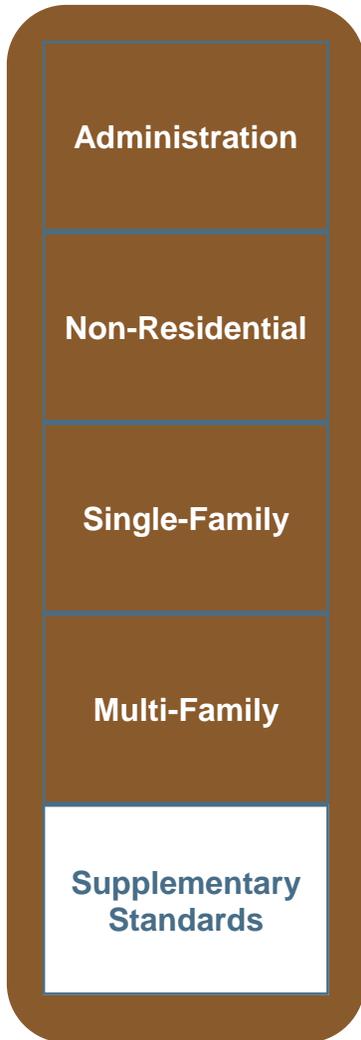


# Chapter 5: Supplementary Standards



## I. INTENT (20-78-5.1)

The Design Review Manual provides overarching principles and guidelines for all development types located within the City. However, there are some areas in the City that warrant special attention due the presence of unique built and environmental characteristics, heritage, uses or visibility. The Lake Pleasant Parkway Corridor enjoys spectacular vistas and serene desert lands. This Chapter attempts to recognize these environmental qualities and initiate the corridor as a signature parkway with theming playing on the presence of Lake Pleasant. A different character area is the historic heart of the City - Old Town. The interests in this area are different. The Manual promotes the appropriate integration of new development in context with its historic character. Therefore, to these ends, Chapter 5 is intended to provide supplemental standards to address the unique needs in these areas. The relevant standards in this Chapter then will be used in conjunction with the applicable standards contained in Chapters 2-4 of this manual.

Finally, this Chapter contains overlay standards for all development types in regards to the location and screening of utility boxes, pedestals, and related appurtenances. The concentration of such facilities at high profile locations can detract from the aesthetic quality of the community. The standards and details are located in one Chapter for ease of use.

This Chapter is therefore organized as follows:

1. Old Town
2. Lake Pleasant Parkway Corridor
3. Utility Cabinets



## II. Old Town

## II. Old Town



## II. OLD TOWN

### A. INTRODUCTION

The Old Town area is of special interest to the City as it represents the City's history and charm. Development in Old Town must be consistent with the existing flavor. The City's vision for this area is a commercially-based core with supporting residential uses. Together, these uses will create a walkable community and comfortable streetscape taking advantage of the existing narrow streets, mature trees and dense development pattern. Though restoration of existing structures is preferred, new development is certainly welcome provided it works in concert with the City's vision for Old Town.

The dominant character of Old Town should be that of a retail-oriented, commercial environment with an active street edge that is inviting to pedestrians, while also accommodating to automobiles. Development should include a mix of building types including older structures and compatible, newer ones. Each building should reflect the style and character of its own time, but should also contribute to a sense of visual continuity for the area.

### B. APPLICABILITY

These guidelines shall apply to the design and construction of new commercial/retail, office, multi-family residential, or mixed-use development in all zoning districts in the Old Town area. These guidelines shall also affect exterior alterations, additions to or the rehabilitation of any existing buildings in Old Town. The guidelines are broken down by concept, applying to *all* land uses in the Old Town area unless otherwise stated. Map 5.1 depicts the affected area of Old Town.

# Old Town Special Area





Figure 5.1—Adjacent buildings share features such as brick facades and arched windows.



Figure 5.2—Primary building entrances are oriented directly toward the street and sidewalk.



Figure 5.3—Building entrances in proportion with the overall façade.

## C. SITE DESIGN (20-78-5.II)

### 1. SITE LAYOUT AND ORIENTATION (20-78-5.II.C.1)

- a. Avoid designs that significantly interrupt the predominant scale of the street.
- b. Development shall be compatible with area buildings in scale, massing, architectural features, solid-to-void relationships, and materials. (Figure 5.1)
- c. Primary building entrances shall be oriented directly toward the street and sidewalk. (Figure 5.2)
- d. Building entrances should appear historically accurate.
- e. Building entrances shall be in scale with the overall façade. (Figure 5.3)
- f. Entrances to residential units shall be oriented toward open spaces whenever possible. First floor entries oriented toward alleys, parking lots, or streets with heavy traffic should be avoided.
- g. Buildings should be oriented in a manner that promotes strong physical connections between public spaces and neighboring development.
- h. New buildings shall be oriented parallel to their lot lines in order to respect the Old Town street grid.
- i. The area between a building and its front setback/build-to line should be used for outdoor dining, entryways, sidewalk sales, and other

pedestrian friendly activities.  
(Figure 5.4 and 5.5)

## 2. ACCESS AND CIRCULATION (20-78-5.II.C.2)

### a. General

1. Streets shall be designed for safe and comfortable pedestrian activity, incorporating amenities such as trees, furniture, and lights.
2. The use of special paving and landscape treatments shall provide distinctive accents to areas with a unique identity, such as important intersections and notable historical and cultural landmarks.

### b. Sidewalks

1. Sidewalks and pathways shall be designed to promote physical continuity and connection.
2. Ample width and design for universal access shall be provided along all pathways and sidewalks.
3. Clear pedestrian passage shall be maintained along public sidewalks without conflicts with utility equipment, street trees, street amenities, or other potential interferences. (Figure 5.6)

### c. Transit Stops

1. Transit stops should be designed as iconic and functional amenities. (Figure 5.7)
2. Transit stops shall be provided with seats and shelter to increase safety and comfort.



Figure 5.4—Outdoor dining area encourages activity and projects “life” onto the street.



Figure 5.5—An outdoor dining space is created between a building and the sidewalk.



Figure 5.6—A sidewalk provides accessibility and opens to a well-lit seating area with trees



Figure 5.7—Artful transit stop provides function and thematic interest.



Figure 5.8—Street furniture creates an inviting respite for pedestrians.



Figure 5.9—Awnings and canopies on old flat facades provide pedestrian coverage and interest.

### 3. PARKING (20-78-5.II.C.3)

- a. Parking lots/structures shall be located behind buildings or interior to a block to support pedestrian-oriented streets.
- b. Parking lots shall be screened. Such screening shall be consistent with the surrounding area.

### 4. PEDESTRIAN AMENITIES/PUBLIC PLACES (20-78-5.II.C.4)

- a. Adequate bicycle parking facilities shall be provided at appropriate locations commensurate with the impact of the use.
- b. Street furniture, including benches, refuse receptacles, and planters/seat walls should be placed at frequent intervals along sidewalks. (Figures 5.8 and 5.25)
- c. Street furnishings, such as drinking fountains, public restrooms, bicycle racks, and kiosks should be integrated, where appropriate, into the Old Town development template.
- d. Awnings over entrances, decorative features on walls, landscaped planters, trees, and other similar amenities shall be used, wherever possible, to enhance pedestrian areas.
- e. Buildings adjacent to the sidewalk should provide overhead cover in the form of canopies, awnings, and overhangs where there is an insufficient or immature street tree canopy. (Figures 5.9 and 5.26)
- f. Gathering areas with fountains, artwork, seating, and enhanced landscaping should be provided in centralized locations. (Figure 5.10)

## 5. OTHER THEMATIC ELEMENTS (20-78-5.II.C.5)

### a. Public Art

1. Public art should be considered in areas with a unique identity or to celebrate the entrance to a public place (e.g. gateways, public places, buildings, etc.)
  2. Public art that celebrates local history and culture shall be encouraged. (Figure 5.11)
  3. Public art shall be made accessible, drawing people through public spaces, but not impeding pedestrian flows.
- b. Covered mailboxes that look like an add-on and not integrated into the building's architecture are prohibited.

## 6. LANDSCAPING (20-78-5.II.C.6)

- a. Landscaping shall be used to define and delineate public streets.
- b. Landscaping shall not be used as a substitute for building enhancements.
- c. Landscaping should accentuate architecture utilizing trees with ornamental tree grates, shrubs, flowers, and ground cover.
- d. Tree canopies should allow sufficient room for growth without impacting building elements or impeding pedestrian and other modes of circulation.
- e. Street trees should define the street and sidewalk, emphasizing the consistent use of tree



Figure 5.10—A water fountain serves as a centralized gathering place.



Figure 5.11—Public art celebrating local history is surrounded by seat walls.



Figure 5.12—Street trees are used to accentuate pedestrian crossings and provide shade.



Figure 5.13—Pedestrian-scale street lights coordinate with a clock feature and bollards.



Figure 5.14—Decorative kiosk directing pedestrians to landmarks and attractions.



