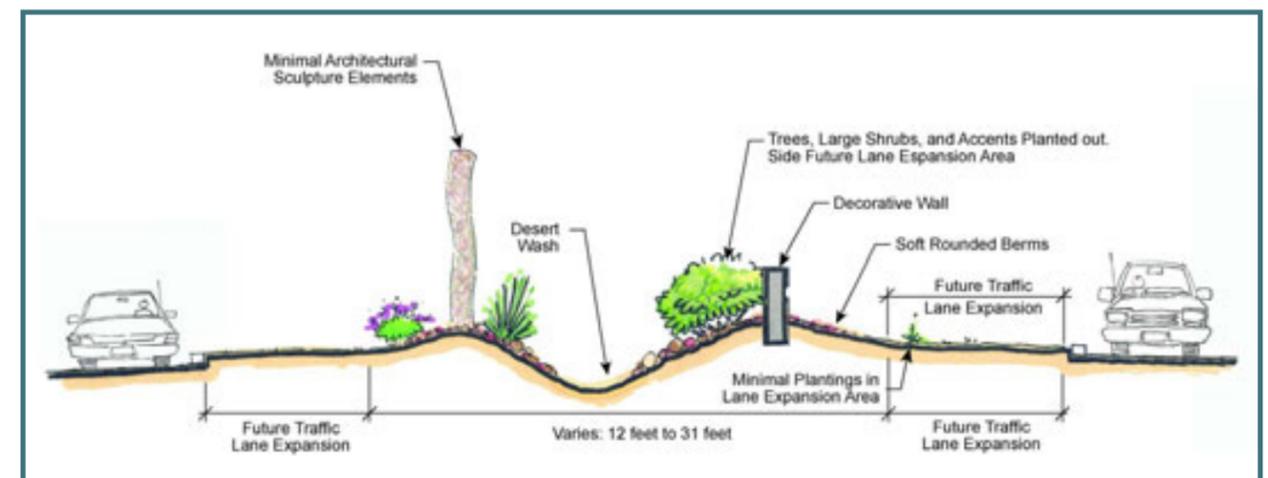
- Clustered in groups of three to five elements at places of special interest in medians wide enough to allow architectural sculpture elements (see typical plan)
- Setbacks – 5' from future back-of-curb

Colors

- Inert materials – native stone to the area surrounding the LPP. (see color palette on page 21)
- Walls – warm earth tones, gold, rust, red, and yellows; accented with greens and blues. Muted washes of these tones. (see wall colors on page 20 and alternate palettes on page 19)



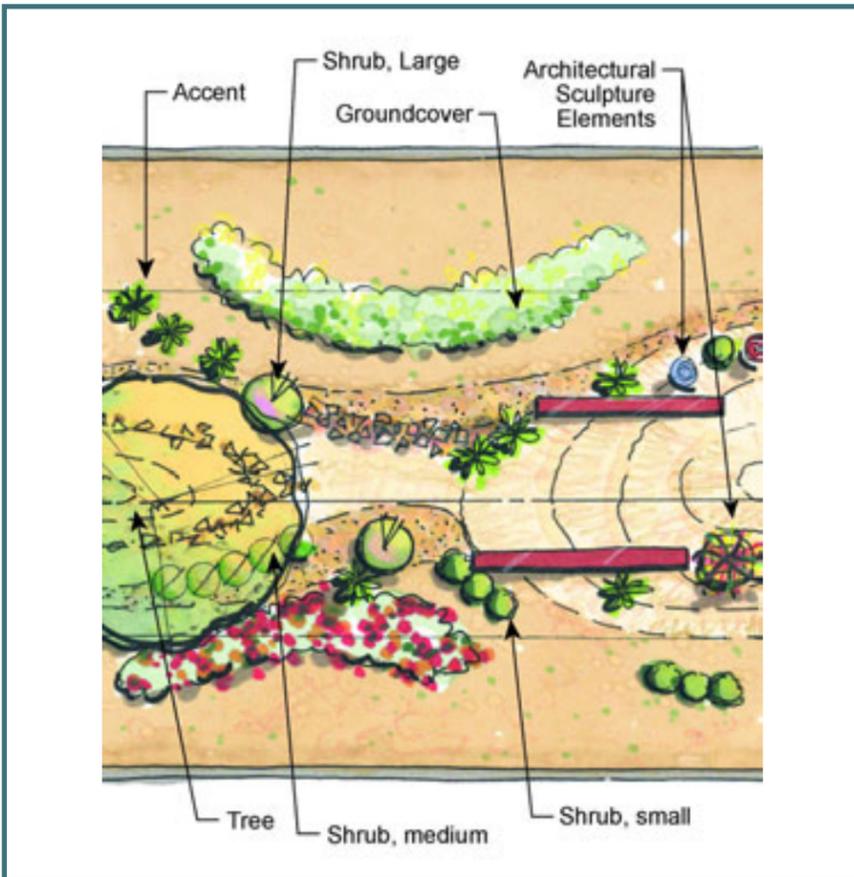
Typical 100' Length Plan



Typical Cross Section

LANDSCAPE MATERIALS

The landscape planting materials for the design concept were selected based on the survey results and the Arizona Department of Water Resources (ADWR) Drought Tolerant/Low Water Use Plant List. Desert-adapted species selected with bold forms and colors.



Typical Landscape Area

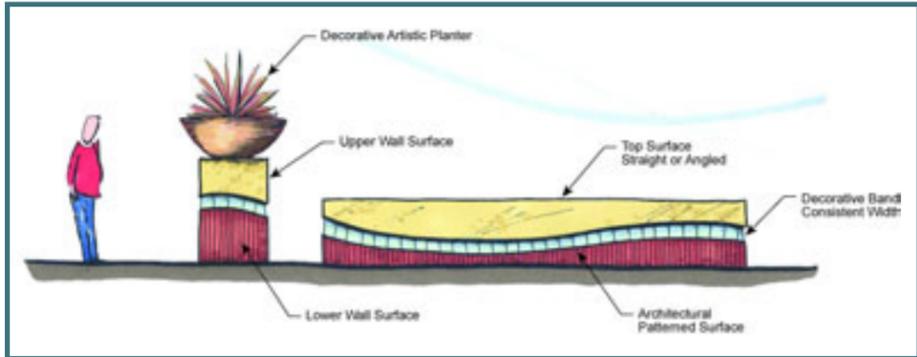
PLANTING PALETTE LIST

PRELIMINARY PLANT PALETTE

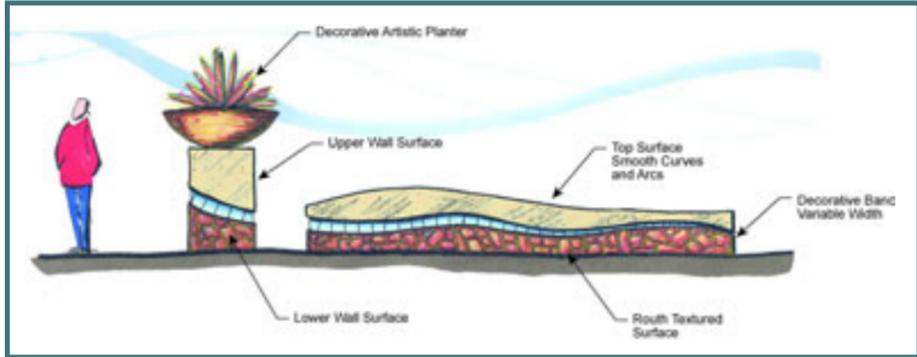
COMMON NAME	BOTANICAL NAME	COMMON NAME	BOTANICAL NAME
Trees		Ground Covers	
Blue Palo Brea	Cercidium praecox	Common Ruellia	Ruellia brittoniana
Foothills Pale Verde	Cercidium microphyllum	Desert Marigold	Baileya multiradiata
Desert Museum	Cercidium hybrid "Desert Museum"	Bush Morning Glory	Convolvulus cneorum
Ironwood	Olneya tesota	Trailing Gazania	Gazania rigens leucolaena
Thornless Mesquite	Prosopis chilensis "Phoenix"	Yellow Malephora	Malephora leteola
Native Mesquite	Prosopis velutina	Verbena	Verbena x sissinghurst
Sweet Acacia	Acacia smallii	Domianita Daisy	Chrysactinia mexicana
Fruitless Olive	Olea europaea "Swan Hill"	New Gold Lantana	Lantana species 'New Gold'
Sissoo Tree	Dalbergia sissoo	White Rain Lilly	Zepheranthes candida
Shrubs		Accents	
Sierra Linda Autumn Sage	Salvia greggii "Sierra Linda"	Century Agave	Agave americana
White Bursage	Ambrosia dumosa	Narrow-Leaf Agave	Agave angustifolia 'marginatus'
Red Bird of Paradise	Caesalpinia pulcherrima	Smooth Agave	Agave desmettiana 'Variegata'
Blue Mist	Caryopteris x clandonensis 'Dark Knight'	Ocahui Agave	Agave ocahui
Hop Bush	Dodonaea viscosa	Aloe Vera	Aloe vera
Brittle Bush	Encelia farinosa	Regal Mist Deer Grass	Muhlenbergia capillaries
Turpentine Bush	Eracameria loricifolia	Autumn Glow Muhly Grass	Muhlenbergia lindheimeria 'Autumn Glow'
Valentine Bush	Eremophila maculate 'Valentine'	Desert Milkweed	Asclepias subulata
Gopher Plant	Euphorbia biglandulos	Desert Spoon	Dasyilirion wheeleri
Red Justicia	Justicia ovata	Saguaro	Carnegiea gigantea
Mexican Honeysuckle	Justicia spicigera	Golden Barrel Cactus	Enchinocactus grusonii
Creosote Bush	Larrea tridentata	Hemp Plant	Agave sisalana
Rain Cloud Sage	Leucophyllum frutescens 'Rain Cloud'	Ocotillo	Fouquieria splendens
Silver Leaf Senna	Senna phyllodinea	Slipper Plant	Pedilanthus macrocarpus
Vista Jojoba	Simmondsia chinensis 'vista'	Giant Yucca	Hesperaloe funifera
		Purple Prickly Pear	Opuntia violaceae santa rita
		Indian Fig	Opuntia ficus-indica