Figure 5.15—Thematic signage announcing entry into a special district.

species, size and spacing along a street to create a pleasant rhythm, and reinforcing the space of the street. (Figure 5.12)

- f. Tree grates should be used in areas with considerable commercial and pedestrian activity to increase sidewalk width, reduce safety hazards, and prevent compaction of the root-ball.

## 7. LIGHTING (20-78-5.II.C.7)

- a. Building mounted light fixtures shall be compatible with the building architecture.
- b. Street light fixture design shall be coordinated with other street amenities to unify areas with a distinct identity. (Figure 5.13)
- c. Low-level lighting of buildings and landscaped areas is encouraged.

## 8. REFUSE ENCLOSURES (20-78-5.II.C.8)

- a. Refuse enclosures shall be compatible with the overall architectural design of a project.

## 9. SIGNAGE (20-78-5.II.C.9)

- a. Signs and graphics should be used to aid pedestrian and vehicular orientation and way finding, and to identify special districts, historical and cultural landmarks, and local attractions. (Figure 5.14)
- b. Signage shall coordinate with the architectural character.
- c. Thematic signage should be used in key locations to define areas of interest. (Figure 5.15)

## D. ARCHITECTURAL FORM (20-78-5.II.D)

### 1. BUILDING MASS (20-78-5.II.D.1)

- a. New construction shall appear similar in mass and scale to historic structures in the area.
- b. All structures should have exposed foundation walls and bases, whenever possible.
- c. Multi-Family Residential
  1. Additions to existing residences shall be positioned in a manner so as to not dominate the existing building. (Figure 5.16)

### 2. BUILDING FORM (20-78-5.II.D.2)

- a. Secondary or non-entrance building façades exposed to public view should complement the primary façade.
- b. The use of projecting private balconies, building wall recesses, building pop-outs, varied wall planes, arched dormers, mansard roofs, decorative window treatments, brick or stone veneers, color changes, and other architectural elements is encouraged. (Figure 5.17)
- c. Storefronts should be continuous along the streetscape to encourage pedestrian movement. (Figure 5.18)
- d. Windows and doors should make up the majority of ground floor façades, creating inviting pedestrian scale streetscapes. Windows and balconies should be used on upper floors to create depth and articulation. (Figure 5.19)



Figure 5.16—Newer residential unit is stepped back without dominating the existing streetscape.

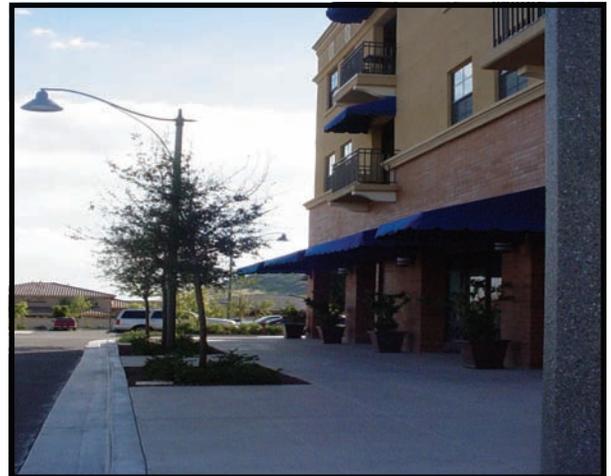


Figure 5.17—Building integrates projecting balconies, brick veneer, and awnings.



Figure 5.18—Continuous storefronts encourage pedestrian movement.



Figure 5.19—Balconies define pedestrian scale and add building articulation.



Figure 5.20—Through adaptive reuse, old-schoolhouse is utilized as a historical museum.



Figure 5.21—Buildings of various ages maintain a sense of continuity.

- e. Awnings/canopies shall appear integrated into the design of the building they are a part of in scale, form, material, and details.

### 3. ARCHITECTURAL CHARACTER (20-78-5.II.D.3)

#### a. Existing Buildings

1. The distinguishing qualities and features of a historic or architecturally significant building, structure, or site shall be maintained.
2. The recycling of historically and architecturally significant Old Town buildings shall be encouraged through adaptive reuse. (Figure 5.20)

#### b. New Buildings

1. The replication and contemporary interpretation of older historic styles for new buildings shall be encouraged. Contemporary interpretations of traditional buildings, which are similar in scale and overall character to those seen historically in Old Town and the region shall be encouraged. (Figure 5.21)

## E. COLORS/MATERIALS (20-78-5.II.E)

### 1. OVERALL DESIGN STANDARDS (20-78-5.II.E.1)

- a. Materials shall appear similar to those used historically in the area (brick, concrete block, etc).
- b. Façade elements shall be aligned along the block, including window sills, moldings, and building cornices. (Figure 5.22)

## 2. RAIN GUTTERS (20-78-5.II.E.2)

- a. Rain gutters shall follow the architectural lines of the building or be encased within the building walls. (Figure 5.23)
- b. Pedestrians shall be protected from water runoff from roofs and balconies through the use of rain gutters, or the proper location of walkways.

## 3. WALL MATERIALS & COLORS (20-78-5.II.E.3)

- a. Upon renovation of historic structures, original materials shall be repaired and retained whenever possible.

## 4. WINDOWS & DOORS (20-78-5.II.E.4)

- a. Windows shall be trimmed with painted wood, anodized metal, shutters, wrought-iron, or a material similar to that historically used in the area.
- b. Window dimensions shall be similar to those traditionally used (dividing frame elements such as mullions, etc.). (Figure 5.24)
- c. Windows shall have multiple panes, divided lights, exposed lintels, trim casing, and/or recessed frames.
- d. Large areas of glass shall be recessed or placed in shadow of projected overhangs.
- e. Windows and doors on ground level storefronts shall utilize transparent glass to insure visibility and minimize glare.



Figure 5.22—Façade elements are substantially aligned along the block.



Figure 5.23—Rain gutters follow the architectural lines of the building.



Figure 5.24—Window proportions are consistent.



Figure 5.25—Thematically consistent seatwall and signage are integral pedestrian ingredients.



Figure 5.26—Building awnings provide proportion and scale and serve as an amenity for pedestrians.



Figure 5.27—Distinguished walkway promotes safer crossing conditions for pedestrians.





### III. Lake Pleasant Parkway

# III. Lake Pleasant Parkway



## III. LAKE PLEASANT PARKWAY

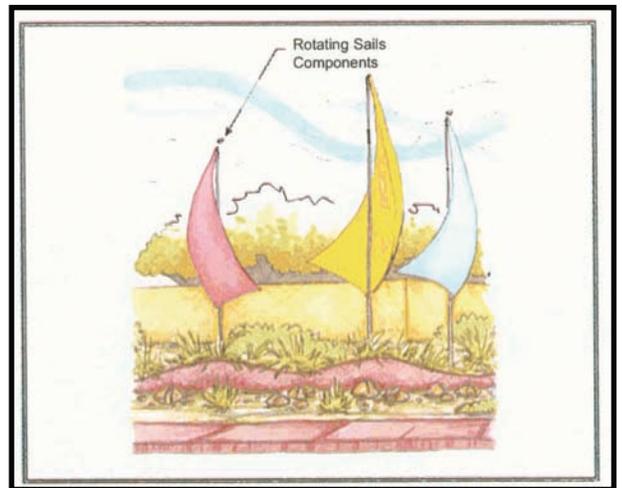
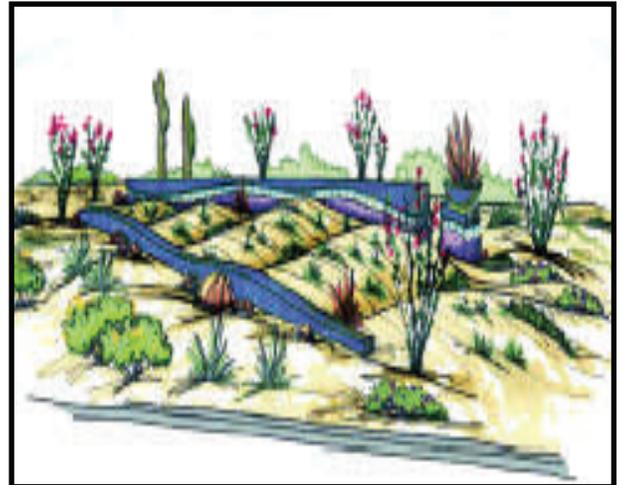
### A. INTRODUCTION (20-78-5.III.A)

During recent plans to widen the Lake Pleasant Parkway, the City of Peoria determined that it had an opportunity to define the parkway as a signature street. The streetscape theme preparation was an opportunity to accentuate the natural beauty of the region and highlight the city's significance in the Valley. The purpose of this chapter is to document the design expectations for the development of landscape aesthetic treatments for the Lake Pleasant Parkway in a clear format. This chapter advances and encases the concepts accepted by the City Council in the 2005 *Lake Pleasant Parkway Design Theme Manual*.

Implementation of this collection of standards and guidelines – collectively, a design theme - will enable the Lake Pleasant Parkway median to visually cohere throughout its entire length. The median treatment alone will elevate the aesthetic experience for this signature roadway. This chapter promotes the extension and integration of this design concept to the edges as private development occurs. Clearly, as each development may have its own character and thematic profile, the “expression” of this design concept may fluctuate. The goal is to create the iconography that will function throughout the City of Peoria as a timeless identifier. The principles and guidelines contained in this chapter are intended to meet the following goals and objectives:

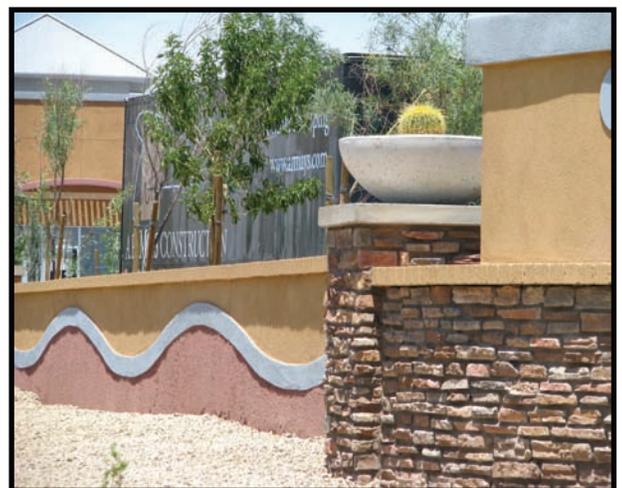
1. Create a signature transportation corridor for the City with a strong identity implemented through creative design;

2. Create a set of written guidelines clearly outlining the Lake Pleasant Parkway aesthetic criteria and physical components;
3. Discourage unwanted mid-street pedestrian crossings through design and plant selection;
4. Introduce opportunities and design parameters for the display and integration of public art into the streetscape and through functional amenities in public areas (e.g., bus stops, utility boxes screening, lighting fixtures, sidewalks and crosswalks, trash receptacles);
5. Create physical and visual link between the City and Lake Pleasant;
6. Preserve, protect and enhance important visual resources throughout the corridor;
7. Define a visual theme through medians designed with distinctive landforms, plantings and architectural features; and
8. Create an icon capable of functioning successfully in multiple applications, indicative of both the Parkway and the broader Peoria image.



## B. APPLICABILITY (20-78-5.III.B)

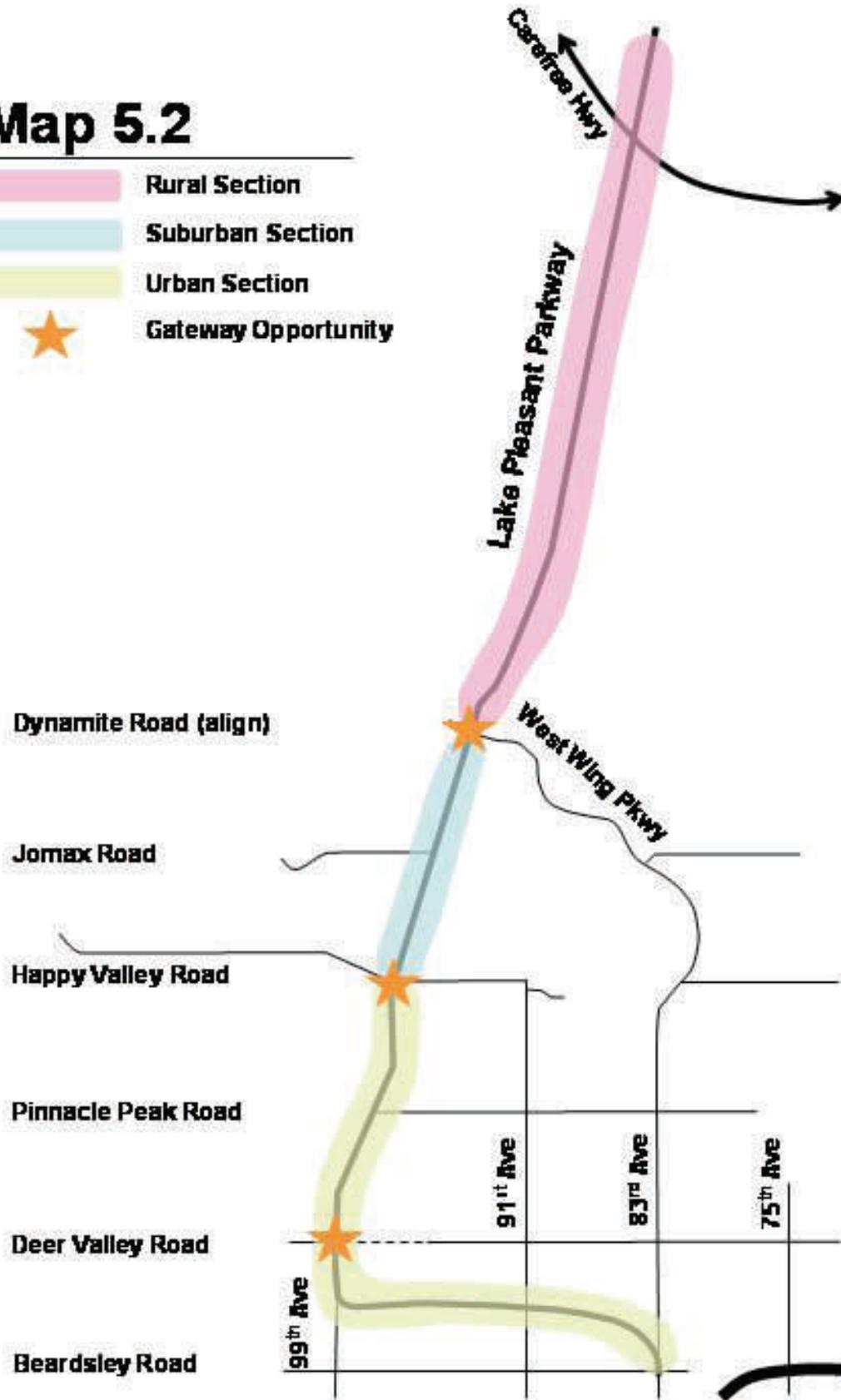
The principles and guidelines in this chapter will apply to the median and edge treatments for the Lake Pleasant Parkway corridor as depicted on Map 5.2. The design theme will be implemented through Capital Improvement Projects (CIP) and new development applications.





# Map 5.2

-  Rural Section
-  Suburban Section
-  Urban Section
-  Gateway Opportunity





## C. EXISTING CONTEXT (20-78-5.III.C)

The Parkway currently touches portions of Fletcher Heights, Camino A Lago, Lake Pleasant Pavilions, Lake Pleasant Town Center, Pleasant Valley Ranch and Cibola Vista Resort and Spa among other existing and proposed residential and commercial developments. The Lake Pleasant Parkway corridor passes through various types of terrain. It begins in a highly urbanized area in the southern portion of the corridor and continues north through undeveloped creosote flats and hillside areas along the northern half of the Parkway. As the Parkway heads north, additional view corridors have been maintained on both sides of the right-of-way to ensure the preservation of the mountain views looking north of Lake Pleasant.



## D. DESIGN STANDARDS (20-78-5.III.D)

Design standards are parameters set forth to ensure that the design philosophy is carried out in a satisfactory manner. The standards established below shall supersede other City Code and Design Review requirements for median design and development within Lake Pleasant Parkway corridor.



This section of the Design Review Manual is intended to guide the landscape, wall, and artwork design located in all medians along Lake Pleasant Parkway to ensure a cohesive design theme along the entire corridor. In addition to guiding median design, this section is also used to influence the design of landscaping, public art, and walls for all private development adjacent to the Parkway.

### 1. WALLS (20-78-5.III.D.1)

- a. Walls shall be used in medians to provide a vertical surface to accentuate thematic concepts and artistic elements.