MEDIAN WALL DESIGN

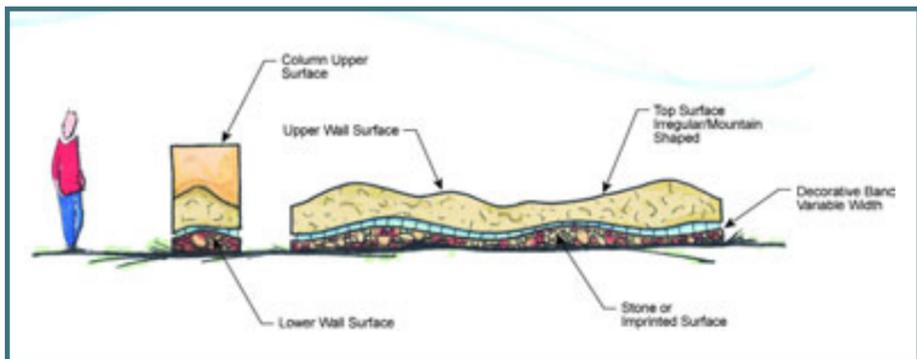
WALL DESIGNS AND MATERIALS



Urban/Commercial Character Area

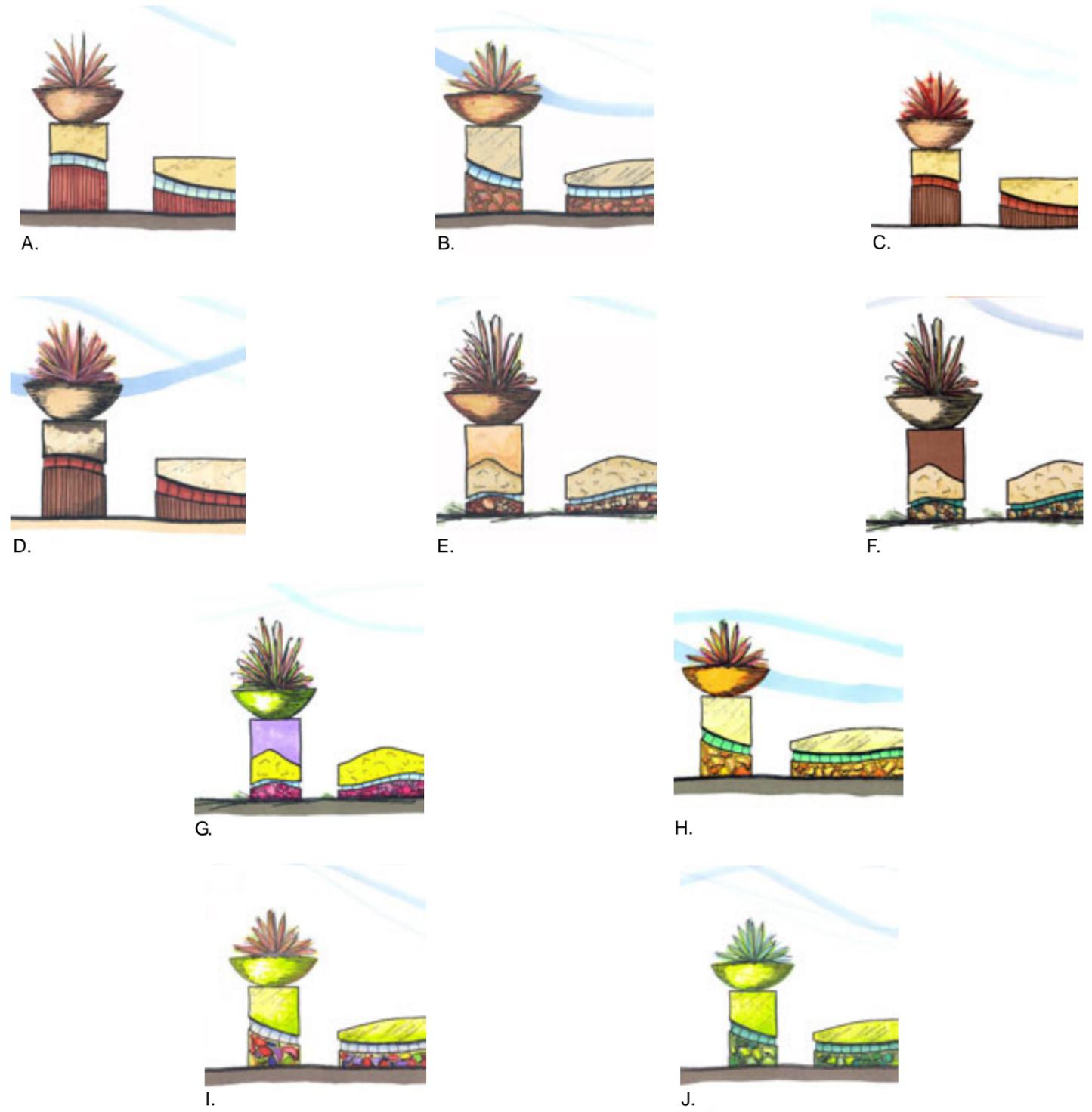


Residential/Medium-density Character Area

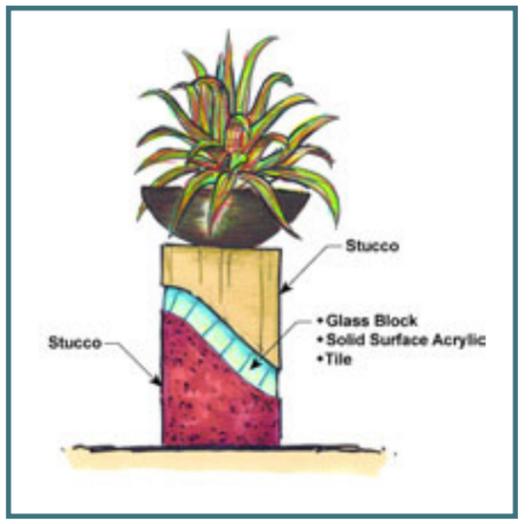
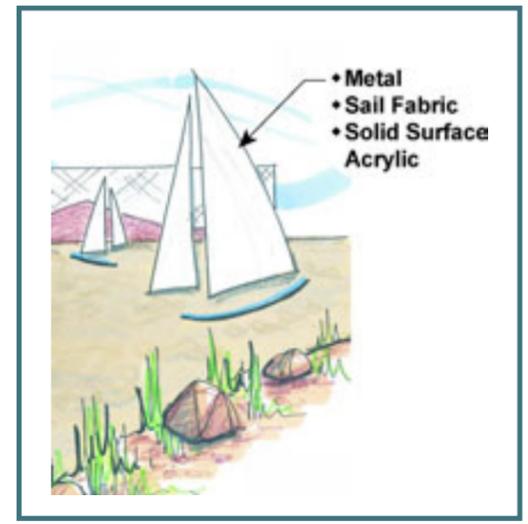


Rural/Open Space Character Area

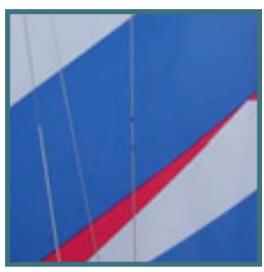
ALTERNATIVE FINISHES AND COLORS



**WALL AND ARCHITECTURAL SCULPTURE
ELEMENT MATERIALS**



Sail Fabric



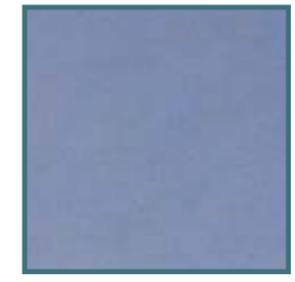
Glass Block



Solid Surface Acrylic

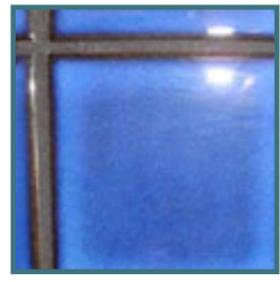


Jade



Blue

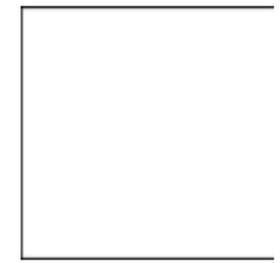
Tile



Blue with Black Grout

COLOR PALETTE

Paint/Stucco (based on Frazee paint samples)



White



Brandons Blue



Mustard Bloom



Coral Gate



Peruvian Orange



Shaded Green



Rust



Buff

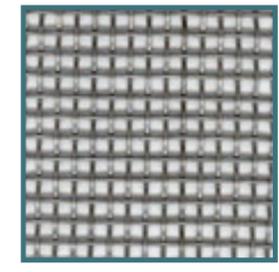


Brown

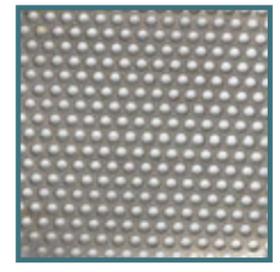
Metal - Steel



Solid - Weathered



Mesh



Perforated



PAVING AND INERT MATERIALS

PAVING

Sidewalks, Curbs, Bull noses

Concrete



Broom Finish



Sand Blasted



Stamped/Patterned

Pavers



Concrete Pavers

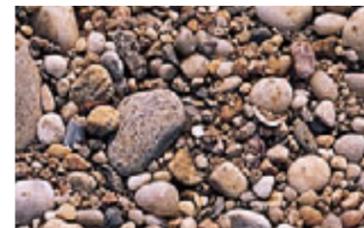
INERT MATERIALS



Desert Wash Sand



Rock Mulch



River Rock 1" to 3" Diameter



Native Boulders
6" to 18" Diameter



Salvaged Soil



Decomposed Granite



"Cobble" Native Rock 1" to 3"
Diameter



Riprap
6" to 18" Diameter

DESIGN GUIDELINES

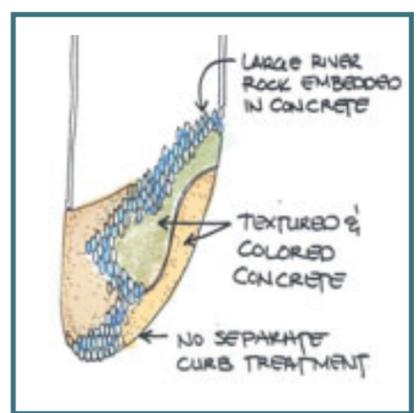
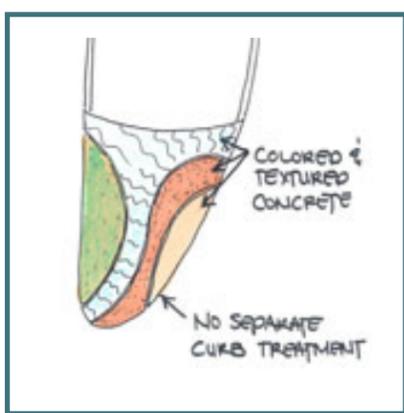
The design guidelines herein are nonquantitative parameters set forth to enumerate the intent of the design philosophy as it could be carried out in streetscape development for areas not constructed by the COP. These guidelines, along with supporting graphics, provide written ideas that reinforce and illustrate the LPP design philosophy's precepts. Development adjacent to LPP is encouraged to utilize this design theme for the development of the back-of-curb streetscape areas.

DESIGN CONSIDERATIONS AND OPPORTUNITIES

CURBS AND PAVING

The curb areas and paving in the rights-of-way offer several opportunities to associate the median materials, colors, and textures with the materials that constitute the bullnoses, sidewalks, and other paving areas.

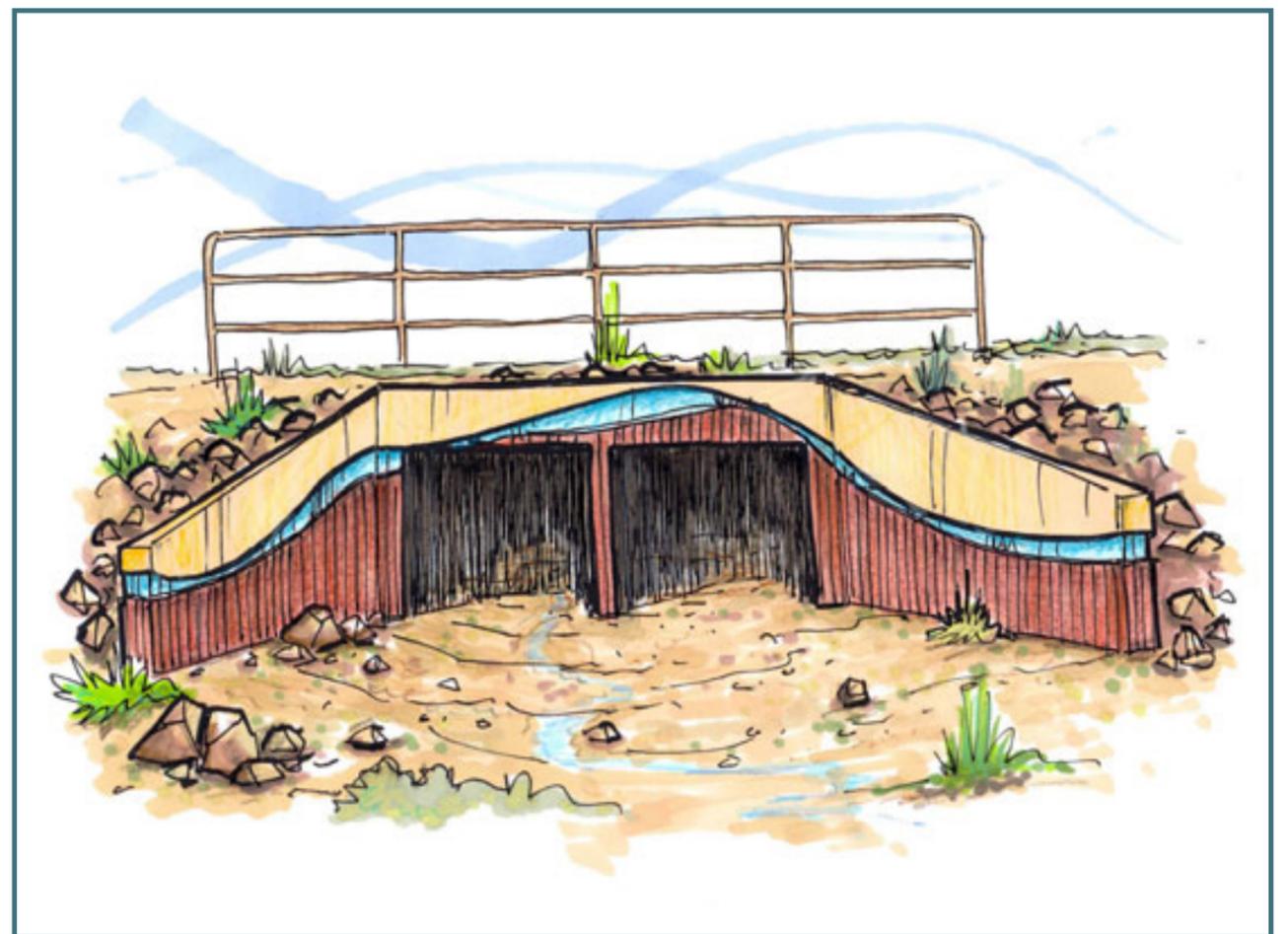
- Bullnoses could have shapes emulating the wall patterns, use similar rock materials
- Sidewalks could use colors similar to the walls or be laid out in sinuous shapes similar to wash or river courses.
- Drainage outflow aprons or headwalls could be made from materials used in the median "wash."



Curb/Median Bull Nose Treatments



Sidewalk and Paving Enlargement Area



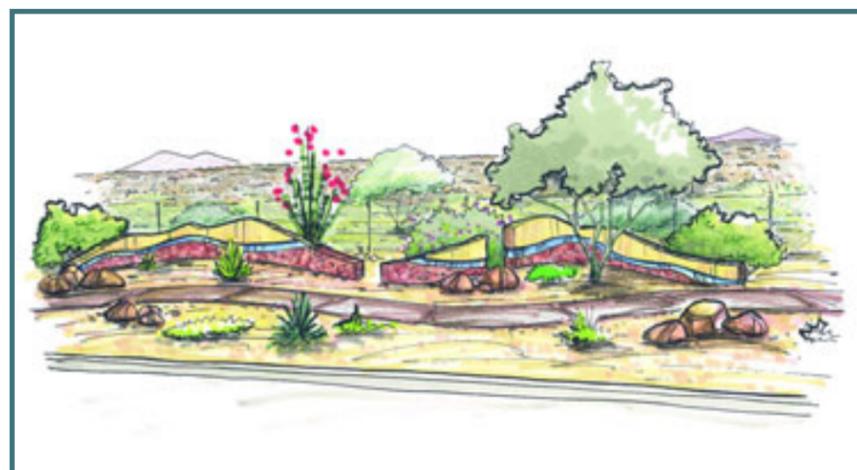
Headwall Perspective Sketch

DESIGN CONSIDERATIONS AND OPPORTUNITIES (CONT.)

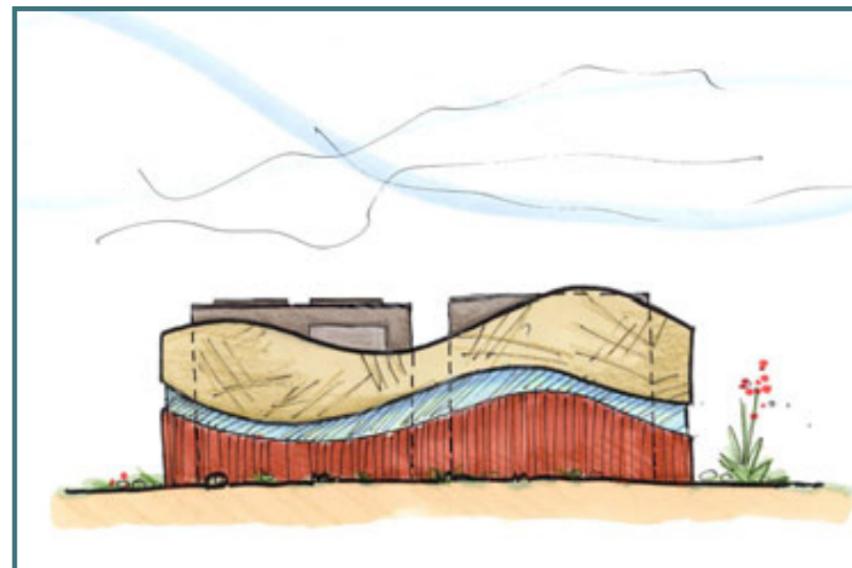
WALLS AND SIGNS

Low decorative walls in the right-of-way can provide reinforcement of the design theme ideas. Matching materials, colors, and/or textures used in the walls are instruments to carry the theme through.

- Provide low screening, block undesirable views, or provide visual interest along stretches of uniformity.
 - ◆ Embankments for the New River Dam
 - ◆ Subdivision walls
 - ◆ Enhance intersections
 - ◆ Utilities and service equipment areas
- Enhance areas of pedestrian activity
 - ◆ Combine with seating or act as seat walls
 - ◆ Separate/protect sidewalks from lanes of traffic
- Vertical surfaces of low walls along the right-of-way are opportunities for business or residential signage. Walls next to seating or pedestrian activity areas could become posting areas for local news and area events.
 - ◆ Shopping center entries
 - ◆ Subdivision signs
 - ◆ Public facility entrances



Low Decorative Walls at New River Dam Embankments



Utility Box Screen Wall

WALL DESIGN ALTERNATIVES



Alternative A



Alternative B

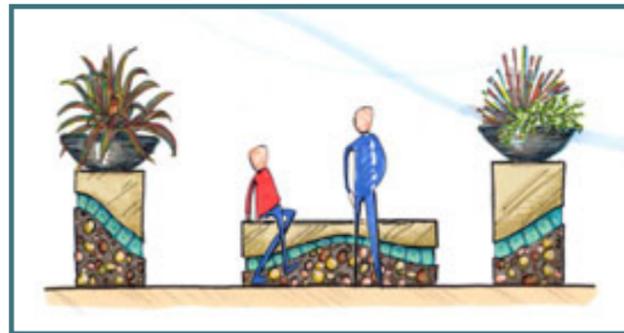


Alternative C

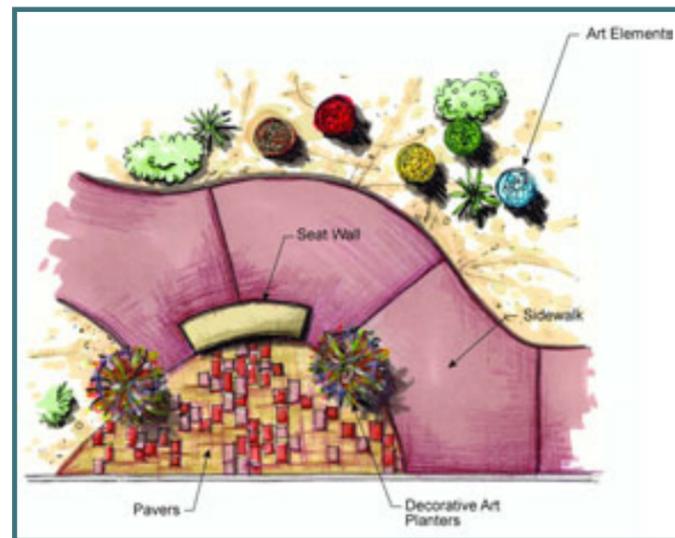
DESIGN CONSIDERATIONS AND OPPORTUNITIES (CONT.)

SEATING AREAS AND BUS STOPS

Opportunities in this section follow many of the examples for theme reinforcement established in the "Walls and signs" section. They are a public convenience that can be enhanced with details of the median walls. Seating areas also provide gathering areas associated with public and integrated art elements.



Seating Area/Bus Stop Elevation



Seating Area/Bus Stop Plan

PUBLIC ART AND INTEGRATED ART

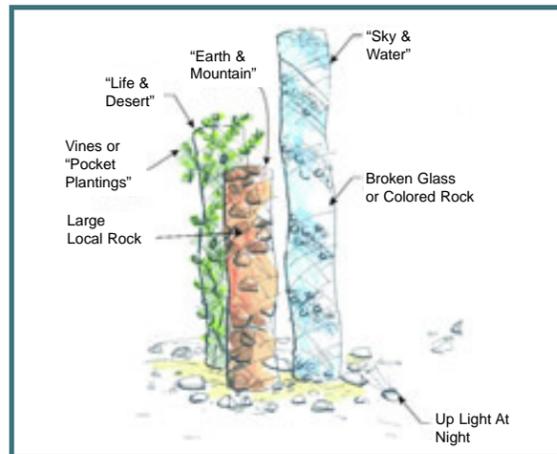
Artistic expressions, either integrated into streetscape elements or as individual artist's creations, can enrich the experience of public spaces and can mark or signal a specific "place."

OPPORTUNITIES

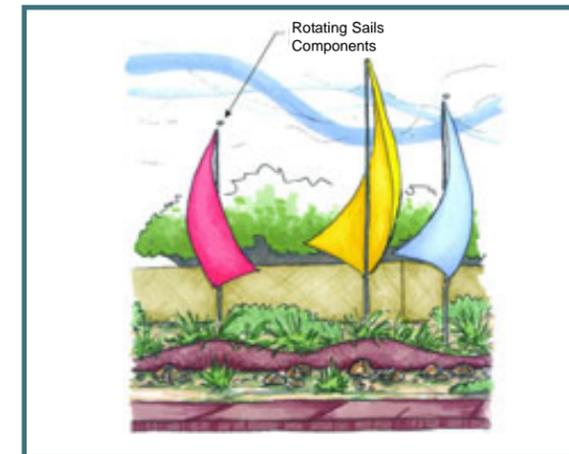
- Seating areas and bus stops
- City gateways
- Subdivision entries
- Trailhead indicators
- Public facility entries

THEME IDEAS

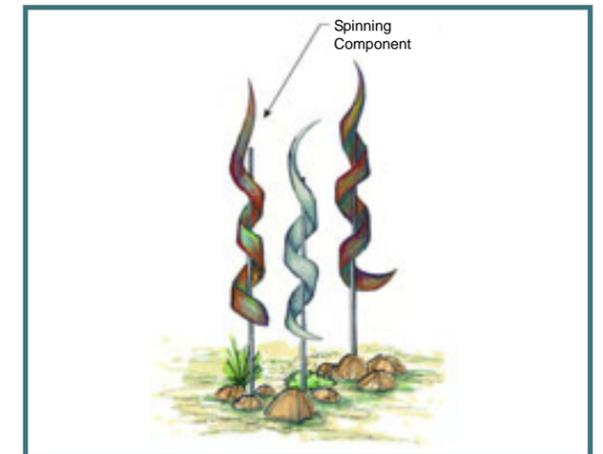
- Sails and spinning sail shapes – water and recreation theme with kinetic component
- Watercourse shapes – sinuous curves on surfaces
- Interpretative desert plant shapes – capturing the desert architecture
- Gabion pillars – vertical columns of manmade structures used to influence water flow
- Wind spirals – capturing wind motion in kinetic art



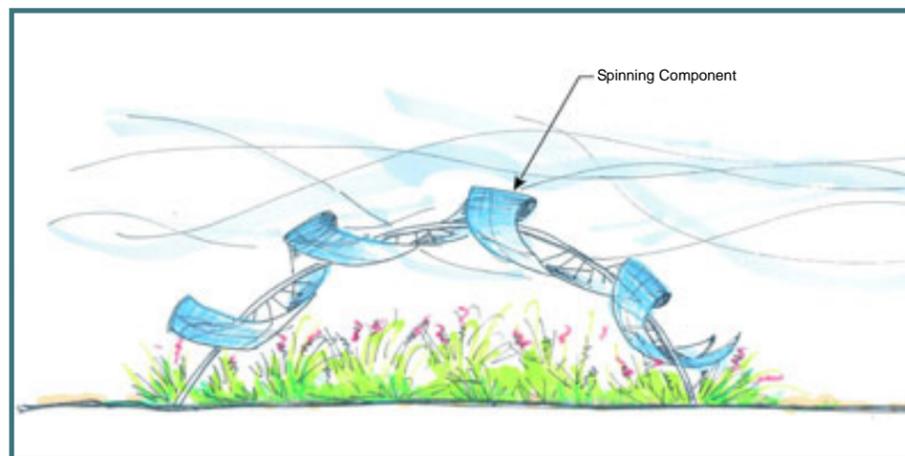
Pillars/Gabion-like Structures



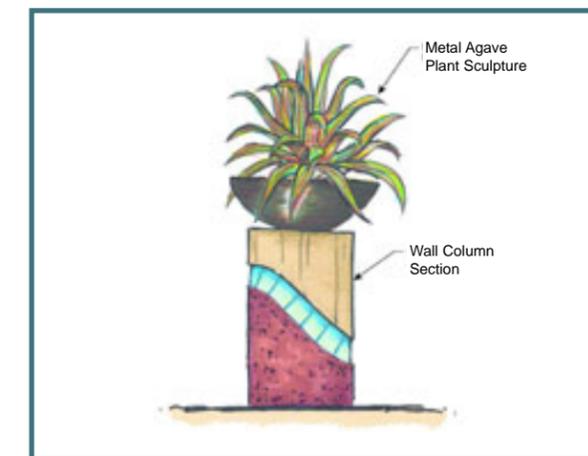
Interpretive Sails



Vertical Wind Spiral



Wind Spiral



Decorative Art Planter

DESIGN CONSIDERATIONS AND OPPORTUNITIES (CONT.)

GATEWAYS AND TRAILHEADS

The gateway opportunities for the LPP are especially important reinforcements to the design theme in areas of the right-of-way. They welcome the public and punctuate the streetscape theme. The gateway and trailhead areas link the pedestrian and vehicular user, and as such they draw heavily on the all the other precepts of the design philosophy. A gateway or trailhead must combine the large visual scale of median requirements with the detail interest and creativity of the artistic requirements. Possible locations for gateways are identified on the Theme Development Map on page 33 of this document.



Sail Gateway



Layered Mountains Gateway



"Spillway" & Pillars Gateway



Small Gateway at Trailhead

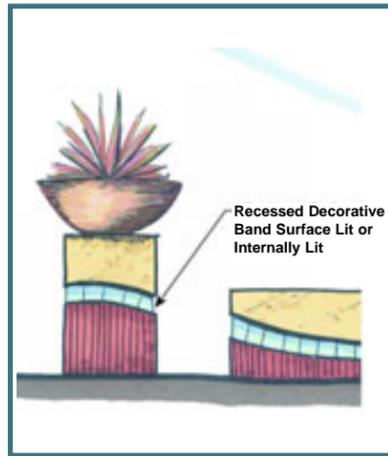
DESIGN CONSIDERATIONS AND OPPORTUNITIES (CONT.)