Figure 5.28—"Canal" style median walls

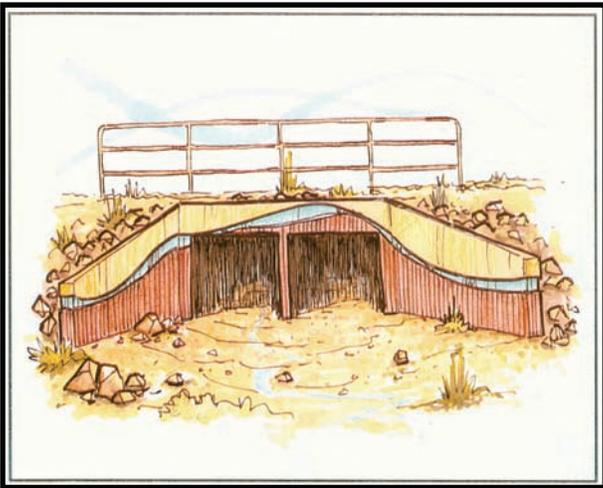


Figure 5.29—Headwall perspective

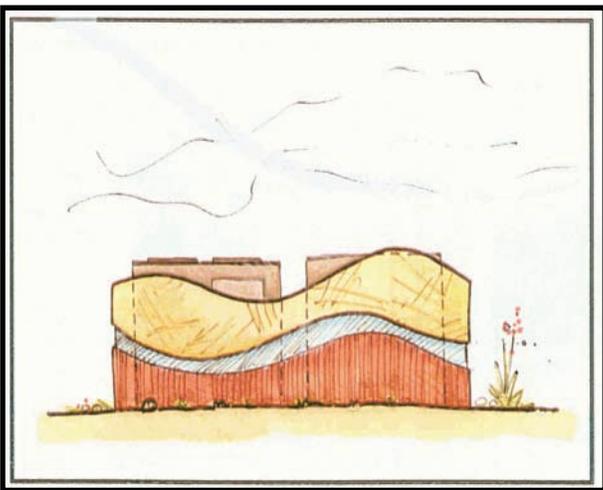


Figure 5.30—Utility box screen wall

- b. Walls shall exhibit warm earth tones, gold, rust, red, and yellows, accented with greens and blues.
- c. Walls shall be accented with materials such as glass block, stone, tile or other suitable materials to add visual interest and texture changes.
- d. Median walls shall be twelve (12) to thirty (30) inches in maximum visible height, but in no case shall the wall exceed thirty-six (36) inches in height above nearest curb.
- e. Median wall sections shall be between twenty-five (25) and forty (40) feet in length and shall be curvilinear in design. Walls or wall patterns shall undulate vertically to represent 'wave' theme. Physical separation of wall segments is strongly encouraged.
- f. Provide nighttime interest with the use of low level lighting.
- g. Drainage outflow aprons or headwalls should be made from materials used in the median "wash". At a minimum, these items shall be dyed/stained or painted to reflect the predominant color found in the area, however the preferred design will incorporate stone facades and enhanced railing.

## 2. BERMS (20-78-5.III.D.2)

- a. Berming shall occur along all walls within the medians and edge treatment.
- b. Berms shall be formed in a manner that will support plant growth and reduce the likelihood of scouring and/or the unintentional movement of landscape materials. Ideally, berming will enhance the 'wave' theme by creating the illusion of the wall diving in and out of the terrain.

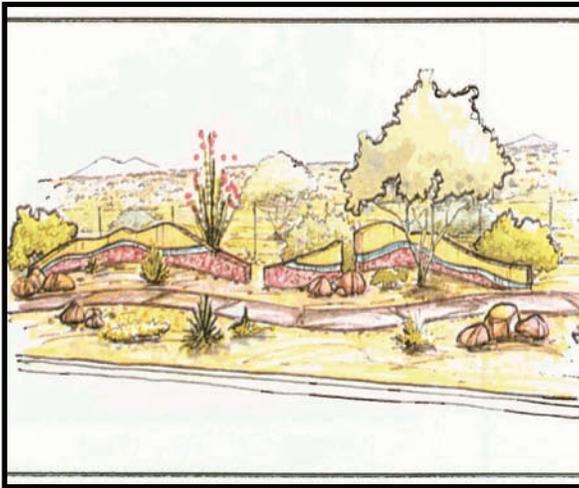


Figure 5.31—Low decorative walls at New River Dam Embankments.

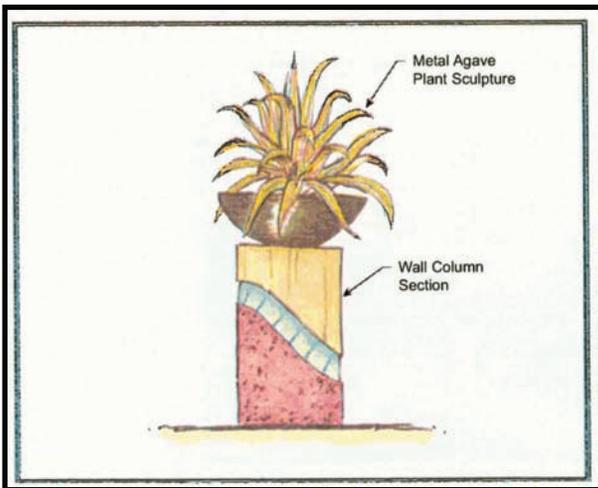


Figure 5.32—Decorative planter

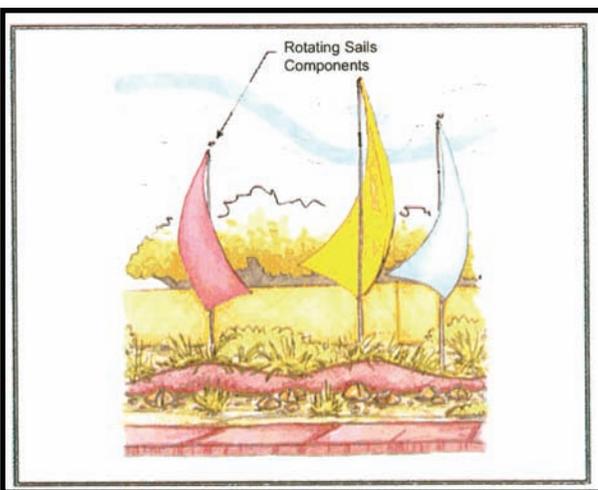


Figure 5.33—Interpretive sails

- c. There shall be a thirty (30) inch maximum grade change above or below median curb elevation.

### 3. ARCHITECTURAL SCULPTURE ELEMENTS (20-78-5.III.D.3)

- a. Elements shall be concentrated in areas of heavy use and visual prominence.
- b. Elements shall be incorporated into streetscape amenities such as bus stops, pedestrian rest areas, or simply as stand-alone focal points for public art displays.

- c. Sculpture elements shall be located outside of sight visibility triangles, rights-of-way, and utility easements. Typically, these elements should be located five (5) to ten (10) feet from the nearest right-of-way line. Deviations from this spacing may be supported depending on specific site conditions.

### 4. LANDSCAPING (20-78-5.III.D.4)

- a. The species and density of landscape materials shall meet or exceed the right-of-way planting standards found in the Zoning Ordinance. Median right-of-way landscape area shall be included as part of the required right-of-way landscaping calculation for properties adjacent to Lake Pleasant Parkway.

- b. Plant species shall be selected from the Arizona Department of Water Resources (ADWR) Landscape Planting Materials list. This list includes a preliminary plant palette of drought tolerant/low water use plants with bold forms and colors.

- c. All required right-of-way landscaping shall be located outside of the ultimate right-of-way.

Interim rights-of-way shall be landscaped with shrubbery, groundcover, and dust proofing with the understanding that future improvements will eliminate this landscaping.

- d. Inert materials shall include: decomposed granite, cobble, riprap, salvaged soil, native boulders, river rock, rock mulch, and desert wash sand.

### 5. PUBLIC ART (20-78-5.III.D.5)

Artistic expressions, either integrated into streetscape elements or as individual creations, should be used to enrich the experience of public spaces and to mark or signal a specific “place”.

- a. Opportunities for public art include:
  1. Seating areas and bus stops.
  2. Gateways areas.
  3. Subdivision entries.
  4. Trailheads.
  5. Public facility entries.
- b. Theme ideas should include:
  1. Sails or interpretive sail shapes.
  2. Watercourse shapes/forms.
  3. Interpretative desert plant shapes or replicated plants.
  4. Gabion pillars used to influence water flow or to enhance wall placement and bus stop design.
  5. Wind spirals – kinetic art or the appearance of kinetics, without becoming distracting or unsafe.

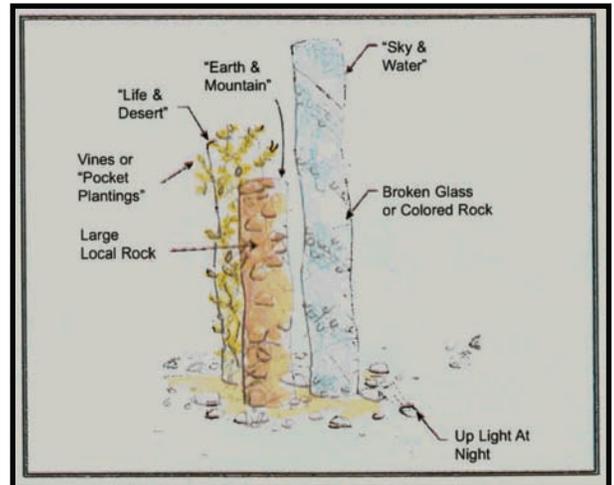


Figure 5.34—Gabion pillars

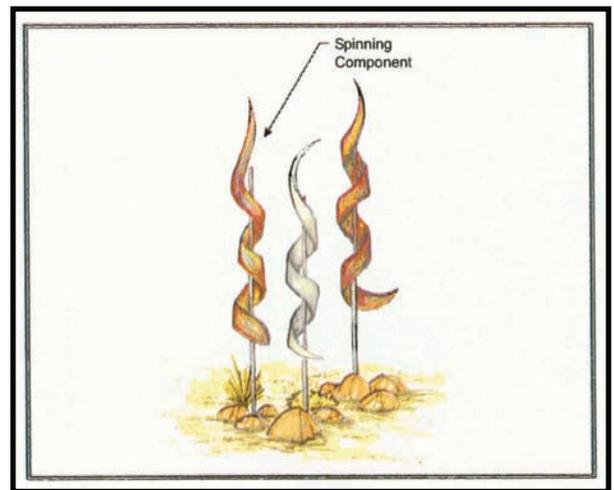


Figure 5.35—Vertical wind spiral

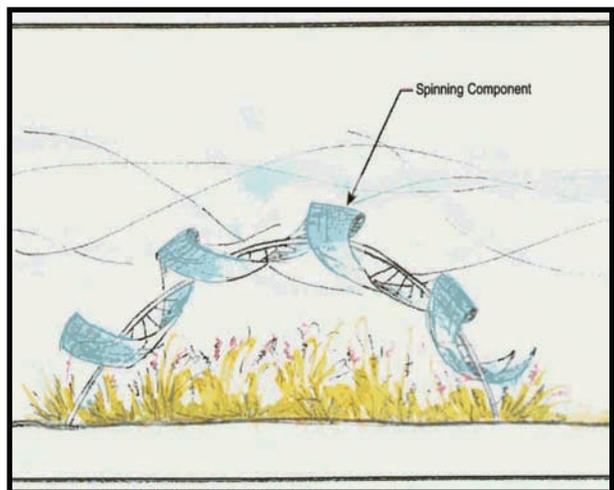


Figure 5.36—Wind spiral

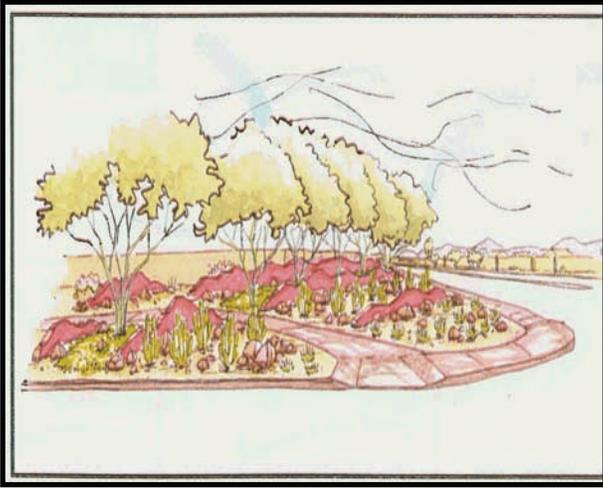


Figure 5.37—Layered mountains gateway

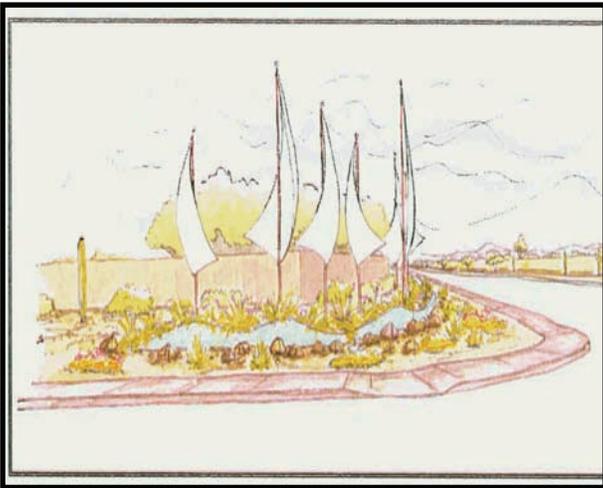


Figure 5.38—Sail gateway

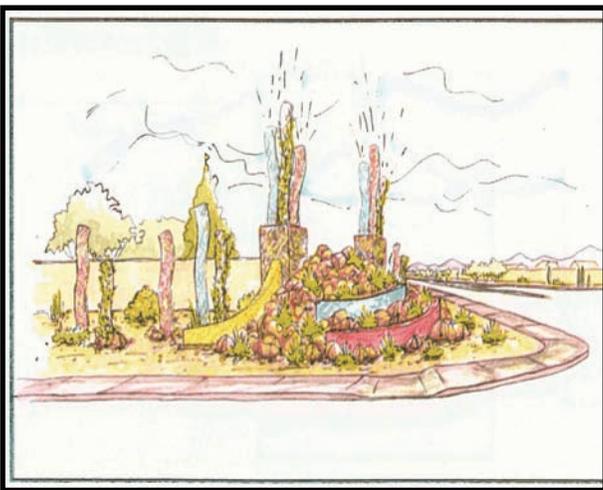


Figure 5.39—"spillway" & pillars gateway

## 6. GATEWAYS AND TRAILHEADS (20-78-5.III.D.6)

Gateway opportunities for Lake Pleasant Parkway should reinforce the design theme in areas of the right-of-way. Gateway and trailhead areas welcome the public and punctuate the streetscape theme, linking the pedestrian and vehicular user. A gateway, corner treatment, or trailhead should combine the large visual scale of median requirements with the detail interest and creativity of the artistic requirements. Possible gateway locations are identified in Map 5.2.

- a. All sides of an intersection should be used to incorporate the theme elements of art, landforms, plantings, and lighting.
- b. To provide a sense of arrival at all themed gateway or trailhead areas, the following elements should be incorporated:
  1. Entry monument or sculpture, water feature or other identifying element consistent with the area and/or the theme established through a particular zone of the Parkway.
  2. Themed Landscaping.
  3. Public Art.
  4. Signage.
  5. Seat and/or Decorative Walls.

## 7. LIGHTING AND BANNERS (20-78-5.III.D.7)

To the maximum extent possible, these elements should share the theme of the particular Parkway zone in which they are located and should be of a scale that is compatible with the pedestrian environment.

- a. Decorative lighting should include:
  1. LED's incorporated in walls.
  2. Tree and shrub lighting.
  3. Low-level bollard lighting in pedestrian areas.
- b. Pole lighting should include:
  1. Paint colors that enhance/reinforce the theme palette.
  2. Banner opportunities, such as panel or sail banners.

### 8. STREETSCAPE AMENITIES (20-78-5.III.D.8)

- a. Streetscape amenities should include furniture, lighting, trash receptacles, and bike racks to provide functional convenience for Parkway users.
- b. Streetscape amenities should integrate art and iconic elements.
- c. Streetscape amenities must be selected to reinforce the design theme through their shapes, colors, and materials.
- d. Right-of-way landscaping and groundcover for all projects along Lake Pleasant Parkway should continue adjacent landscaping themes to the extent feasible.

## E. CHARACTER AREAS DESIGN CONCEPTS AND GUIDELINES (20-78-5.III.E)

The primary design concept for Lake Pleasant Parkway is based on the Parkway's namesake—Lake Pleasant. This concept presents itself in the