LIGHTING AND BANNERS

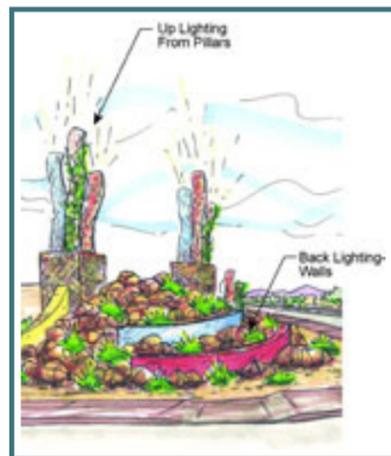
Street lighting: decorative, bollard, and pole lighting are street amenities that enhance the theme design intent by providing nighttime interest to LPP.

DECORATIVE

- ☼ LED's incorporated in walls, changing effects with seasons or events
- ☼ Tree and shrub lighting



Median Detail



Gateway Application

BOLLARD

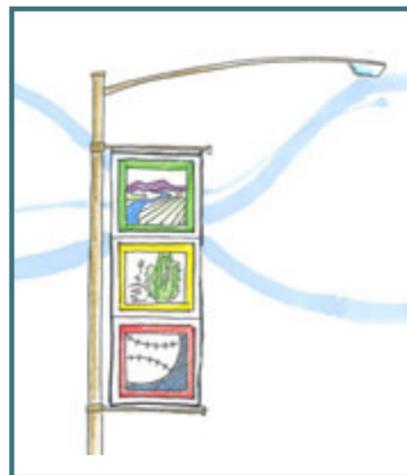
- ☼ Materials used to reinforce wall designs or artistic elements
- ☼ Dual function as protective barriers



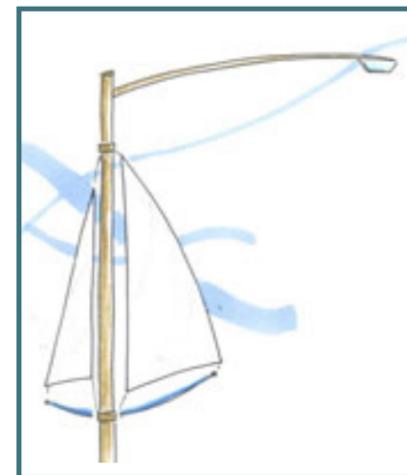
Stone Column Bollards

POLES

- ☼ Paint colors that enhance/reinforce the theme palette
- ☼ Banner opportunities



Panel Banners

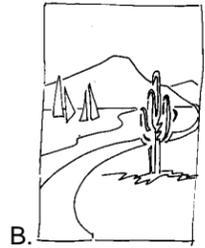


Sail Banners

BANNER IDEAS



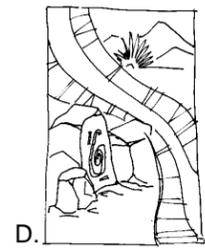
A.



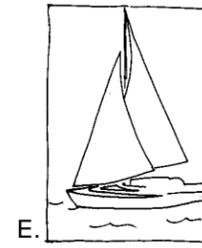
B.



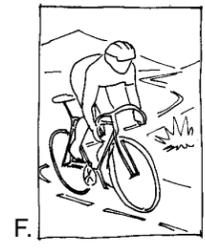
C.



D.



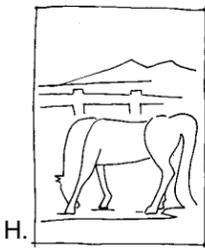
E.



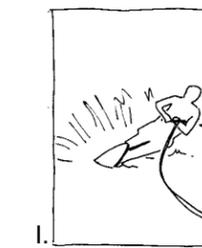
F.



G.



H.



I.



J.

DESIGN CONSIDERATIONS AND OPPORTUNITIES (CONT.)

ICON DEVELOPMENT AND BRANDING

The overall artistic and theme design development for the LPP streetscape is, in part, icon development and branding. The COP wants to advance this a step further in its creative realization by consciously developing a bridge icon that embodies not only the streetscape theme, but also the city image.

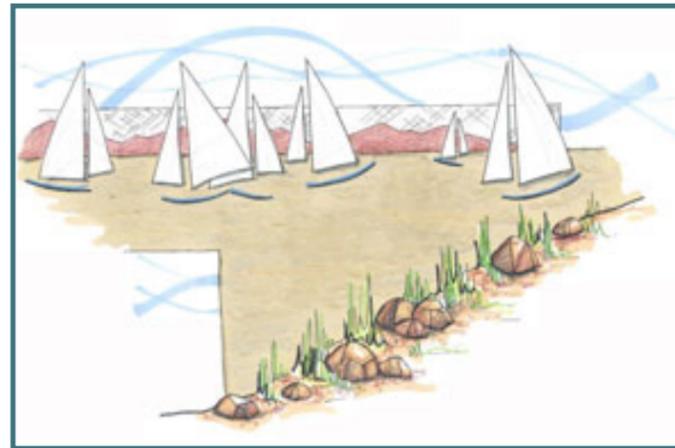
INTEGRATED DESIGNS

- ✿ Sails and mountains
- ✿ Script and Arcs

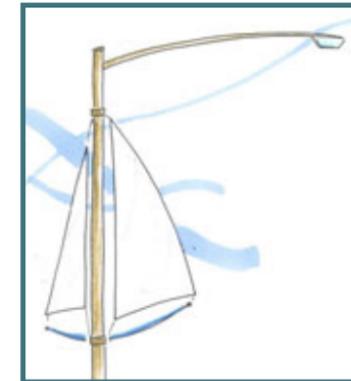
ISOLATED DESIGNS

- ✿ Window frame with images
- ✿ Sail and sunset

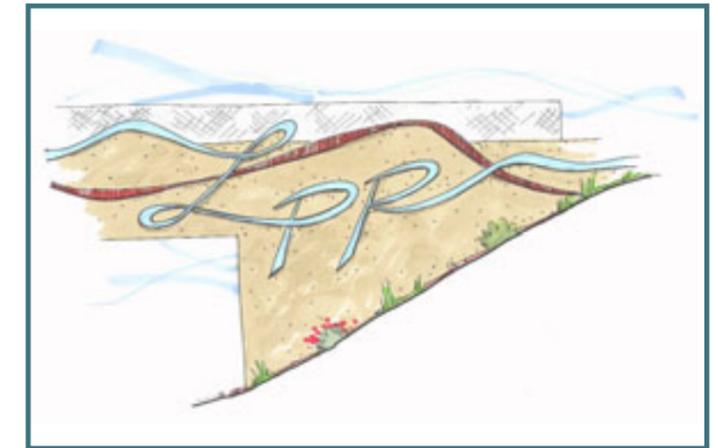
Elements of the bridge icon can then be integrated into other objects typically found in public spaces such as trash receptacles, banners, or benches.



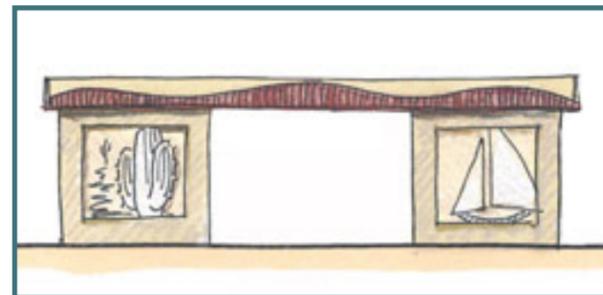
Sails Applied on Bridge



Sail Banner



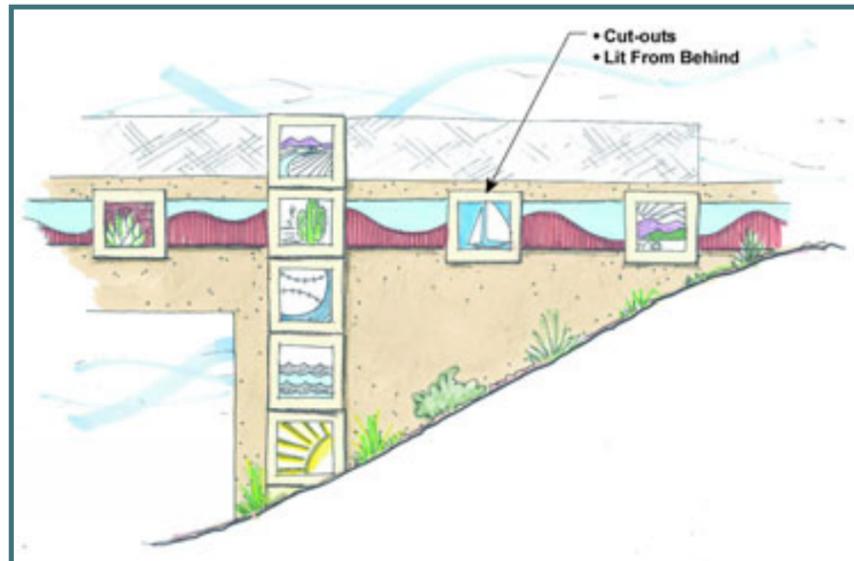
Lake Pleasant Parkway "LPP" Script and Arcs



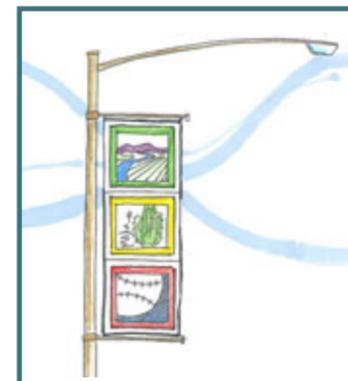
Bench with Window Frame Blocks



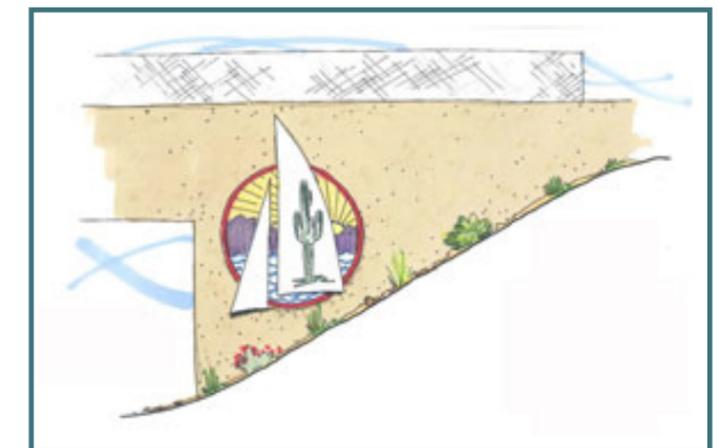
Trash Can With Sail/Sunset Icon



Window Frame Blocks



Window Frame Panel Banner



Sail and Sunset Bridge

PRELIMINARY COST ESTIMATE

The preliminary costs detailed in this document contain four different breakdowns: 1) Urban/Commercial Character Area 100' typical plan median area, 2) Residential/Medium-density Character Area 100' typical plan median area, and 3) Rural/Open Space Character Area 100' typical plan median area.

LAKE PLEASANT PARKWAY
Urban/Commercial Character Area
100' Typical Plan Area (40' Average Median Width)
Preliminary Cost Estimate Analysis (November 2005)

Item Description	Unit	Estimated Quantities	Unit Price	Total Amount	Percent of Total Amount
Landscape					
Groundcover, 1 Gallon	EA	100	\$6.00	\$600	4.62%
Shrubs, 1 Gallon	EA	12	\$6.00	\$72	0.55%
Shrubs, 5 Gallon	EA	15	\$20.00	\$300	2.31%
Shrubs, 15 Gallon	EA	2	\$32.00	\$64	0.49%
Shrubs, 15 Gallon (Accent Cactus/Succulent)	EA	10	\$160.00	\$1,600	12.33%
Trees, 48" Box	EA	2	\$1,200.00	\$2,400	18.49%
Saguaro (8' Spears)	EA	1	\$550.00	\$550	4.24%
Irrigation					
Lump Sum	LS	1	\$2,400.00	\$2,400	18.49%
Hardscape					
Rock Mulch, 1/2" minus (2" thickness)	CY	18	\$75.00	\$1,350	10.40%
Rock Mulch (Cobble), 1" to 3" diameter (4" thickness)	CY	5	\$80.00	\$400	3.08%
Desert Wash Sand, salvaged (4" thickness)	CY	13	\$45.00	\$585	4.51%
Riprap, 6" to 18" diameter	SY	9	\$9.00	\$81	0.62%
Landscape Grading	SF	2,500	\$0.07	\$175	1.35%
Decorative Wall, +30" above Top of Roadway Curb	LF	40	\$60.00	\$2,400	18.49%
Total Landscape			\$5,586	43.05%	
Total Irrigation			\$2,400	18.49%	
Total Hardscape			\$4,991	38.46%	
Total Estimate			\$12,977	100.00%	
With 10% Contingency			\$14,275	110.00%	
Cost per S.F.			\$3.24		
W/ Contingency			\$3.57		
Sculptural Accents					
Architectural Sculpture Elements (undetermined design)	EA	1	\$2,000.00	\$2,000	13.35%
Total Sculptural			\$2,000	13.35%	
Total Estimate w/ Accents			\$14,977		
With 10% Contingency			\$16,475		
Cost per S.F.			\$3.74		
W/ Contingency			\$4.12		