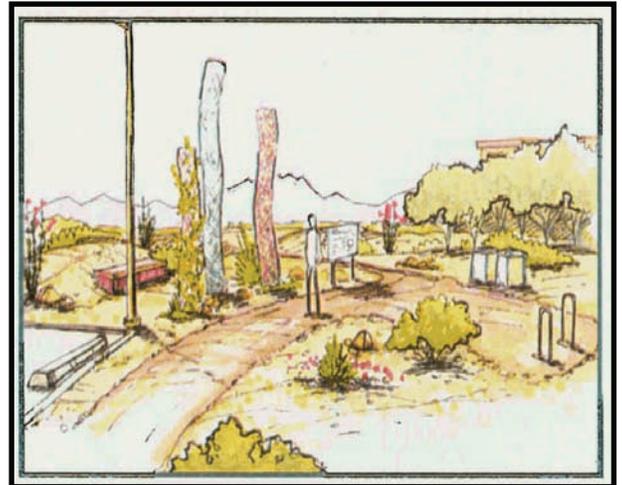


Figure 5.40—Small gateway at trailhead

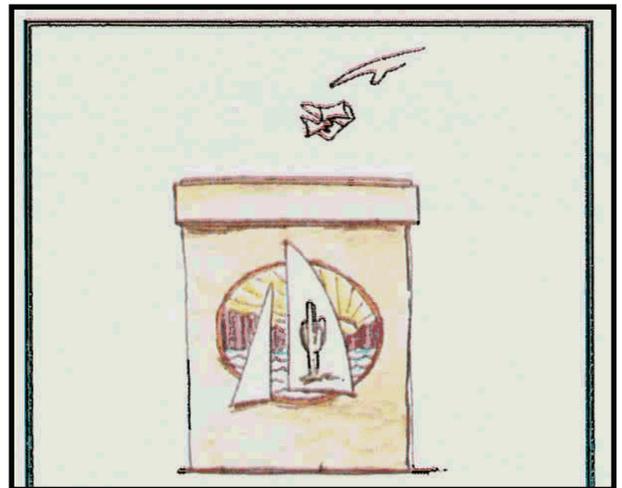


Figure 5.41—Trashcan with sail/sunset icon

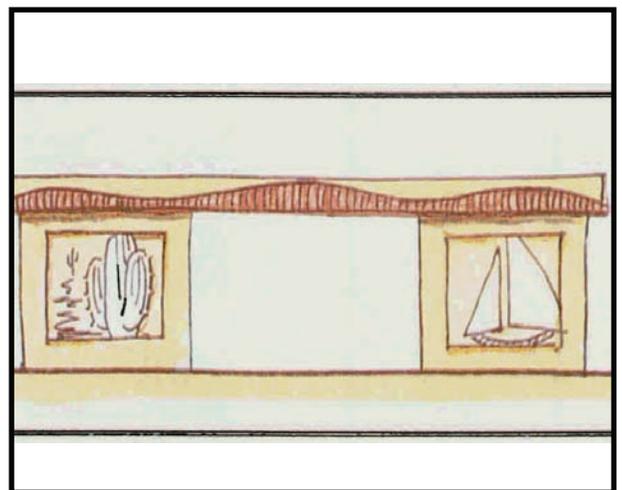


Figure 5.42—Bench with window frame blocks

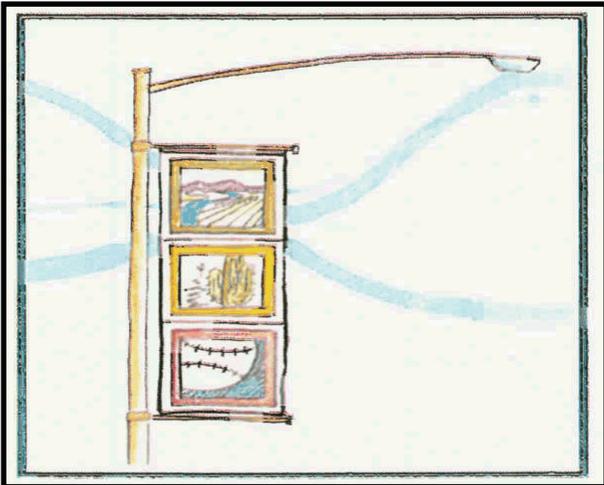


Figure 5.43—Window frame panel banner

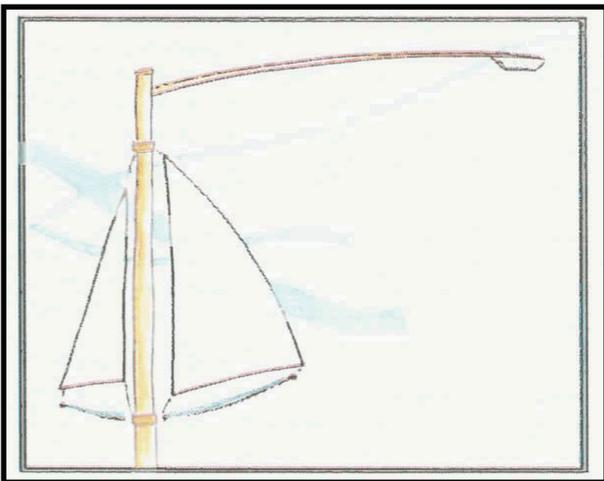


Figure 5.44—Sail banner

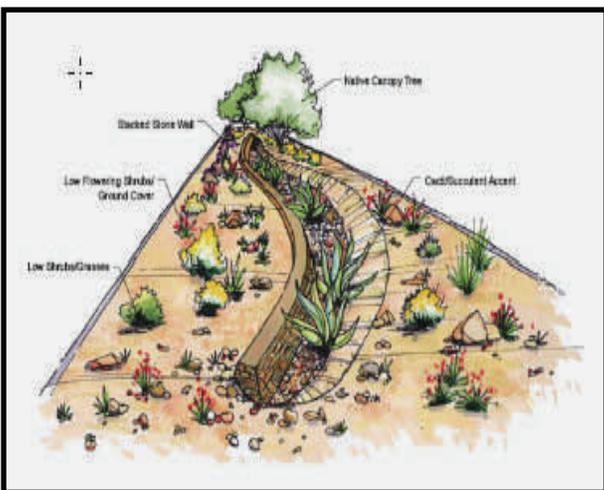


Figure 5.45—Typical urban median planting with decorative low walls

form of underlying ‘waterway’ or ‘wave’ elements in wall design, landscaping, artistic endeavors, and hardscape patterns and materials.

The City has established a series of zones along the Parkway which were created based on existing and proposed land uses and densities. These zones have been labeled ‘Urban’, ‘Suburban’ and ‘Rural’ in concert with the zones established in the General Plan Land Use Element. Medians and right-of-way landscape areas will be treated accordingly depending upon the zone in which they occur. The following design guidelines are intended to illustrate the vision for each zone along the Parkway and may be expanded depending upon the nature of each individual project.

### 1. URBAN ZONE (20-78-5.III.E.1)

Through the General Plan Land Use Element defines the Urban Zone along the Parkway, all Commercial nodes as identified on the General Plan Land Use Map, will be subject to the Urban Zone design standards as provided below. The established view corridor setback criteria will remain as stated in the General Plan.

- a. Thematic elements shall be contemporary in nature, but shall provide a range of materials, colors and forms that maintain the ‘wave’ theme. Use of stone, split face block, brick, or other materials associated with texture is discouraged. Materials such as smooth block, glass block, tile, smooth finish stucco, or similar materials are encouraged in the Urban Zone. Deviations from these materials may be considered if a severe contrast with surrounding architecture is demonstrated.
- b. Built elements such as walls, bus stops, and pedestrian amenities shall reflect regional architecture types.
- c. This zone shall contain the highest concentration of artistic elements and public art.
- d. Plantings shall be formalized and of the highest

density, with a high concentration of flowering species and street trees.

e. Walls shall be:

1. Placed in grouped series of segments in lieu of a single long segment.
2. Less curvilinear and possibly straight runs of walls parallel to traffic lanes, often overlapping each other to form canal-like runs.
3. Architectural forms, with level or angled surfaces and edges.
4. Reflecting architectural themes with bright solid areas of colors.
5. Enhanced with low-level lighting, possibly tinted to complement the colors of the wall itself.

f. Berms shall be constructed with defined edges and/or man made forms.

g. Architectural sculpture elements shall be distributed throughout median areas in clusters of five (5) to seven (7) elements for every five hundred (500) linear feet where medians are wide enough to allow them. If unable to provide these quantities in the median, the option to provide in the adjacent right-of-way landscape areas is strongly encouraged where appropriate.

h. Inert materials shall include decomposed granite, cobble and riprap. Colors shall be primarily browns with assorted accents of blues, greens, and grays.

## 2. SUBURBAN ZONE (20-78-5.III.E.2)

Located between Happy Valley Road and Dynamite Boulevard.

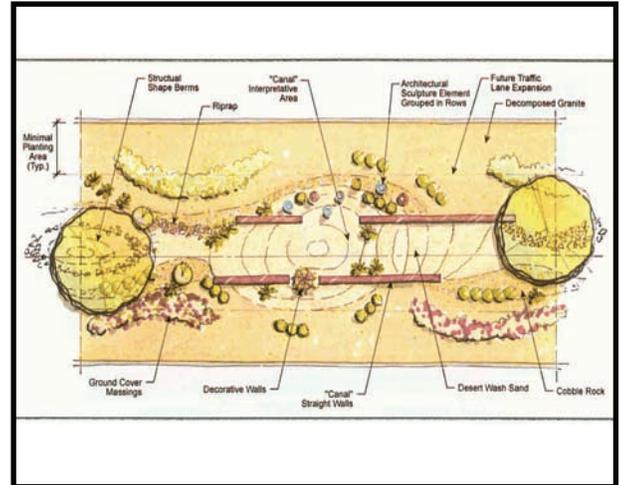


Figure 5.46—Typical urban 00' plan

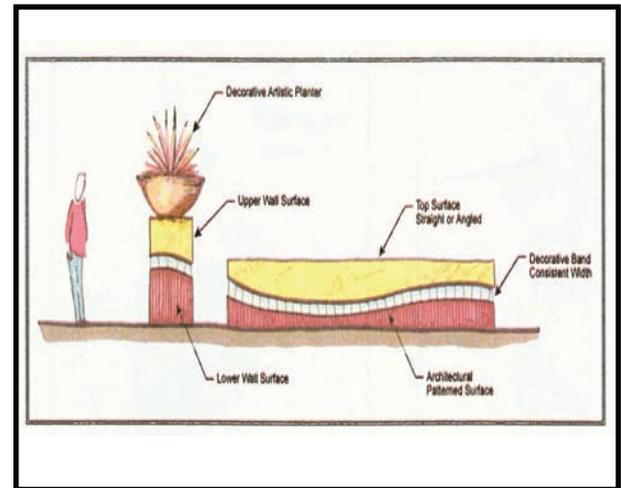


Figure 5.47—Urban character wall design

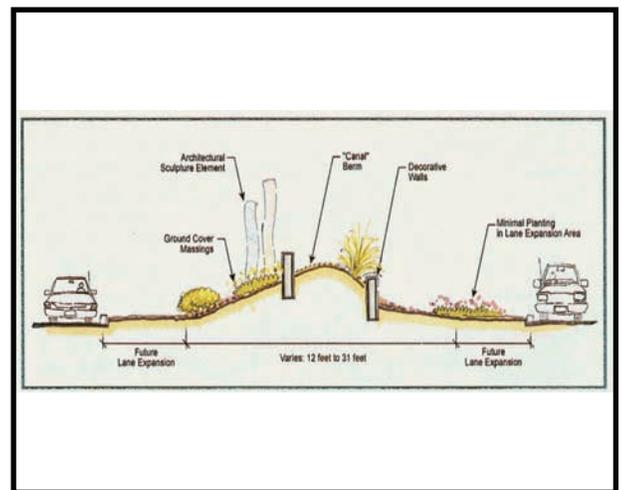


Figure 5.48—Typical urban cross section

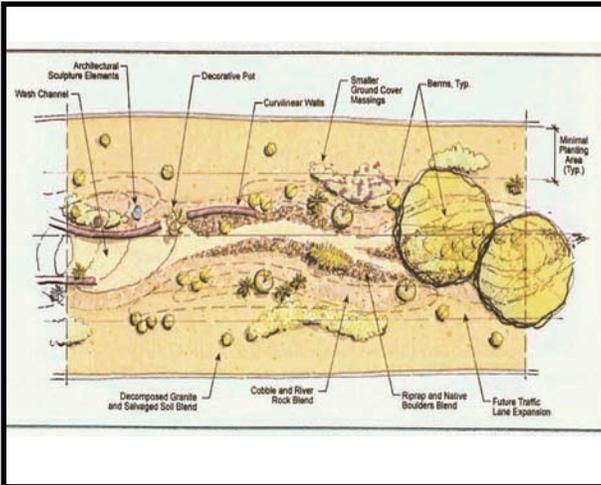


Figure 5.49—Typical suburban 100' plan

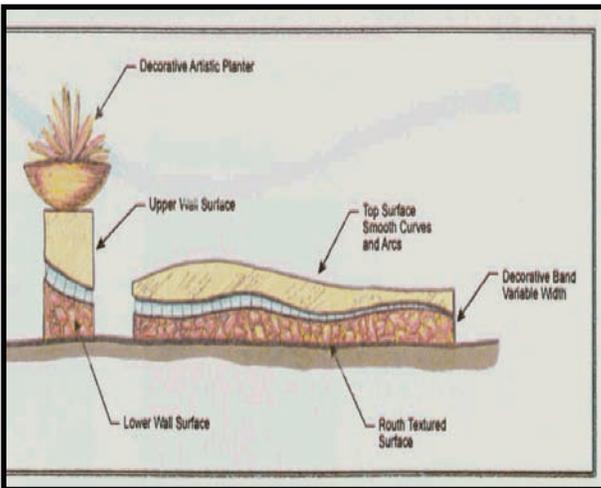


Figure 5.50—Suburban Character wall design

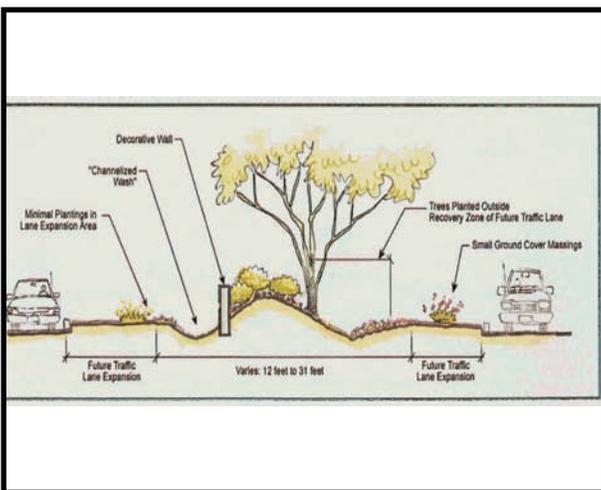


Figure 5.51—Typical suburban cross section

- a. Theme elements shall suggest organic forms with interpretative shapes of the surrounding desert landscape.
- b. Built elements shall be less dense in median design than what is found in the Urban Zone.
- c. Architectural sculpture elements and public art opportunities shall be provided at a rate of three (3) to five (5) per five hundred (500) linear feet where medians are wide enough to allow them. If unable to provide these quantities in the median, the option to provide in the adjacent right-of-way landscape areas is strongly encouraged where appropriate.
- d. Plantings shall be a mix of naturalized forms and bold accents, but shall be planted with lower-level and less clustered plantings than in the Urban Zone.
- e. Walls shall be:
  1. More curvilinear in form than in the Urban Zone and shall begin more pronounced vertical undulation.
  2. Constructed in overlapping segments to offer visual relief and opportunities for the placement of artwork.
  3. Man-made forms inspired from natural forms with the 'wave' theme incorporated into wall elevation through use of materials and colors. Materials with a more natural feel and/or coarse textures may be transitioned into wall design.
  4. In muted desert tones having accent areas of bold solid tones.

- f. Berms shall have softened edges.
- g. Inert materials shall include decomposed granite and salvaged soil, cobble, river rock, and blended riprap with native boulders. Colors shall be those native to the area surrounding Lake Pleasant Parkway, mixed with richer browns accented with blues, greens, golds, reds, and grays.

### 3. RURAL ZONE (20-78-5.III.E.3)

Located north of Dynamite Boulevard.

- a. Theme elements shall integrate organic shapes, that mimic the surrounding desert forms.
- b. Constructed theme elements shall be placed at ratios that are less dense, but equally as meaningful as what is provided in the Suburban Zone.
- c. Architectural sculpture elements shall be provided at a rate of three (3) to five (5) per seven hundred and fifty (750) linear feet where medians are wide enough to allow them. If unable to provide these quantities in the median, the option to provide in the adjacent right-of-way landscape areas is strongly encouraged where appropriate. In this zone only, public art features may be concentrated at intersections or other focal points.
- d. Plantings shall reflect the surrounding desert, with less formal groupings and more natural plant densities and species.
- e. Walls shall be:
  - 1. The lowest of the three (3) zones in order to maximize views and minimize visual distraction in the area.

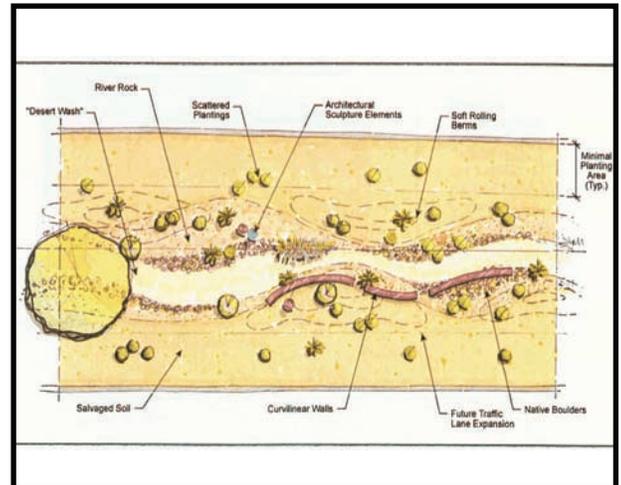


Figure 5.52—Typical rural 100' plan

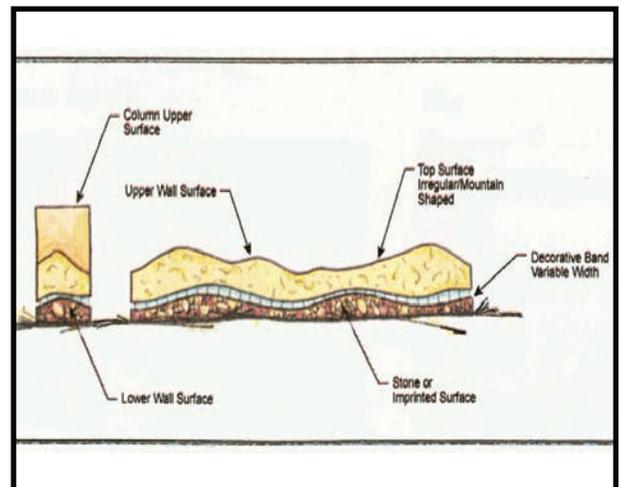


Figure 5.53—Rural character wall design

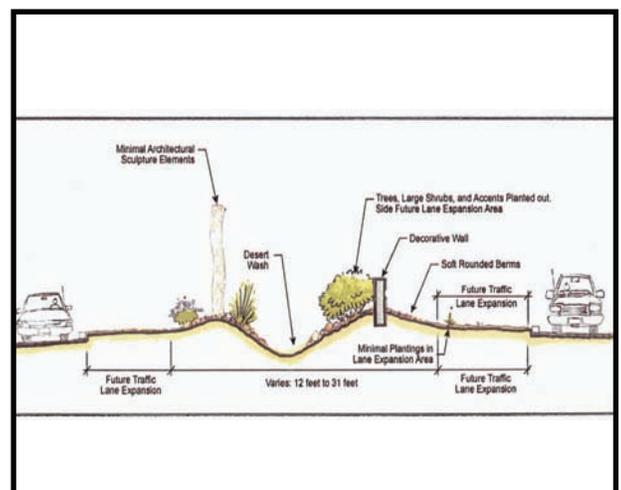


Figure 5.54—Typical rural cross section

2. Designed to maximize vertical and horizontal undulation in conjunction with integrated berming.
  3. Separated into multiple segments with sections overlapping by five (5) to ten (10) feet.
  4. Constructed of coarse textures and natural materials including stone, rock-filled gabions, block, and other similar materials.
- f. Saguaros should be provided in exchange for median and right-of-way trees at a ratio of one (1) mature specimen saguaro for two (2) mature trees to serve as visual indicators of place.
  - g. Berms shall be softly rounded in natural shapes.
  - h. Inert materials shall include salvaged soil, river rock and native boulders. Colors shall be stone native to the area surrounding Lake Pleasant Parkway.