LAKE PLEASANT PARKWAY
Residential/Medium-density Character Area
100' Typical Plan Area (40' Average Median Width)
Preliminary Cost Estimate Analysis (November 2005)

Item Description	Unit	Estimated Quantities	Unit Price	Total Amount	Percent of Total Amount
Landscape					
Groundcover, 1 Gallon	EA	50	\$6.00	\$300	2.88%
Shrubs, 1 Gallon	EA	10	\$6.00	\$60	0.58%
Shrubs, 5 Gallon	EA	18	\$20.00	\$360	3.45%
Shrubs, 15 Gallon	EA	3	\$32.00	\$96	0.92%
Shrubs, 15 Gallon (Accent Cactus/Succulent)	EA	7	\$160.00	\$1,120	10.74%
Trees, 48" Box	EA	1	\$1,200.00	\$1,200	11.50%
Saguaro (8' Spears)	EA	1	\$550.00	\$550	5.27%
Irrigation					
Lump Sum	LS	1	\$2,000.00	\$2,000	19.17%
Hardscape					
Native Salvaged Soil, (2" thickness)	CY	9	\$55.00	\$495	4.75%
Rock Mulch, 1/2" minus (2" thickness)	CY	9	\$75.00	\$675	6.47%
River Rock - native surface stone, 1" to 3" diameter (4" thickness)	CY	5	\$175.00	\$875	8.39%
Rock Mulch (Cobble), 1" to 3" diameter (4" thickness)	CY	4	\$65.00	\$260	2.49%
Desert Wash Sand, salvaged (4" thickness)	CY	7	\$45.00	\$315	3.02%
Native Boulders, 6" to 18" diameter (salvaged)	SY	10	\$9.00	\$90	0.86%
Riprap, 6" to 18" diameter	SY	9	\$9.00	\$81	0.78%
Landscape Grading	SF	2,200	\$0.07	\$154	1.48%
Decorative Wall, +30" above Top of Roadway Curb	LF	30	\$60.00	\$1,800	17.26%
Total Landscape			\$3,686	35.34%	
Total Irrigation			\$2,000	19.17%	
Total Hardscape			\$4,745	45.49%	
Total Estimate			\$10,431	100.00%	
With 10% Contingency			\$11,474		
Cost per S.F.			\$2.61		
W/ Contingency			\$2.87		
Sculptural Accents					
Architectural Sculpture Elements (undetermined design)	EA	1	\$2,000.00	\$2,000	16.09%
Total Sculptural			\$2,000	16.09%	
Total Estimate w/ Accents			\$12,431		
With 10% Contingency			\$13,674		
Cost per S.F.			\$3.11		
W/ Contingency			\$3.42		

PRELIMINARY COST ESTIMATE (CONT.)

LAKE PLEASANT PARKWAY
 Rural/Open Space Character Area
 100' Typical Plan Area (40' Average Median Width)
 Preliminary Cost Estimate Analysis (November 2005)

Item Description	Unit	Estimated Quantities	Unit Price	Total Amount	Percent of Total Amount
Landscape					
Groundcover, 1 Gallon	EA	20	\$6.00	\$120	1.25%
Shrubs, 1 Gallon	EA	15	\$6.00	\$90	0.93%
Shrubs, 5 Gallon	EA	13	\$20.00	\$260	2.70%
Shrubs, 15 Gallon	EA	3	\$32.00	\$96	1.00%
Shrubs, 15 Gallon (Accent Cactus/Succulent)	EA	7	\$160.00	\$1,120	11.63%
Trees, 24" Box	EA	1	\$200.00	\$200	2.08%
Saguaro (8' Spears)	EA	1	\$550.00	\$550	5.71%
Irrigation					
Lump Sum	LS	1	\$1,800.00	\$1,800	18.69%
Hardscape					
Native Salvaged Soil, (2" thickness)	CY	20	\$65.00	\$1,300	13.50%
Native Surface Stone (River Rock), 1" to 3" diameter (4" thickness)	CY	10	\$175.00	\$1,750	18.17%
Desert Wash Sand, salvaged (4" thickness)	CY	6	\$45.00	\$270	2.80%
Native Boulders, 6" to 18" diameter (salvaged)	SY	10	\$15.00	\$150	1.56%
Landscape Grading	SF	1,800	\$0.07	\$126	1.31%
Decorative Wall, +30" above Top of Roadway Curb	LF	30	\$60.00	\$1,800	18.69%

Total Landscape	\$2,436	25.29%
Total Irrigation	\$1,800	18.69%
Total Hardscape	\$5,396	56.02%
Total Estimate	\$9,632	100.00%
With 10% Contingency	\$10,595	
Cost per S.F.	\$2.41	
W/ Contingency	\$2.65	

Sculptural Accents					
Architectural Sculptural Elements (undetermined design)	EA	0.5	\$2,000.00	\$1,000	9.41%

Total Sculptural	\$1,000	9.41%
Total Estimate w/ Accents	\$10,632	
With 10% Contingency	\$11,695	
Cost per S.F.	\$2.66	
w/ Contingency	\$2.92	



MEETINGS: AGENDAS AND NOTES

**LAKE PLEASANT PARKWAY
(UNION HILLS DRIVE TO LAKE PLEASANT)
LANDSCAPE THEMING**

AGENDA

1. Introductions
2. Goal of Meeting
3. Review of Project
4. Images / Discussion
5. Wrap-Up

Lake Pleasant Parkway – Streetscape and Median Theming & Design (COP)

Meeting Notes

Meeting Date: 01-24-05 Meeting Location: City of Peoria

Attendees:

Stacy Caudell	City of Peoria (C.O.P.) Engineering	stacyc@peoriaaz.gov
Louisa Garbo	C.O.P. Planning	louisag@peoriaaz.gov
Kirk Haines	C.O.P. Comm. Services	kirkh@peoriaaz.gov
Jeff Sargent	C.O.P.	jeffas@peoriaaz.gov
Meredith Flinn	C.O.P.	meredith@peoriaaz.gov
Debra Stark	C.O.P.	debras@peoriaaz.gov
Steve Lohide	LSD	slohide@lsdaz.com
Amy Schuchert	LSD	aschuchert@lsdaz.com

General Notes:

1. LSD opened the meeting with description of the tasks and timing for the theming process. With information gathered during this meeting LSD will take 3 to 4 weeks to develop 2-3 theme options to present to the COP. In the next month the preferred option and other comments will be incorporated into a document packet with graphics and descriptions for use in directing the development of future projects along the corridor.
2. The COP stated that 2 options were possibly enough if they were dynamic and changed character depending on the location along the corridor and asked for clarification on the contracted deliverables.
3. LSD responded that the packet would be a guide for development, one that could be used by Community Services and Planning to write specific development standards, it would provide written and graphic guidelines including cross section and typical plans with possible additional sketches to further illustrate the design intent. LSD would also produce a computer fly-thru for a portion of the roadway segment to demonstrate the visual effect.
4. LSD was asked if the production of specific development standards was in their current contract. LSD stated that it was not and that Community Services and Planning would most likely have input they would want to add to the development of those standards so the standards would be consistent with the City's goals.

Design/ Theming Discussion:

1. The design standards were discussed. Concern was expressed that the roadway needed to have consistency throughout the total project. COP is afraid that the current development would detract for the ultimate design.
2. COP stated that the proposed theming options should incorporate the current city standards as much as possible and incorporate principles in the Lake Pleasant Parkway Specific Plan.
3. Medians will be the areas of significant impact, they should stand out and stand alone. The typical desert landscaping used BOC often seems benign, this project needs to stand out.

1

Lake Pleasant Parkway – Streetscape and Median Theming & Design (COP)

4. The option of a strait sidewalk as opposed to the current layout was discussed. Don, Al, Scott, and Bob agreed that the sidewalk would be linear for the length of the project, except as necessary to follow the curb and other site obstructions.
5. The sidewalk across the residential properties fronting Hayden was discussed. Don requested that the sidewalks be pulled back at the driveway aprons to maintain level grade and to pull them off BOC when possible, which would also address ADA accessibility issues for sidewalk users.
6. LSD is to update sidewalks on both the east and west sides of Hayden to reflect the current situation with site walls and landscaping features.

Design Ideas:

1. 12" rise to median curbs with raised planting (1) to screen oncoming headlights (2) deter mid street crossings.
2. Create some sort of physical barrier to further deter crossings.
3. Landscape and design should become more native the further north you go on the project.
4. Dynamic changes are good, a change in seasonal color adds to the changing views throughout the year. Choose plants that flower at different times.
5. Kinetic sculptures, objects that move with wind are other options for dynamic changes with environment.
6. Plantings/ xeriscape should be utilized for water-wise design.
7. Be water wise
8. (F) Municipal Image 1 is more appropriate for urban areas.
9. Palms – may be controversial, could be used if low water variety and growth was appropriate for area.
10. Look for opportunities to include recreation activities of area, water, lake, trails, equestrian activities
11. Banners are good idea.

Conditions/ Parameters/ Opportunities

1. The corridor widths increase as the road travels north.
2. Several water sources in area to draw inspiration from.
3. Existing native peoples (Hohokam) ruins and sites on buttes bordering road, some may become park areas, possibly try to utilize in icon and theming and need to be considered for preservation possibilities.

Actions

1. LSD to summarize the notes and comments from the meeting and survey results.
2. LSD was directed to coordinate the next progress meeting with Jeff Sargent for 3 to 4 weeks from this meeting.
3. LSD directed to develop options for icon and roadway theming.

The above notes were prepared by Amy Schuchert and Steve Lohide and reflect their understanding of the events and discussion at the meeting. If corrections, additions or clarifications are necessary, please contact Amy at aschuchert@lsdaz.com or call (480) 967-1343 by February 11, 2005

2



MEETINGS: AGENDAS AND NOTES (CONT.)



LOGAN SIMPSON
DESIGN INC.

**LAKE PLEASANT PARKWAY
STREETSCAPE AND MEDIAN THEMING**

MEETING #2 NOTES

Meeting #2 Date: March 11, 2005
Meeting Location: City of Peoria City Hall, 3rd Floor Conference Room

Attendees:	Stacy Coudell Kirk Haines Jeff Sargent Meredith Flinn Debra Stark Prisila Ferrara Cathy Carlat Chris Hassert Vaughn Bennett Steve Lohide Amy Schuchert	City of Peoria (COP) Eng. COP Comm. Services COP COP/DCM COP COP/DCM COP Kirkham Michael Kirkham Michael LSD LSD	stacyc@peoriaaz.gov kirkh@peoriaaz.gov jeffas@peoriaaz.gov meredith@peoriaaz.gov debras@peoriaaz.gov prisila@peoriaaz.gov cathyca@peoriaaz.gov chassett@kirkham.com vbennett@kirkham.com slohide@lsdaz.com aschuchert@lsdaz.com
------------	--	--	---

GENERAL NOTES:

- LSD opened the meeting with a brief summary of the results from 01-24-05 meeting and recapped that with information gathered during that meeting LSD used the data to develop 2 theme options for presentation to the COP.
- LSD passed out several handouts; a spreadsheet summary of the 01-24-05 survey, a streetscape design elements list, a planting palette booklet, and a theming concept packet. Two - 1/4 mile plans with representative designs were presented in conjunction with the other information.
- Several of the COP personnel stated that no matter what theme is selected the concept must be bold.
- LSD and COP discussed two alternatives for the spatial organization for the theme components along the 11 miles of roadway; Alternate A, linear progression from south to north or Alternate B, an arrangement based on the COP future growth plan from the City's General Plan.
- Cathy Carlat asked for clarification on what the spatial organization would accomplish. LSD stated that the design options utilized different sub-themes depending on the surrounding uses.

51 West Third Street Suite 450 Tempe, AZ 85281 Phone: (480) 967-1343 Fax: (480) 966-9222 e-mail: ls@lsdaz.com



These sub-themes within the median design would help emphasize the differences of those uses and provide visual interest long the 11 miles of roadway. In urban areas the theme may be more architectural in nature with could be more informal and less densely planted with more native species.

- COP staff expressed a preference for Alternate B for the theme organization to be used for the preferred theme option.
- In the next month the preferred option and other comments will be incorporated into a document packet with graphics and descriptions for use in directing the development of future projects along the corridor. LSD will work with Jeff Sargent to coordinate a meeting date to present the preferred option and determine the meeting format.

DESIGN/ THEMING DISCUSSION:

Concept A – River Curves Theme

- LSD described the theme representing the 3 rivers within Peoria, utilizing the shape of rivers in plan to develop a rhythm for the design. The differences between the curves are emphasized with separate planting palettes representing water, desert, and mountain regions. The 3 rivers also used low decorative walls in broken segments following the curves and reinforce the design in the vertical plane. The theme would be restricted to the median at the southern end with widening curves as it moved north, into more open/ undeveloped areas, also using intersections as opportunities to cross the roadway into the R/W.
- Art elements/Obelisks based on a light bollard from the survey were placed in groupings along the median for further visual interest and as an icon for the roadway, it was suggested they could be lighted at night. The art elements/obelisks are intended to be mixed with saguaro spears as a link between art and nature. Sketches of the median with the obelisks and as a gateway option were included for clarification.
- Median cross sections and example wall elevations showed the possible arrangements of theme elements within the median and options for the materials and finishes for the medians.