## IV. Utility Cabinet Screening

# IV. Utility Cabinet Screening

## IV. UTILITY CABINET SCREENING

### A. INTENT (20-78-5.IV.A)

The purpose of this section is to establish placement and screening criteria for utility cabinets and infrastructure to reduce the visual impacts created by the location of equipment along arterial and collector roadways.

### B. STANDARDS (20-78-5.IV.B)

#### 1. PLACEMENT STANDARDS (20-78-5.IV.B.1)

The developer is responsible to work with the utility companies to coordinate the location of utility cabinets according to the provisions listed below. If required, an on-site easement shall be provided by the developer. The City shall comment on the location and screening methods of all utility cabinets in standard comment form and will prepare a standard Condition of Approval / Stipulation upon site plan approval, which must be satisfactorily addressed during building permit review. The City shall have the authority to modify the placement of the utility cabinets, walls, berming and landscaping to provide sufficient screening from public rights-of-way, public access easements, roadway easements, and private streets.

The utility cabinets shall be placed in the designated areas as indicated below. Deviation from these areas may be approved by the Planning Manager or designee provided sufficient justification is afforded by the applicant.

a. **Utility Cabinet Placement**

Non-Residential / Multi-Family

1. Landscape Buffer or Side/Rear Setback area. (Figure 5.55)

Utility cabinets shall be located in landscape buffers or side / rear building setback areas. The location shall be outside of retention and street frontage landscape areas and must be accessible by service vehicles.

2. Street Frontage Landscape Area (Figure 5.56)

Utility cabinets of any height shall not be located within sight visibility triangles (per Engineering Department detail) associated with driveways and road intersections. Cabinets exceeding thirty (30) inches in height shall be located beyond the first one hundred and fifty (150) feet of street frontage and primary driveways measured from face of curb of the intersection and at least fifty (50) feet from any sight visibility triangle. Cabinets less than thirty (30) feet in height may be located within the first one hundred and fifty (150) feet of the site, but may not be located in a 75' x 75' triangle at the intersection of two (2) arterials. For roads that are recognized as scenic roadway corridors or requiring a special setback / landscape buffer, utility cabinets shall be placed along a side street, access aisle or completely outside of the setback/ landscape buffer area. In all locations, cabinets must be outside of retention areas and must be accessible by service vehicles.

Single-Family Residential

1. Residential Development (Figure 5.57)

To the maximum extent possible, utility cabinets are to be located on streets classified as residential/ local streets only. The cabinets shall be located in PUE's in areas contained within the side / rear yard landscape tracts. In the event that placement along collector or

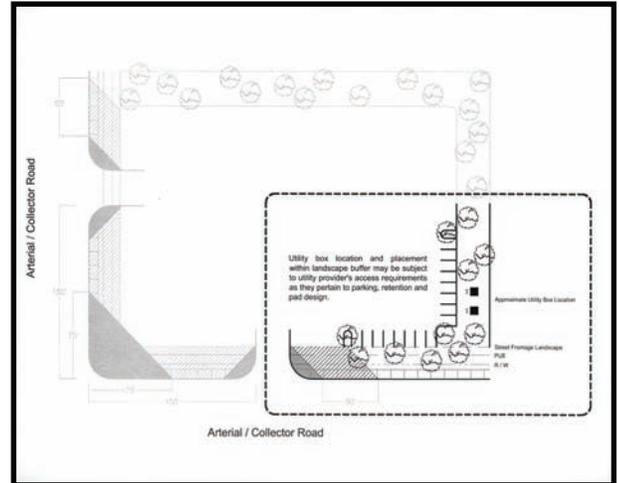


Figure 5.55—Landscape Buffer of Side/Rear Setback Area.

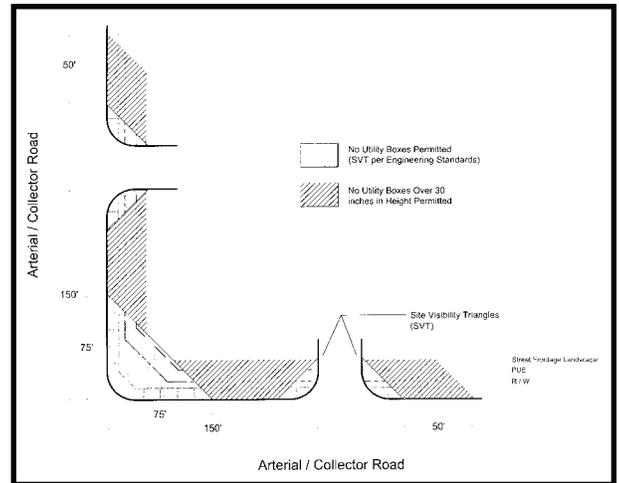


Figure 5.56—Street Frontage Landscape Area.

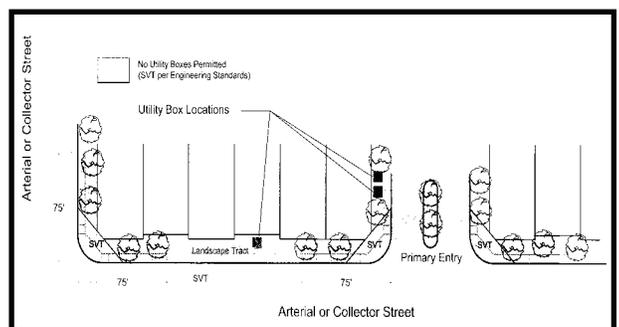


Figure 5.57—Residential Development.



Figure 5.58—Service Entrance Section screened with landscaping and an architecturally compatible wall.

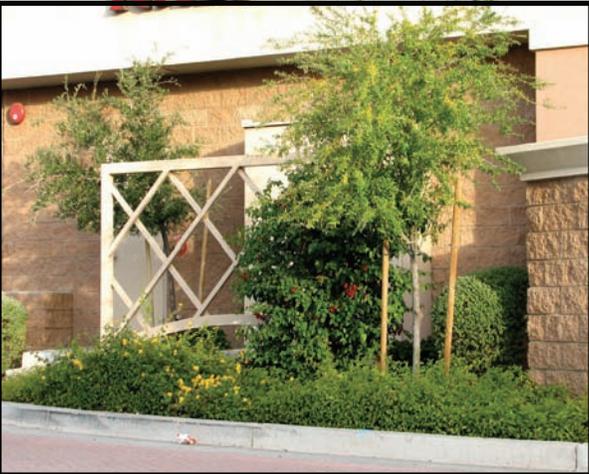


Figure 5.59—Trellis and dense landscaping provide ample screening of Service Entrance Section



Figure 5.60—Screen wall conceals a Service Entrance Section on an industrial building

arterial streets is unavoidable, a modified version of Option #2 above will be applied. For all primary entrances, no cabinets shall be located along the collector or arterial street within seventy five (75) feet of the entry. Flexibility will be granted for the distance requirement along the entry 'throat' into the development due to variations in minimum lot depth. Cabinets located along collectors and arterials shall be inset into required wall undulations, provided accessibility is maintained. In all locations, cabinets must be outside of retention areas and must be accessible by service vehicles.

## 2. SCREENING DESIGN STANDARDS (20-78-5.IV.B.2)

### a. Utility Cabinets

Utility cabinets shall be screened by architecturally integrated walls that complement the architecture of the surrounding development, berming and / or dense landscaping. Walls cannot limit accessibility to the cabinet.

The exterior of all utility cabinets shall utilize earth tone colors to match the predominate color of the surrounding groundcover material / screenwalls.

### b. Exterior Wall Equipment

Exterior wall equipment, including electrical meters, electrical distribution cabinets, service entrance sections (SES) shall be incorporated in the design of the building, whether in a separate utility room or in an alcove to allow the equipment to be flush with the building façade.

Exterior wall equipment that is contained in an alcove located on the street side of the building and shall be fully screened from view through the use of screen walls, landscaping, trellis, green screens, or a combination thereof.



Figure 5.61—Exterior wall equipment is located in alcove in loading area for protection and screening.