Concept B – Waterway Theme

- This theme represents the different types of waterways within COP, utilizing a wash/river concept for the center of the median, with walls to emphasize canals in more urban areas and the rivers/lake in the residential and rural areas. The low decorative walls and formality of the plantings reinforce the design in the vertical plane. The theme would be restricted to the median throughout the length of the road with elements that could easily be used within the R/W for further emphasis.
- Wall option sketches demonstrated surface material and finish treatments. And further sketches illustrated architectural walls as opposed to organic wall forms.
- Inert materials ranging from wash sand to rip rap would be used to emphasize the differences in the waterway types.

Logan Simpson Design Inc. Lake Pleasant Parkway – Streetscape and Median Theming
March 29, 2005 Meeting #2



Gateway Concepts/ Aesthetics

- LSD discussed options and opportunities to develop gateways along the roadway using two sketches to illustrate some ideas.
- Additional art elements with sail/boating and wind capturing ideas.
- LSD and COP discussed the banner options from sketches within the theming packet showing ideas for multi panel banners with nature and recreation themes.
- Bullnose treatments and design options were presented in sketches.

Comments

- Whatever the design the final theme cannot afford to be subtle it must be bold.
- Concept B – Waterway Theme was preferred in a vote of three to one during the meeting.
- Favored wall designs were on pg. 13 and 14, pg. 9. Design B was also a well received for its color combination and the use of several smaller walls to convey a larger pattern.
- Glass block and lighting the walls at night for all around visual interest. Consider maintenance issues when designing the walls and the elements (e.g. rain, mud).
- Consider further how the theme can be expressed in the R/W if developers utilize the guidelines and how the theme will mesh with the developer driven properties.
- Further determine the gateway concepts with ideas about locations and the elements that dictate the gateway guidelines.
- Both the sail and wind spiral art (pg. 23 & 24) were well liked; these options should be explored in further detail.
- On bullnose treatments the LP on pg. 29 was well received, but concern was expressed that it wouldn't read for traffic or be mis read upside down, maybe more pots or decorative art would be better.
- Several of the banner ideas were well liked, both the recreation themes and composite sketches.
- Banners may not be appropriate for this type of roadway, several COP personnel agreed but would like to keep those ideas for other opportunities.
- If banners were used there was concern expressed that they should not be used for advertisements, but more graphic and artistic.
- Pass through trash should be considered when selecting plants, try to raise the shrub and accent canopy to keep them from collecting trash. Also think about the amount of thorny species that collect trash.
- Remember to consider pedestrian friendly plants and materials in those areas affected.
- Engineering requested that any design of walls or other elements meet clear zone requirements. LSD assured everyone that the design would be coordinated to ensure all safety requirements (ASHTO) were met.

Logan Simpson Design Inc. Lake Pleasant Parkway – Streetscape and Median Theming
March 29, 2005 Meeting #2



MEETINGS: AGENDAS AND NOTES (CONT.)

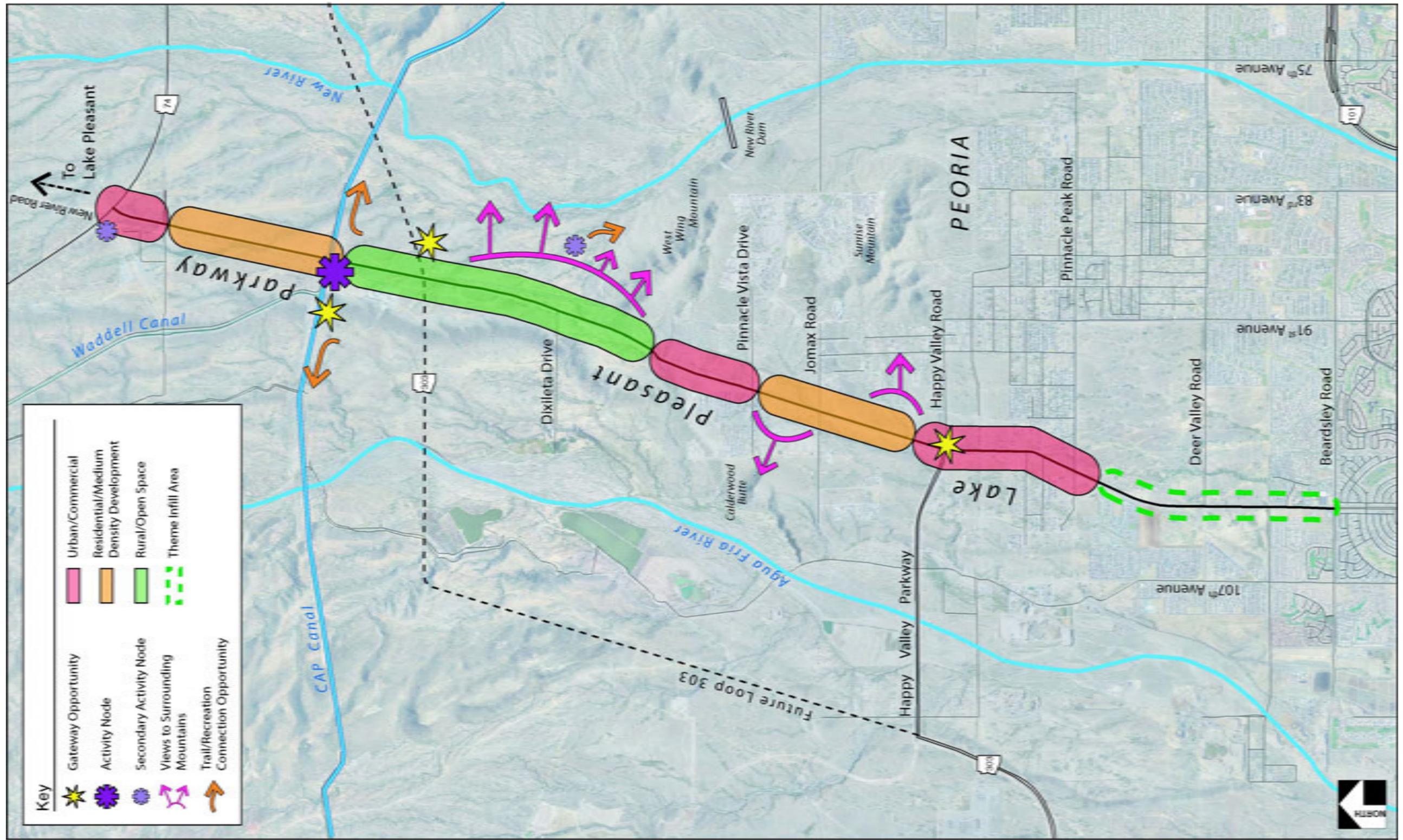


15. Krikham-Michael representatives mentioned that several utilities would be present within the roadway, and coordination would be required to know where architectural elements could go. LSD agreed.
16. Cathy provide LSD with several written comments: (paraphrased below)
 - a. Loves the wind spiral, median sails, and obelisk (using different colors and differently lighted) (pgs. 24,23, & 7)
 - b. Good concept of disconnected walls (pg. 9) and motion in wall (pg. 5)
 - c. Glass block good, lighted from within (pg. 9)
 - d. Use pots (pg. 8)
 - e. Indented (colored and/or glass) (pg. 13)
 - f. Doesn't like to see scrubby plantings, prefers a more structured landscape
 - g. Concerned about walls framing both sides of landscape in medians
 - h. Prefers varying color and texture in bulboses

Actions

1. LSD to summarize the notes and comments from the meeting.
2. LSD was directed to coordinate the next progress meeting with Jeff Sargent.
3. LSD was directed to develop the preferred option for the roadway theming, further develop the gateway design and explore the locations for such a gateway, and integrate the comments from the meeting into the design elements.
4. LSD was directed to compile a project notebook containing all handouts and meeting minutes into a single binder as documentation for the project.

The above notes were prepared by Amy Schuchert and Steve Lohide and reflect their understanding of the events and discussion of the meeting. If corrections, additions or clarifications are necessary, please contact Amy at aschuchert@laskar.com or call or Steve (480) 967-1343 by April 6, 2005.

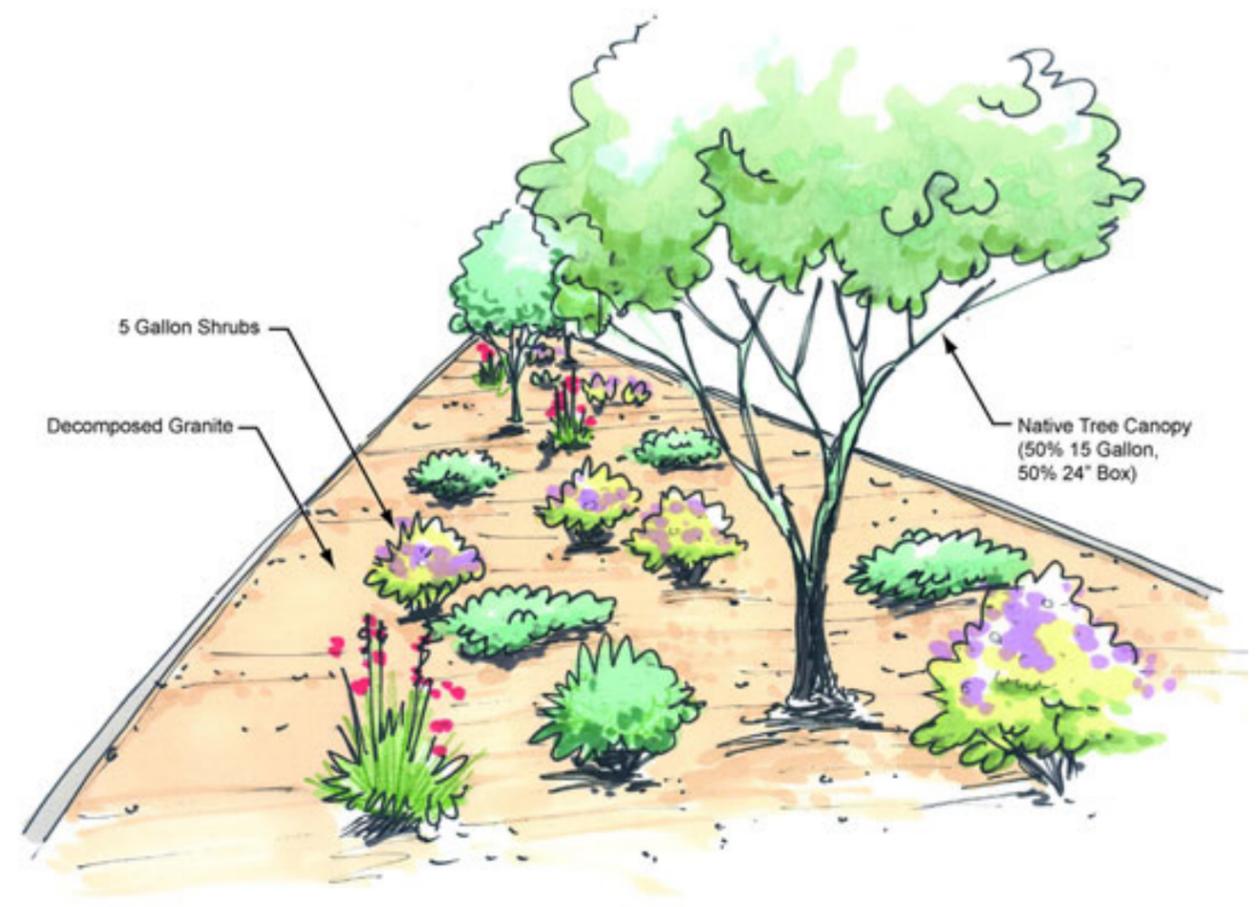


LAKE PLEASANT PARKWAY THEME DEVELOPMENT MAP

LANDSCAPE COST ESTIMATE COMPARISON

LAKE PLEASANT PARKWAY
 Typical Median Design for City of Peoria Residential/Medium-density
 100' Typical Plan Area
 Preliminary Cost Estimate Analysis

Item Description	Estimated Quantities	Unit	Unit Price	Total Amount
Landscape				
Shrubs, 5 Gallon	EA	200	\$30.00	\$6,000
Trees, 15 Gallon	EA	4	\$100.00	\$400
Trees, 24" Box	EA	4	\$300.00	\$1,200
Irrigation				
Lump Sum	LS	1	\$2,000.00	\$2,000
Hardscape				
Rock Mulch, no size (2" thickness)	CY	25	\$60.00	\$1,500
Total Landscape				\$7,600
Total Irrigation				\$2,000
Total Hardscape				\$1,500
Total Estimate				\$11,100
Cost per S.F.				\$2.78



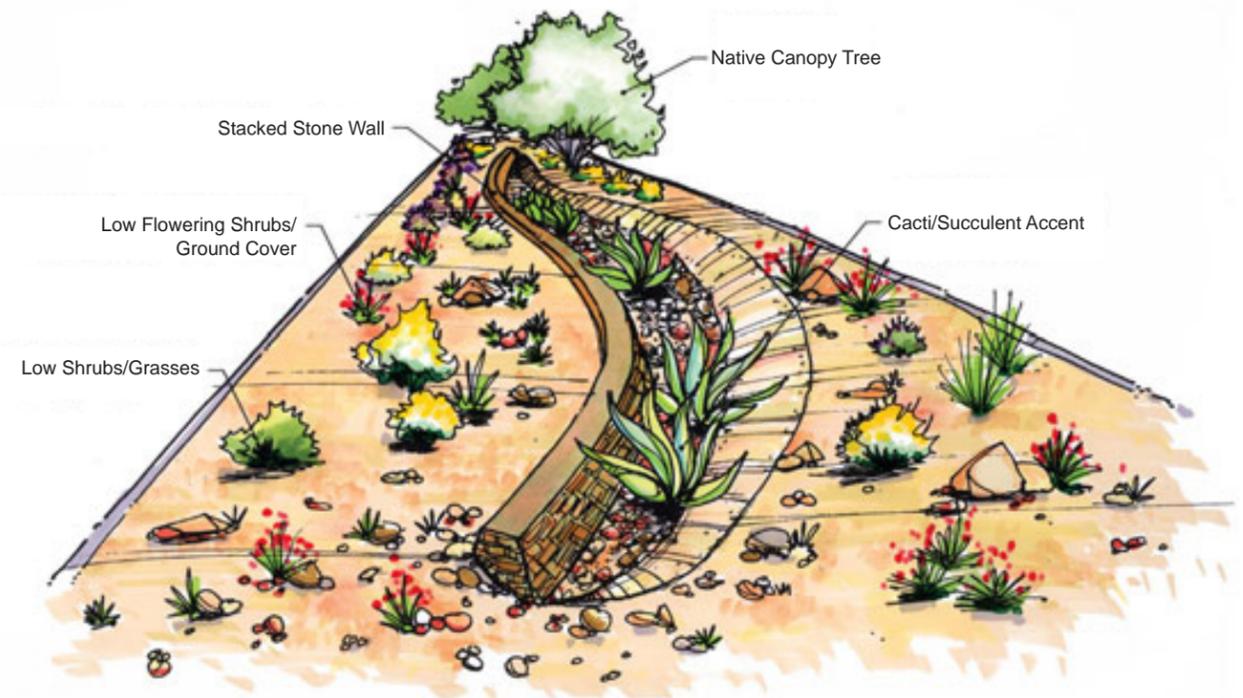
Typical Median Shown with City of Peoria Landscaping Requirements (Article 14-35)

LANDSCAPE COST ESTIMATE COMPARISON

CITY OF SCOTTSDALE
Urban/Commercial Typical Median Planting
100' Typical Plan Area (15' Average Median Width)
Cost Estimate Analysis (2003)

Item Description	Unit	Estimated Quantities	Unit Price	Total Amount
Landscape				
Ground Cover, 1 Gallon	EA	67	\$7.00	\$469
Shrubs, 5 Gallon	EA	27	\$25.00	\$675
Shrubs, 5 Gallon (Accent/Succulent)	EA	9	\$55.00	\$495
Shrub, 15 Gallon (Agave, 15 Gallon)	EA	3	\$40.00	\$120
Trees, 48" Box	EA	2	\$750.00	\$1,500
Irrigation				
Lump Sum	LS	1	\$1,800.00	\$1,800
Hardscape				
Decomposed Granite Mulch (2" Thick, 1/2" Minus)	SY	122	\$2.25	\$275
River-Run Rock (Native Surface Stone, 3"-8")	SY	45	\$6.00	\$270
Specialties (Stone Wall, +18 above Top of Roadway Curb)	LF	25	\$45.00	\$1,125

Total Landscape	\$3,259
Total Irrigation	\$1,800
Total Hardscape	\$1,670
Total Estimate	\$6,729
Cost per S.F.	\$4.49



Typical Urban/Commercial Median Planting with Decorative Low Walls

SURVEY RESULTS SPREADSHEET

Lake Pleasant Parkway – Character Theming Survey

Survey Results										
	Participant's Survey Answers							Totals	Cumulative Total	# of participants
	1	2	3	4	5	6	7			

Highlighted lines rated 2.0 and above on the survey results. These images were most often selected by survey participants as appropriate for Lake Pleasant Parkway under the category that the image is listed.

Municipal Image

Page 1										
A	2	2	2	1	1	1	2	11	1.57	7
B		2	1	1	1	1	2	8	1.33	6
C	2	1	2	2	2	2	1	10	1.67	6
D	1	1	3	1	1	1		8	1.33	6
E	1	3	3	3	2	1		13	2.17	6
F	1	2	1	1	1	1		7	1.17	6
G	1	2	1	1	1	2		8	1.33	6
H	2	2	1	1	1	1		8	1.33	6
I	1	2	2	1	2	1		9	1.50	6
J	2	1	2	1	1	2	1	10	1.67	6
K	1	1	2	1	1	1		7	1.17	6
L	2	2	3	1	2	2		12	2.00	6
Page 2										
A	1	1	2	1	1	1		7	1.17	6
B	1	1	2	1	1	1		7	1.17	6
C	1	3	3	3	2	2		15	2.50	6
D	1	1	2	2	2	1		9	1.50	6
E	1	3	3	1	2	1		11	1.83	6
F	1	3	3	2	2	1		12	2.00	6
G	1	1	1	1	1	1		6	1.00	6
H	1	1	1	1	1	1		6	1.00	6
I	1	1	2	2	1	3	2	12	1.71	7
J	1	3	3	2	3	1		13	2.17	6
K	1	3	3	1	3	1		12	2.00	6
L	1	3	1	1	2	2		10	1.67	6
M	1	1	1	1	1	1		6	1.00	6
N	2	2	2	2	1	1	1	11	1.57	7
O	1	1	3	1	1	1		8	1.33	6
P	1	3	3	1	2	1		11	1.83	6

Visual Experience

Page 1										
A	2	2	1	2	1	3	2	13	1.86	7
B	1	1	2	3	3	1	1	12	1.71	7
C		2	2	2	1	2	2	11	1.83	6

1 = inappropriate 5/5/2005 2 = appropriate 3 = very appropriate 1

Visual Experience, cont.

D	1	2	1	2	1	1	2	10	1.43	7
E	1	2	3	1	1	3	1	12	1.71	7
F		2	2	1	1	1	2	9	1.50	6
G	1	2	1	3	2	1	1	11	1.57	7
H	1	2	2	1	1	1	1	9	1.29	7
I	2	1	1	3	1	2	1	11	1.57	7
J	2	1	1	3	2	3	2	14	2.00	7
K	2	2	2	1	1	3	1	12	1.71	7
L			1	1	1	1	1	5	1.00	5
M			1	1	2	2	2	8	1.60	5
N	3		2	3	3	3	3	17	2.83	6
Page 2										
A	2	3	3	2	3	3		16	2.67	6
B	2	3	3	2	2	3		15	2.50	6
C	1	1	1	1	1	1		6	1.00	6
D	2	2	3	2	2	3		14	2.33	6
E	2	3	3	2	3	2		15	2.50	6
F	1	2	2	3	3	2	3	16	2.29	7
G	1	1	1	1	1	1		6	1.00	6
H	1	2	1	1	1	2	1	9	1.29	7
I	1	1	1	1	1	1		6	1.00	6
J	1	1	1	1	1	2	3	9	1.50	6
K	1	2	1	2	1	3	3	13	1.86	7

Land Forms

A		1	3	3	2	2	2	13	2.17	6
B		1	1	2	1	2	3	10	1.67	6
C	1	1	1	1	1	1	1	7	1.00	7
D		2	3	1	2	2	3	13	2.17	6
E		1	3	3	3	3	3	16	2.67	6
F		1	3	2	2	3	2	13	2.17	6
G		2	3	3	3	3	3	17	2.83	6
H		2	1	3	2	2	1	11	1.83	6
I		2	1	1	1	1	1	7	1.17	6
J			3	3	2	3	3	14	2.80	5
K	2	2	2	3	2	2	3	18	2.29	7
L	3	1	1	2	1	2	2	12	1.71	7
M		1	2	2	3	2	3	13	2.17	6

1 = inappropriate 5/5/2005 2 = appropriate 3 = very appropriate 2

Walls and Fences

A	2	1	2	2	1	2	2	12	1.71	7
B	3	2	3	2	1	3	2	18	2.29	7
C	2	1	3	2	1	1	2	12	1.71	7
D	2	2	3	1	1	1	2	12	1.71	7
E	2	2	3	2	1	2	2	14	2.00	7
F		2	3	2	1	1	2	11	1.83	6
G	1	1	2	1	1	1	1	8	1.14	7
H	1	2	2	2	2	3	1	13	1.86	7
I	2	1	1	2	1	2	1	10	1.43	7
J	2	2	3	2	2	3	3	17	2.43	7
K	2	2	2	3	2	3	3	17	2.43	7
L	2	2	1	2	3	3	1	14	2.00	7
M	1	1	3	2	1	3	1	11	1.83	6
N	1	1	3	2	1	1		9	1.50	6
O	1	2	1	2	1	1	1	9	1.29	7

Bollards and Planters

A	3	1	1	1	1	2	3	12	1.71	7
B	3	2	3	2	1	3	3	17	2.43	7
C	3	1	2	2	1	1	1	11	1.57	7
D	2	1	1	1	1	3	1	10	1.43	7
E	1	2	2	2	1	1	2	11	1.57	7
F	3	2	3	1	2	1	3	15	2.14	7
G	3	2	3	3	3	2	3	19	2.71	7
H	3	2	3	1	2	3	3	17	2.43	7
I	3	1	3	2	1	3	3	16	2.29	7

Street Amenities

A		1	1	1	1	1	1	6	1.00	6
B		1	1	2	1	1	1	7	1.17	6
C		1	3	1	1	2	2	10	1.67	6
D		2	3	1	1	2	1	10	1.67	6
E	2	2	3	2	1	1	1	12	1.71	7
F		2	3	2	1	2	3	13	2.17	6
G		2	3	1	1	1	1	9	1.50	6
H	2	1	2	2	2	2	2	13	1.86	7
I	2	2	3	2	2	3	2	16	2.29	7
J		1	2	2	1	3	1	10	1.67	6
K		2	2	2	1	3	3	13	2.17	6

1 = inappropriate 5/5/2005 2 = appropriate 3 = very appropriate 3



SURVEY RESULTS SPREADSHEET (CONT.)

Street Amenities, cont.												
L			2	3	2	1	1	2		11	1.83	6
M			2	3	2	1	2	1		11	1.83	6
N			1	2	2	1	1	1		8	1.33	6
O			1	3	2	1	2	1		10	1.67	6
P			3	2	3	3	1	3	3	18	2.57	7
Q			2	3	3	1	3			12	2.40	5

Lighting												
A			2	3	1	1	1	2		10	1.67	6
B			2	2	1	1	3	3		12	2.00	6
C			1	2	1	1	1	2		8	1.33	6
D			2	2	2	1	3	2		12	2.00	6
E			2	2	2	1	3	2		12	2.00	6
F			2	3	3	3	3	3		17	2.83	6
G			3	1	3	1	1	2	1	12	1.71	7
H			2	1	3	1	1	1	2	11	1.57	7
I			1	3	1	1	2	2		10	1.67	6
J			1	1	1	1	1	1		6	1.00	6
K			1	1	2	2	2	1		9	1.50	6
L			3	1	2	1	1	2	2	12	1.71	7
M			2	1	1	1	2	2		9	1.50	6
N			1	1	1	1	2	1		7	1.17	6
O			2	1	1	1	2	1		8	1.33	6
P			2	2	1	1	1	1		8	1.33	6
Q			2	1	2	1	1	1		8	1.33	6
R			2	1	2	1	2	1		9	1.50	6
S			1	1	1	1	1	1		6	1.00	6
T			2	1	1	1	1	1		7	1.17	6
U			3	2	3	1	1	3	2	15	2.14	7

Art Components												
A			3	2	2	1	2	3	3	16	2.29	7
B			1	1	3	2	1	1	1	10	1.43	7
C			1	1	3	2	1	2	3	13	1.86	7
D			2	2	2	3	2	3	3	17	2.43	7
E			2	2	2	3	1	3	1	14	2.00	7
F			2	2	2	2	1	3	3	15	2.14	7

1 = inappropriate 5/5/2005 2 = appropriate 3 = very appropriate 4

Art Components, cont.												
G			3	2	3	1	1	1	2	13	1.86	7
H			1	2	3	1	1	3	3	14	2.00	7
I			3	2	2	2	1	3	3	16	2.29	7
J			3	2	2	2	2	3	2	16	2.29	7
K			3	2	2	3	1	3	3	17	2.43	7

Paving Materials, Textures, & Color												
A			2	2	2	1	1	3		11	1.83	6
B			2	2	2	1	1	1		9	1.80	5
C			2	2	2	1	1	2		10	1.67	6
D			2	3	3	2	3	3		16	2.67	6
E			1	2	2	1	1	2		9	1.50	6
F			1	2	2	1	1	1		8	1.33	6
G			1	1	2	1	2	1		8	1.33	6
H			1	1	2	1	3	1		9	1.50	6
I			1	1	2	1	2	2		9	1.50	6
J			1	1	2	1	2	1		8	1.33	6
K			2	2	3	2	2	3		14	2.33	6
L			2	2	2	2	2	1		11	1.83	6
M			1	2	2	1	1	1		8	1.33	6
N			1	2	2	1	1	1		8	1.33	6
O			2	2	2	1	1	3		11	1.83	6
P			2	2	2	1	3	3		13	2.17	6
Q			2	3	2	2	3	3		15	2.50	6
R			2	2	3	3	3	2		15	2.50	6
S			2	3	2	1	1	1		10	1.67	6
T			2	3	2	1	2	1		11	1.83	6
U			2	3	2	1	2	2		12	2.00	6
V			1	3	2	1	2	1		10	1.67	6
W			2	3	3	2	3	2		15	2.50	6
X			2	3	2	1	2	2		12	2.00	6
Y			2	1	2	1	3	3		12	2.00	6
Z			2	1	3	2	1	2		11	1.83	6

Landscape Design												
A			2	2	1	2	1	2	3	13	1.86	7
B			2	2	1	2	2	3	2	14	2.00	7
C			2	2	2	1	1	1		10	1.43	7

1 = inappropriate 5/5/2005 2 = appropriate 3 = very appropriate 5

Landscape Design, cont.												
D			1	2	2	2	2	1	2	12	1.71	7
E			1	1	1	1	1	3	1	9	1.29	7
F			2	1	1	3	3	3	2	15	2.14	7
G			1	2	1	1	1	1	1	8	1.14	7
H			1	1	1	1	1	2	1	8	1.14	7
I			2	1	2	2	1	1	1	10	1.43	7
J			3	1	2	2	1	2	1	12	1.71	7
K			1	1	2	3	3	3	1	14	2.00	7
L			1	1	2	2	1	2	1	10	1.43	7
M			2	3	2	2	3	3		15	2.50	6
N			1	2	1	3	1	3	1	12	1.71	7

Planting Materials												
Page 1												
A			3	2	2	1	2	2		12	2.00	6
B			2	1	2	1	3	3		12	2.00	6
C			2	3	2	2	2	2		13	2.17	6
D			2	3	1	1	1	3		11	1.83	6
E			2	2	2	3	3	1		13	2.17	6
F			2	3	3	3	1	3		15	2.50	6
G			2	3	3	3	2	1		14	2.33	6
H			2	3	2	1	1	3		12	2.00	6
I			3	3	1	2	1	2		12	2.00	6
J			2	3	2	1	2	3		13	2.17	6
K			2	2	1	1	2	1		9	1.50	6
L			2	2	2	1	2	2		11	1.83	6
M			2	2	3	2	2	1		12	2.00	6
N			2	1	3	3	2	1		12	2.00	6
O			1	2	3	3	3	2		13	2.60	5
P			2	2	1	1	2	3		11	1.83	6
Q			2	2	3	2	3	1		13	2.17	6
R			2	1	3	3	2	1		12	2.00	6
Page 2												
A			2	3	3	2	3	3		16	2.67	6
B			2	2	2	1	3	3		13	2.17	6
C			3	1	2	2	2	1		11	1.83	6
D			2	2	2	1	1	2		10	1.67	6
E			3	3	1	1	1	3		12	2.00	6
F			2	3	2	1	2	2		12	2.00	6
G			2	2	3	3	2	2		14	2.33	6

1 = inappropriate 5/5/2005 2 = appropriate 3 = very appropriate 6

SURVEY RESULTS SPREADSHEET (CONT.)

Planting Materials, cont.										
H	2	2	2	2	3	2	13	2.17	6	
I	2	2	2	1	2	1	10	1.67	6	
J	1	1	1	1	1	1	6	1.00	6	
K	2	2	1	1	1	1	8	1.33	6	
L	2	3	3	3	2	1	14	2.33	6	
M	3	3	3	2	2	3	16	2.67	6	
N	1	1	3	3	3	2	13	2.17	6	
O	2	2	2	1	3	1	11	1.83	6	

1 = inappropriate
5/5/2005

2 = appropriate

3 = very appropriate
7



ACKNOWLEDGEMENTS

Owing to the significance of the project to the COP several members of the staff, city council, and consultants had an active role in development of this Design Theme Manual. The contributions from all participant stakeholders toward were appreciated and important to the successful development of a design theme for LPP.

PROJECT STAKEHOLDER PARTICIPANTS

Project Manager

Jeff Sargent, ASLA, RLA
*City of Peoria Park Projects Administrator
 Community Services Department*

CITY COUNCIL

Mayor	John Keegan
Acacia District	Vicki Hunt
Ironwood District	Bob Barrett
Mesquite District	Cathy Carlat
Palo Verde District	Pat Dennis
Pine District	Carlo Leone
Willow District	Joan Evans

CITY OF PEORIA STAFF

J.P. de la Montaigne
Community Services Director

Kirk Haines
Parks & ROW Manager

Dave Moody, PE
City Engineer

Stacy Caudell, PE
Engineering Department

Debra Stark
Community Development Director

Chad Daines
Planning and Zoning Manager

Meredith Flinn
Deputy City Manager for Development and Community Services

Prisila Ferrara
Deputy City Manager for Safety and Administrative Services

CONSULTANTS

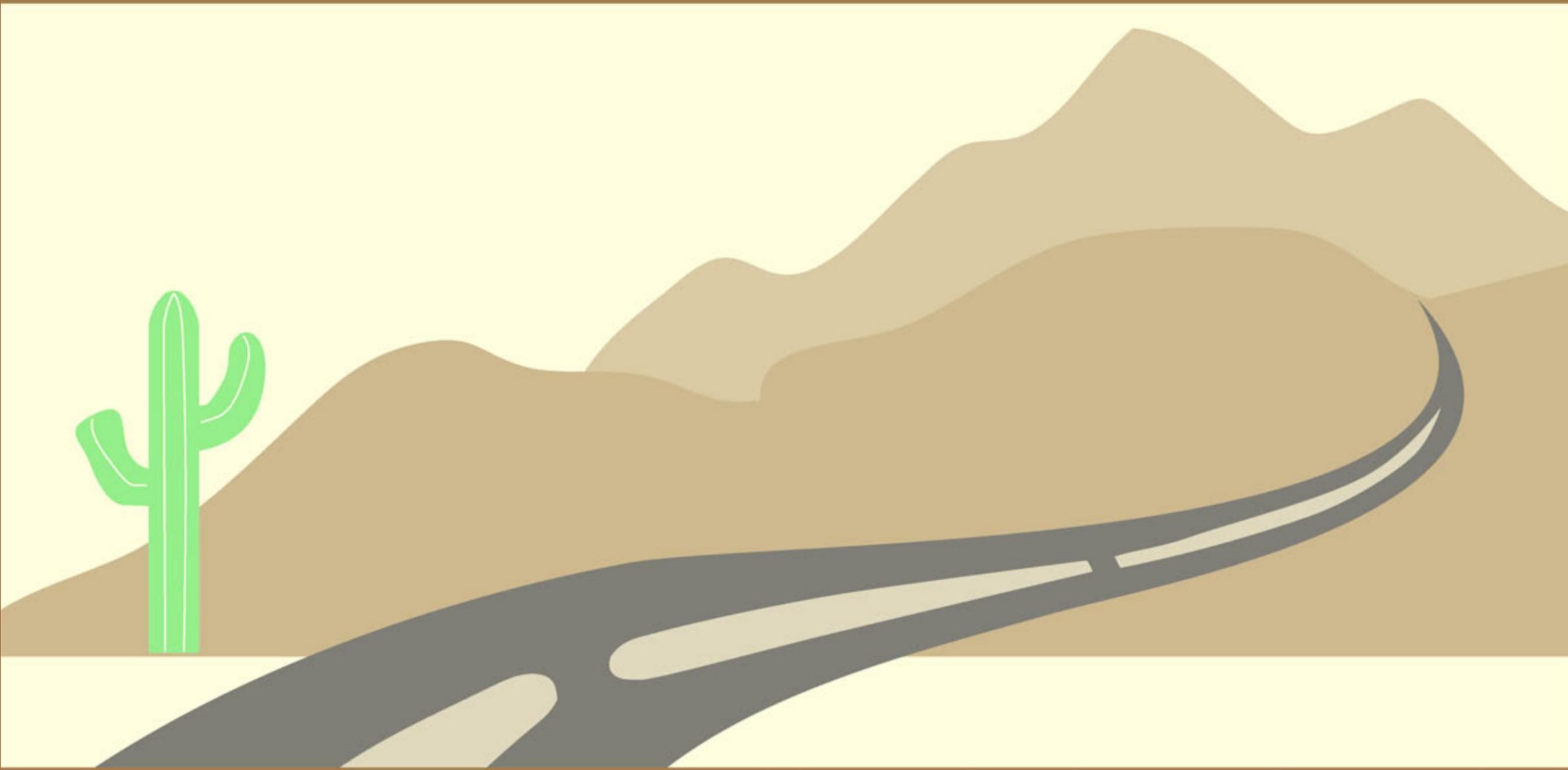
LANDSCAPE ARCHITECTS

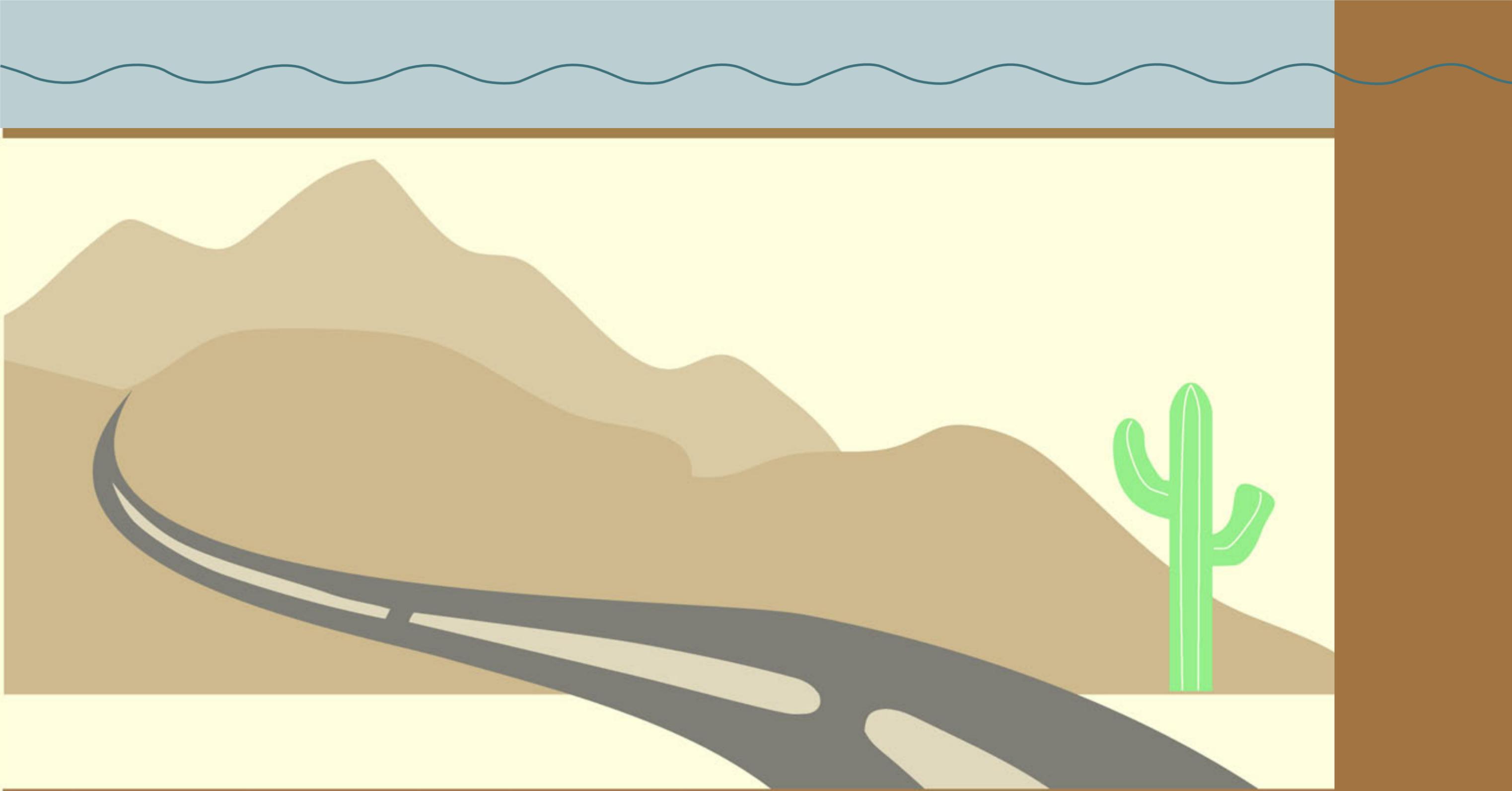
Amy Schuchert
Logan Simpson Design, Inc.

Steve Lohide
Logan Simpson Design, Inc.

ENGINEERS

Steve Martin
Kirkham Michael, Inc.





51 West Third Street • Suite 450 • Tempe, Arizona 85281 • Phone (480) 967-1343 • Fax (480) 966-9232 • www.logansimpsondesign.com